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PR-2018-001577, SI-2024-00994

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CITY OF ALBUQUERQUE

Albuquerque, New Mexico


Planning Department

Mayor Timothy M. Keller

INTER-OFFICE MEMORANDUM

September 16, 2024

TO: Dan Lewis, President, City Council

FROM: Alan Varela, Planning Director 
Alan Varela (Sep 23, 2024 09:41 MDT)

SUBJECT: AC-24-26, PR-2018-001577, SI-2024-00994

Six appellants—the nonprofit organization NAEVA, the Pueblo of Laguna, Santa Fe Village Neighborhood Association (SFVNA), West Side Coalition of Neighborhood Associations (WSCONA), Michael T. Voorhees, and Jane Baechle, appeal the Environmental Planning Commission (EPC) decision to approve the Site Plan for the Double Eagle II (DEII) Airport (for all or a portion of Tract A-1, Plat of Tract A-1 & Tract L-1 Parcels 1-5, Double Eagle II Airport; Tract C and Parcel-1 Tract M Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; the Southeasterly Portion of Tract D-1-A-1, Tract D-1-A-2, and Tract S-1-A Plat of Tracts D-1-A-1, D-1-A-2, & S-1-A of Amended Bulk Land Plat for Aerospace Technology Park; Tract E, F, and K Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; Tract N-1 and Tract N-2 AKA Parcel 4 Bulk Land Plat of Tracts N-1, O-1, & N-2 Parcels 1, 2, 3, & 4 Double Eagle II Airport & Adjacent Lands; and Tract S-2 Bulk Land Plat for Aerospace Technology Park, Tracts D & S of Double Eagle II Airport & Adjacent Lands), located at 7401 Paseo Del Volcan NW, northwest of the intersection of Atrisco Vista Blvd. NW and Shooting Range Access Rd., approximately 4,200 acres (C-4, C-5, C-6, D-4, D-5, D-6, E-4, E-5, E-6, F-4, F-5, F-6, G-4, G-5, G-6).

REQUEST

This is an appeal of the EPC's decision to approve a Site Plan – EPC for the Double Eagle II (DEII) Airport. The request documents the existing, as-built airport facilities and proposes Design Standards required for future development at the DEII Airport under the NR-SU zone district. The NR-SU zone district requires a Site Plan that establishes allowable uses and development standards, that are negotiated at the EPC through the EPC application and the Site Plan. The Site Plan must also be in compliance with the Master Plan for DEII.

The Site Plan application was submitted and accepted as complete in July 2024, prior to the effective date of the most recent IDO update, so the decision and the appeal are subject to the IDO amended as of July 2023, so no tribal referral was required. The Master Plan amendment adopted by Council on August 5, 2024 was not yet in effect for this application.

EPC DECISION

The EPC heard and approved the request at a quasi-judicial hearing on August 15, 2024. The decision was based on testimony at the hearing and 24 findings of fact with 10 conditions of approval that are elaborated in the Official Notification of Decision (NOD) dated August 15, 2024.

APPEAL

Appeal procedures are found in the Integrated Development Ordinance (IDO) §14-16-6-4(V). All six appellants—NAEVA, the Pueblo of Laguna, Santa Fe Village Neighborhood Association, Westside Coalition of Neighborhood Associations, Michael T. Voorhees, and Jane Baechle—made an appearance of record by providing verbal comments at the August 15, 2024 public hearing; SFVNA, WSCONA, and Jane Baechle also provided written comments.

Standing should be confirmed by the Land Use Hearing Officer but may include the following:

- Pueblo of Laguna: a governmental agency whose properties or interest may be affected by the application [6-4(V)(2)(a)2].
- NAEVA, Santa Fe Village Neighborhood Association, West Side Coalition of Neighborhood Associations, Michael T. Voorhees, and Jane Baechle: Any other person or organization that can demonstrate that his/her/its property rights or other legal rights have been specially and adversely affected by the decision[6-4(V)(2)(a)4].

The IDO's Criteria for Decision of an Appeal (§14-16-6-4(V)(4)) is whether the decision-making body or the prior appeal body made one (1) of the following mistakes:

- 6-4(V)(4)(a)** The decision-making body or the prior appeal body acted fraudulently, arbitrarily, or capriciously.
- 6-4(V)(4)(b)** The decision being appealed is not supported by substantial evidence.
- 6-4(V)(4)(c)** The decision-making body or the prior appeal body erred in applying the requirements of this IDO (or a plan, policy, or regulation referenced in the review and decision-making criteria for the type of decision being appealed).

In an August 29, 2024 appeal, the appellants assert that the decision from the EPC is not supported by substantial evidence [§14-16-6-4(V)(4)(b)] and was made in error in applying the requirements of the IDO [§14-16-6-4(V)(4)(c)].

Accordingly, the appellants argue that:

- (1) The EPC decision presents a major threat to the Petroglyph National Monument and the rights of Indigenous people to use of the Petroglyph National Monument and who are specially and adversely affected by the EPC decision.
- (2) There are culturally relevant medicinal plants that grow along the pathways up to the Petroglyphs that could be affected by the air pollution from increased traffic.
- (3) Neighborhood interests were not protected, such as building heights, views, protection of Major Public Open Space, and quality of life.

- (4) The decision specially and adversely affects neighborhoods and petitioning individuals by reducing their enjoyment of the Major Public Open Space by degrading the character of the cultural landscape.
- (5) The decision will result in loss of use and scenic viewscape, as well as views in the neighborhood and near the Petroglyph National Monument, and
- (6) other considerations in IDO §14-16-5-2(A).

RESPONSES

The EPC applied the requirements of the IDO based on substantial evidence in the record. The Official Notification of Decision contains 24 Findings and 10 Conditions of Approval that support the EPC's decision. The Findings were developed based on thorough analysis contained in the Staff report.

1. **Appellant:** The EPC lacks authority to approve an “as-built” Site Plan with design features, including for very large and high buildings, for 4,200 acres covered by a Master Plan recently amended.

Staff Response: A Site Plan is required for any development to occur under the NR-SU zone district. There is no Site Plan on file for the subject site. The request documents the existing, as-built airport facilities and proposes Design Standards required for future development at the Double Eagle II (DEII) Airport. The applicant is required to document the existing development in an “as-built” Site Plan to establish a baseline for any proposed future development. The NR-SU Site Plan can establish allowable uses and development standards as negotiated at the EPC. The Site Plan must be in compliance with the Master Plan for DEII. This Site Plan application was accepted as complete prior to the effective date of the latest amendment to the DEII Master Plan, so this Site Plan was required to comply with the Master Plan last amended in 2019 [Finding #4].

2. **Appellant:** The approval is premature when the Master Plan for Double Eagle II is available for amendment by the City. The new Site Plan appears to be in lieu of a properly processed amendment to the Double Eagle II Master Plan. The new Site Plan of 4,200 acres is enormous for a “Site Plan” and appears to be coextensive with the Master Plan.

Staff Response: The subject site is within the boundaries of the existing Rank 3 DEII Airport Master Plan (see [Enacting Resolution, R-2019-065](#)). The subject site for the request does not include 2 areas covered by separate Site Plans, one for Eclipse Aviation and one for the Aerospace Technology Park. A Site Plan is required for development to occur under the NR-SU zone district.

The Eclipse Aviation Campus Site Plan for Subdivision (PR 1003125 & 06DRB-00401) and the Aerospace Technology Park Site Development Plan for Subdivision (PR 1003125 03EPC-02054 & 05EPC-00566) govern portions of the site that were not part of the request for Site Plan – EPC (PR-2018-001577/ SI-2024-00994) as indicated on proposed Site Plan, Sheet SDP1.1 and as addressed in staff's response to IDO §14-16-6-6(I)(3)(b).

The IDO defines a Site Plan as “an accurate plan that includes all information required for that type of application, structure, or development” [§14-16-7-1]. There is no size limit on Site Plans.

The Site Plan is required to comply with the Master Plan in effect when the application was accepted as complete [IDO §14-16-1-7(C)]. Amendments to the Master Plan were heard before the Environmental Planning Commission (EPC) at the April 18, 2024 hearing and then approved by City Council August 5, 2024. (See [R-24-71](#).) They were not in effect when the Site Plan application was accepted as complete in July 2024.

3. **Appellant:** The approved “as-built” Site Plan is not in conformance with the Double Eagle II Master Plan and related approved site plans.

Staff Response: The “as-built” Site Plan was analyzed and found to conform with the DEII Master Plan [Finding #6]. The other two approved site plans do not include the subject site, and the subject site does not include the areas in the approved site plans. A Site Plan is required for development under the NR-SU zone district.

4. **Appellant:** The subject site apparently has multiple existing approved Site Plans which should be reviewed and considered in connection with any “as built” or new site plan.

Staff Response: Pursuant to IDO §14-16-6-6(I)(1)(b), “A Site Plan – EPC may be approved for property with a prior-approved Site Plan, regardless of whether the prior approved Site Plan is still valid pursuant to Subsection 14-16-6-4(X), subject to allowable uses and development standards in this IDO. If any portions of the proposed boundary overlap with a prior-approved Site Plan that will remain in place, a Major Amendment shall be required as described in Subsection 14-16-6-6(I)(2)(d) below.” In this case, there is no overlap with a prior-approved Site Plan, and no development was proposed as part of this request. Future development will require amendments to the relevant Site Plan.

The Eclipse Aviation Campus Site Plan for Subdivision (PR 1003125 & 06DRB-00401) and the Aerospace Technology Park Site Development Plan for Subdivision (PR 1003125 03EPC-02054 & 05EPC-00566) govern portions of the site that were not part of the request for Site Plan – EPC (PR-2018-001577/ SI-2024-00994) as indicated on proposed Site Plan, Sheet SDP1.1 and as addressed in staff’s response to IDO §14-16-6-6(I)(3)(b).

5. **Appellant:** The subject site apparently is not platted into a single property.

Staff Response: Pursuant to IDO §14-16-6-6(I), “a Site Plan – EPC may only be approved for legally platted or nonconforming lots, and may not be approved on unsubdivided property, *except for development in the PD or NR-SU zone districts* and any development on a site 5 acres or greater adjacent to Major Public Open Space, in which case a Site Plan approval is required prior to any platting action” [emphasis added].

6. **Appellant:** It appears that the decision does not comply the requirements of IDO Section 5-2(J) concerning Major Public Open Space (“MPOS”).

Staff Response: The request is for an as-built Site Plan and Design Standards consistent with a NR-SU zone district with no proposal for development. The DEII Airport property extends to

both sides of Alta Vista Blvd. NW, a Principal Arterial, and both IDO §14-16-5-2(J)(1) (Lots within 330 Feet of Major Public Open Space) and IDO §14-16-5-2(J)(2) (Lots Adjacent to Major Public Open Space), each with specific standards governing development to enhance and protect Major Public Open Space, will apply to any future development.

7. **Appellant:** The applicant did not comply with the Tribal Meeting requirements of IDO Section 6-4(B). The rushed timing and circumstances of the City's application suggest intent to avoid the meeting requirements of IDO Section 6-4(B).

Staff Response: The Pre-Submittal Tribal Meeting requirement was adopted by City Council on June 17, 2024 and went into effect August 3, 2024 (see [Pre-submittal Tribal Meeting Request](#)), after the Site Plan – EPC request was accepted as complete by the City on July 5, 2024. The City Council did not establish a moratorium on accepting applications between the adoption and effective date of the IDO.

Pursuant to IDO §14-16-1-7(C), the Site Plan was reviewed and decided based on the regulations and plans in effect when the application was accepted as complete by the City.

8. **Appellant:** The Comprehensive Plan analyses of the decision are in error. The decision is not consistent with the Comprehensive Plan.

Staff Response: EPC found the Site Plan to be generally consistent with applicable Comprehensive Plan Goals and policies. See Findings #10-18.

9. **Appellant:** EPC Finding 19A is in error because the new Site Plan is not consistent with the Comprehensive Plan.

Staff Response: See the response to #8 in this Memo. EPC deliberations and subsequent revisions of Conditions of Approval recommended by Planning staff addressed the conflicts with Comp Plan Goals and policies, particularly as they relate to reducing maximum building heights and establishing areas off limits to development or landscaping near Major Public Open Space. (See Condition of Approval #6.)

Any future development must abide by the proposed design standards. Where those approvals are silent, provisions in the IDO will apply.

10. **Appellant:** EPC Finding 19B is in error because some provisions of the new Site Plan conflict with existing plans.

Staff Response: No provisions of the proposed Site Plan conflict with any other existing plan. The proposed site plan does not include a portion of any prior-approved Site Plan.

11. **Appellant:** EPC Finding 19C is in error for various reasons.

Staff Response: See Finding 19C. More information is needed from the appellant in order to respond.

- 12. Appellant:** EPC Finding 19D is in error because approval of an “as built” Site Plan, if that is possible, is not the place to approve proposed new future development.

Staff Response: There is no development being proposed as a part of the submitted request. This request documents the existing, as-built airport facilities and proposes Design Standards required for the DEII Airport under the NR-SU zone district. The NR-SU zone district requires a Site Plan, which can establish allowable uses and development standards. The IDO does not distinguish an “as-built” Site Plan from a Site Plan for proposed development. EPC found the proposed Site Plan to meet the review and decision criteria in §14-16-6-6(I)(3) [Finding #19].

- 13. Appellant:** EPC Finding 19E is in error because the Site Plan does not mitigate any significant adverse impacts, including buildings of excessive size and height, to the surrounding area particularly the Petroglyph National Monument.

Staff Response: The requested Site Plan establishes a baseline for allowable uses, design standards, and other development regulations for current and any future development. Future development will require a Site Plan amendment. The proposed development will need to meet or amend these Site Plan standards and conform to relevant IDO standards, where the proposed controlling Site Plan is silent.

EPC deliberations and subsequent revisions of Conditions of Approval recommended by Planning staff responded to and helped to mitigate potential impacts related to building heights and development near Major Public Open Space. Maximum building heights were reduced near Major Public Open Space to the east and to the west, and areas where development (including parking or landscaping) is prohibited were established to create a buffer to MPOS areas. (See Condition of Approval #6.)

- 14. Appellant:** The City lacks vested rights in any future development of the site except as controlled by the IDO and the Master Plan.

Staff Response: The City of Albuquerque owns the parcels comprising the subject site. The controlling Rank 3 Double Eagle II Airport Master Plan was developed by City Aviation and approved by City Council to guide the development, maintenance, and operation of individual public resources or facilities. For the purposes of the State Constitution, the Master Plan is a duly adopted plan or any of its parts for the development of an area within the planning and platting jurisdiction of a municipality for the general purpose of guiding and accomplishing coordinated, adjusted, and harmonious development.

The IDO establishes an airport as a use that requires NR-SU zoning [§14-16-2(E)(2)(a)], and the NR-SU zone district requires a Site Plan to establish allowable uses and standards for development [§14-16-2-5(E)(3)].

Pursuant to IDO §14-16-6-6(I), the proposed Site Plan is an accurate plan that includes all information required for any proposed future development and meets the required Review and Decision Criteria [Finding #19].

15. Appellant: The new Site Plan may lack all information required for the type of application, structure, or development.

Staff Response: Where the proposed controlling Site Plan is silent, IDO standards apply [§14-16-2-5(E)(3)(c)].

16. Appellant: The EPC erred in not considering Federal Aviation Administration regulations applicable to the site, under IDO Section 3-3 and other laws, ordinances or regulations.

Staff Response: The proposed Site Plan documents all existing development. Any future proposed development will be required to consider all FAA as well as IDO laws, ordinances or regulations, which includes IDO §14-16-3-3 Airport Protection Overlay (APO) Zone.

17. Appellant: The City Council and its authorized delegees are partial for this application by the City.

Staff Reponse: The EPC is a Council-delegated body charged with reviewing and deciding applications based on criteria established in the IDO. EPC found the Site Plan to meet the review and decision criteria in §14-16-6-6(I)(3) [Finding #19].

CONCLUSION

The EPC acted within its authority and voted to Approve the Site Plan based on Findings #1-24 and Conditions of Approval #1-10. The EPC carefully considered all relevant factors in arriving at its decision based on substantial evidence in the record. The decision is supported by the record.

APPROVED:

/ Mikaela Renz-Whitmore /

Mikaela Renz-Whitmore, Division Manager
Urban Design & Development Division
Planning Department






AC-24-26 transmittal memo_fin

Final Audit Report

2024-09-23

Created:	2024-09-23
By:	Lucinda Montoya (lucindamontoya@cabq.gov)
Status:	Signed
Transaction ID:	CBJCHBCAABAAbJdU8V3_FqJuwFOgsajavIXSaAw-Vo7b

"AC-24-26 transmittal memo_fin" History

-  Document created by Lucinda Montoya (lucindamontoya@cabq.gov)
2024-09-23 - 2:34:36 PM GMT
-  Document emailed to Alan Varela (avarela@cabq.gov) for signature
2024-09-23 - 2:34:41 PM GMT
-  Email viewed by Alan Varela (avarela@cabq.gov)
2024-09-23 - 3:41:42 PM GMT
-  Document e-signed by Alan Varela (avarela@cabq.gov)
Signature Date: 2024-09-23 - 3:41:58 PM GMT - Time Source: server
-  Agreement completed.
2024-09-23 - 3:41:58 PM GMT



Please check the appropriate box and refer to supplemental forms for submittal requirements. All fees must be paid at the time of application.

Administrative Decisions	Decisions Requiring a Public Meeting or Hearing	Policy Decisions
<input type="checkbox"/> Archaeological Certificate (Form P3)	<input type="checkbox"/> Site Plan – EPC including any Variances – EPC (Form P1)	<input type="checkbox"/> Adoption or Amendment of Comprehensive Plan or Facility Plan (Form Z)
<input type="checkbox"/> Historic Certificate of Appropriateness – Minor (Form L)	<input type="checkbox"/> Master Development Plan (Form P1)	<input type="checkbox"/> Adoption or Amendment of Historic Designation (Form L)
<input type="checkbox"/> Alternative Signage Plan (Form P3)	<input type="checkbox"/> Historic Certificate of Appropriateness – Major (Form L)	<input type="checkbox"/> Amendment of IDO Text (Form Z)
<input type="checkbox"/> Minor Amendment to Site Plan (Form P3)	<input type="checkbox"/> Demolition Outside of HPO (Form L)	<input type="checkbox"/> Annexation of Land (Form Z)
<input type="checkbox"/> WTF Approval (Form W1)	<input type="checkbox"/> Historic Design Standards and Guidelines (Form L)	<input type="checkbox"/> Amendment to Zoning Map – EPC (Form Z)
<input type="checkbox"/> Alternative Landscaping Plan (Form P3)	<input type="checkbox"/> Wireless Telecommunications Facility Waiver (Form W2)	<input type="checkbox"/> Amendment to Zoning Map – Council (Form Z)
		Appeals
		<input checked="" type="checkbox"/> Decision by EPC, DHO, LC, ZHE, or City Staff (Form A)

APPLICATION INFORMATION

Applicant: <i>Naeva and all appellants listed on attached list</i>		Phone:
Address: <i>110 Yntema Law Firm P.A. (Hessel E. Yntema III)</i>		Email:
City:	State:	Zip:
Professional/Agent (if any): <i>Hessel E. Yntema III</i>		Phone: <i>505-843-9565</i>
Address: <i>215 Gold Ave SW Suite 201</i>		Email: <i>hessel@yntema-law.com</i>
City: <i>Albuquerque</i>	State: <i>NM</i>	Zip: <i>87102</i>
Proprietary Interest in Site:		List all owners: <i>City of Albuquerque</i>

BRIEF DESCRIPTION OF REQUEST

<i>Appeal of August 15, 2024 EPC decision to approve the Double Eagle II Site Plan</i>
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SITE INFORMATION (Accuracy of the existing legal description is crucial! Attach a separate sheet if necessary.)

Lot or Tract No.: <i>DE II Airport, various tracts</i>	Block:	Unit:
Subdivision/Addition:	MRGCD Map No.:	UPC Code:
Zone Atlas Page(s): <i>VARIOUS</i>	Existing Zoning: <i>NR-SU with site plan</i>	Proposed Zoning: <i>Site Plan - EPC</i>
# of Existing Lots:	# of Proposed Lots:	Total Area of Site (acres): <i>4,200</i>

LOCATION OF PROPERTY BY STREETS

Site Address/Street: <i>DE II Airport</i>	Between: <i>7401 Paseo del Volcan</i> and:
---	--

CASE HISTORY (List any current or prior project and case number(s) that may be relevant to your request.)

<i>PR-2018-001577 and various administrative site plans</i>

Signature: <i>Hessel E. Yntema III</i>	Date: <i>8-29-2024</i>
Printed Name: <i>Hessel E. Yntema III</i>	<input type="checkbox"/> Applicant or <input checked="" type="checkbox"/> Agent

FOR OFFICIAL USE ONLY

Case Numbers	Action	Fees	Case Numbers	Action	Fees
Meeting/Hearing Date:			Fee Total:		
Staff Signature:			Date:		
			Project #		

FORM A: Appeals

Complete applications for appeals will only be accepted within 15 consecutive days, excluding holidays, after the decision being appealed was made.

- ☐ APPEAL OF A DECISION OF CITY PLANNING STAFF (HISTORIC PRESERVATION PLANNER) ON A HISTORIC CERTIFICATE OF APPROPRIATENESS – MINOR TO THE LANDMARKS COMMISSION (LC)
- ☐ APPEAL OF A DECISION OF CITY PLANNING STAFF ON AN IMPACT FEE ASSESSMENT TO THE ENVIRONMENTAL PLANNING COMMISSION (EPC)
- ☒ APPEAL TO CITY COUNCIL THROUGH THE LAND USE HEARING OFFICER (LUHO)

___ Interpreter Needed for Hearing? ___ if yes, indicate language: ___

☒ A Single PDF file of the complete application including all documents being submitted must be emailed to PLNDRS@cabq.gov prior to making a submittal. Zipped files or those over 9 MB cannot be delivered via email, in which case the PDF must be provided on a CD. PDF shall be organized with the Development Review Application and this Form A at the front followed by the remaining documents in the order provided on this form.

☒ Project number of the case being appealed, if applicable: PR-2018-001577

☒ Application number of the case being appealed, if applicable: SI-2024-00994

☒ Type of decision being appealed: Site Plan - EPC

☒ Letter of authorization from the appellant if appeal is submitted by an agent

☒ Appellant's basis of standing in accordance with IDO Section 14-16-6-4(V)(2)

☒ Reason for the appeal identifying the section of the IDO, other City regulation, or condition attached to a decision that has not been interpreted or applied correctly, and further addressing the criteria in IDO Section 14-16-6-4(V)(4)

☒ Copy of the Official Notice of Decision regarding the matter being appealed

I, the applicant or agent, acknowledge that if any required information is not submitted with this application, the application will not be scheduled for a public meeting or hearing, if required, or otherwise processed until it is complete.

Signature: Hessel E. Yntema III

Date: 8-29-2024

Printed Name: Hessel E. Yntema III

☐ Applicant or ☒ Agent

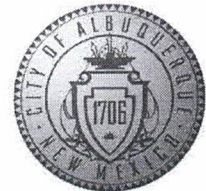
FOR OFFICIAL USE ONLY

Case Numbers:

Project Number:

Staff Signature:

Date:



LIST OF APPELLANTS

1. Naeva, a New Mexico domestic nonprofit corporation
2. Pueblo of Laguna
3. Santa Fe Village Neighborhood Association
4. Westside Coalition of Neighborhood Associations
5. Michael T. Voorhees
6. Jane Baechle

August 27, 2024

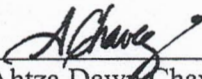
City of Albuquerque Planning Department
600 Second Street NW
Albuquerque, NM 87102

Authorization Letter for Representation for
Appeal of PR-2018-001577, SI-2024-00994, Site
Plan – EPC, Double Eagle II Master Plan

Dear Planning Department:

This letter is to authorize Hessel E. Yntema III, Yntema Law Firm P.A., to represent Naeva (fka Native American Voters Alliance) in the appeal of the referenced approval of a Site Plan – EPC by the EPC on August 15, 2024.

Naeva (fka Native American Voters Alliance)

By 
Ahtza Dawn Chavez, Executive Director

Address:
7906 Menaul NE
Albuquerque, NM 87110



August 29, 2024

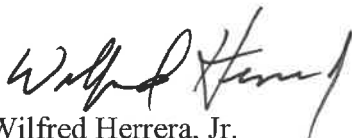
City of Albuquerque Planning Department
600 Second Street NW
Albuquerque, NM 87102

Re: Authorization Letter for Representation for
Appeal of PR-2018-001577, SI-2024-00994,
Site Plan – EPC, Double Eagle II Master
Plan

Dear Planning Department:

This letter is to authorize Hessel E. Yntema III, Yntema Law Firm P.A., to represent the Pueblo of Laguna in an administrative appeal of the referenced approval of a Site Plan – EPC by the EPC on August 15, 2024.

Sincerely,
PUEBLO OF LAGUNA


Wilfred Herrera, Jr.
Governor

8/27/2024

City of Albuquerque Planning Department
600 Second Street NW
Albuquerque, NM 87102

Authorization Letter for Representation for
Appeal of PR-2018-001577, SI-2024-00994, Site
Plan – EPC, Double Eagle II Master Plan

Dear Planning Department:

This letter is to authorize Hessel E. Yntema III, Yntema Law Firm P.A., to represent Westside Coalition of Neighborhood Associations in the appeal of the referenced approval of a Site Plan – EPC by the EPC of August 15, 2024.

WESTSIDE COALITION OF NEIGHBORHOOD
ASSOCIATIONS

By Elizabeth Kay Haley
Elizabeth Kay Haley, Its President

Address:
6005 Chaparral Circle NW
Albuquerque NM, 87114

August 27, 2024

City of Albuquerque Planning Department
600 Second Street NW
Albuquerque, NM 87102

Authorization Letter for Representation for
Appeal of PR-2018-001577, SI-2024-00994, Site
Plan – EPC, Double Eagle II Master Plan

Dear Planning Department:

This letter is to authorize Hessel E. Yntema III, Yntema Law Firm P.A., to represent Santa Fe Village Neighborhood Association in the appeal of the referenced approval of a Site Plan – EPC by the EPC of August 15, 2024.

SANTA FE VILLAGE NEIGHBORHOOD
ASSOCIATION

By 
SFVNA President

Address:
5601 Bogart St NW
ABQ, NM 87120

August 26, 2024

City of Albuquerque Planning Department
600 Second Street NW
Albuquerque, NM 87102

Authorization Letter for Representation for
Appeal of PR-2018-001577, SI-2024-00994,
Site Plan – EPC, Double Eagle II Master Plan

Dear Planning Department:

This letter is to authorize Hessel E. Yntema III, Yntema Law Firm P.A., to represent Michael T. Voorhees in the appeal of the referenced approval of a Site Plan – EPC by the EPC on August 15, 2024.

A handwritten signature in black ink, appearing to read "Michael T. Voorhees", is written over a horizontal line.

MICHAEL T. VOORHEES

Address:
6320 Camino Alto NW
Albuquerque, NM 87120

August 27, 2024

City of Albuquerque Planning Department
600 Second Street NW
Albuquerque, NM 87102

Authorization Letter for Representation for
Appeal of PR-2018-001577, SI-2024-00994, Site
Plan – EPC, Double Eagle II Master Plan

Dear Planning Department:

This letter is to authorize Hessel E. Yntema III, Yntema Law Firm P.A., to represent Jane Baechle in the appeal of the referenced approval of a Site Plan – EPC by the EPC on August 15, 2024.


JANE BAECHLE

Address:
7021 Lamar Ave NW
ABQ, NM 87120

BASIS OF STANDING

All Appellants appeared at the EPC hearing by written submissions or testimony.

Appellant Naeva (fka Native American Voters Alliance) is a New Mexico domestic nonprofit corporation organized to protect the rights and interests of Indigenous people in New Mexico and their communities on and off the reservation. Naeva collaborates with New Mexico's Tribal communities including the 20 Pueblos, the Navajo Nation, Jicarilla Apache Nation, and Mescalero Apache Tribe. In the 1990s, Naeva co-founders helped to lead the fight to protect the Petroglyphs from the Paseo Del Norte expansion. Since then, the defense of the Petroglyph National Monument has been a high priority for Naeva. Members regularly make cultural pilgrimages to the Petroglyphs. There are culturally relevant medicinal plants that grow along the pathways up to the Petroglyphs that could be affected by the air pollution from increased traffic. The EPC decision at issue presents a major threat to the Petroglyph National Monument and the rights of Indigenous people to use of the Petroglyph National Monument and who are specially and adversely affected by the EPC decision.

Appellant Pueblo of Laguna is a federally recognized Indian tribe whose Indigenous members have for centuries used land within what is now the Petroglyph National Monument and surrounding areas for cultural and religious purposes and who are specially and adversely affected by the EPC decision.

Appellant Westside Coalition has standing under IDO Section 6-4(V)(2)(a)(4) because Westside Coalition has legal rights under the IDO to protect neighborhood interests in West Side land use decisions such as building heights, views, protection of West Side MPOS and quality of life issues, and also concerning land use decision process issues. Westside Coalition's membership has individuals who frequently use and enjoy the Westside MPOS. The EPC

decision specially and adversely affects them and Westside Coalition by reducing their enjoyment of the MPOS and by the anticipated loss of use and views in the neighborhood and near the Petroglyph National Monument, and the other factors set out in IDO Section 5-2(A).

Appellant SFVNA has standing under IDO Section 6-4(V)(2)(a)(4) because SFVNA has legal rights under the IDO to protect neighborhood interests in and near its area Westside land use decisions such as building heights, views, protection of Westside MPOS and quality of life issues, and also concerning land use decision process issues. SFVNA's membership has individuals who frequently use and enjoy the Westside MPOS. The EPC decision specially and adversely affects them and SFVNA by reducing their enjoyment of the MPOS and by the anticipated loss of use and views in the neighborhood and near the Petroglyph National Monument, and the other factors set out in IDO Section 5-2(A).

Appellant Voorhees has standing under IDO Section 6-4(U)(2)(a)(4) as a nearby resident living at 6320 Camino Alto NW, approximately 4.2 miles from the subject site, who frequently uses and enjoys MPOS in the area, including Petroglyph National Monument. The decision specially and adversely affects him by potentially reducing his enjoyment of the MPOS by degrading the character of the cultural landscape and scenic viewscape near Petroglyph National Monument, and the other factors set out in IDO Section 5-2(A). Mr. Voorhees is also an FAA Certificated Commercial Pilot and has an interest in the development and character of the Federally managed Air Space and related facilities which could be specially and adversely affected by the EPC decision, as well as a continuing interest that the City of Albuquerque, as the owner and operator of an airport regulated by the Federal Aviation Administration, complies with all relevant Federal Laws, regulations, and lawful Orders, including the National Environmental Policy Act and FAA Order 5050.4b.

Appellant Baechle has standing under IDO Section 6-4(U)(2)(a)(4) as a nearby resident living at 7021 Lamar Ave NW, abutting the escarpment and eastern boundary of the Petroglyph National Monument which in its entirety represents a significant cultural landscape with material cultural and economic benefits. She frequently hikes the Petroglyph National Monument including from designated crossings to the mesa top with both views and trail access to the volcanoes. The decision specially and adversely affects her by potentially reducing her enjoyment of the MPOS by degrading the character of the cultural landscape and scenic viewscape near Petroglyph National Monument, and the other factors set out in IDO Section 5-2(A). Additionally, Ms. Baechle has been a Petroglyph National Monument VIP (Volunteers in Parks) for over five years, serving both Visitor Services and Trail Watch, educating visitors from all over the world about the geologic and cultural significance of the Petroglyph National Monument and the surrounding area and has an interest in the protection of the entire Monument and all of its resources.

REASONS FOR THE APPEAL

Under IDO Section 6-4(V)(4), the criteria for review for this appeal shall be whether the Environmental Planning Commission (“EPC”) made 1 of the following mistakes:

- (a) the EPC acted fraudulently, arbitrarily or capriciously.
- (b) the EPC decision is not supported by substantial evidence.
- (c) the EPC erred in applying the requirements of the IDO (or a plan, policy or regulation referenced in the applicable review and decision-making criteria).

The EPC made the following mistakes:

1. The EPC lacks authority to approve an “as-built” Site Plan with design features, including for very large and high buildings, for 4,200 acres covered by a Master Plan recently amended.
2. The approval is premature when the Master Plan for Double Eagle II is available for amendment by the City. The new Site Plan appears to be in lieu of a properly processed amendment to the Double Eagle II Master Plan. The new Site Plan of 4,200 acres is enormous for a “Site Plan” and appears to be coextensive with the Master Plan.
3. The approved “as-built” Site Plan is not in conformance with the Double Eagle II Master Plan and related approved site plans.
4. The subject site apparently has multiple existing approved Site Plans which should be reviewed and considered in connection with any “as built” or new site plan.
5. The subject site apparently is not platted into a single property.
6. It appears that the decision does not comply the requirements of IDO Section 5-2(J) concerning Major Public Open Space (“MPOS”).

7. The applicant did not comply with the Tribal Meeting requirements of IDO Section 6-4(B). The rushed timing and circumstances of the City's application suggest intent to avoid the meeting requirements of IDO Section 6-4(B).

8. The Comprehensive Plan analyses of the decision are in error. The decision is not consistent with the Comprehensive Plan.

9. EPC Finding 19A is in error because the new Site Plan is not consistent with the Comprehensive Plan.

10. EPC Finding 19B is in error because some provisions of the new Site Plan conflict with existing plans.

11. EPC Finding 19C is in error for various reasons.

12. EPC Finding 19D is in error because approval of an "as built" Site Plan, if that is possible, is not the place to approve proposed new future development.

13. EPC Finding 19E is in error because the Site Plan does not mitigate any significant adverse impacts, including buildings of excessive size and height, to the surrounding area particularly the Petroglyph National Monument.

14. The City lacks vested rights in any future development of the site except as controlled by the IDO and the Master Plan.

15. The new Site Plan may lack all information required for the type of application, structure, or development.

16. The EPC erred in not considering Federal Aviation Administration regulations applicable to the site, under IDO Section 3-3 and other laws, ordinances or regulations.

17. The City Council and its authorized delegees are partial for this application by the City.

Appellants do not have the record of the EPC proceedings at this time and reserve the right to amend or supplement their Reasons for Appeal after review of the record.

PLANNING DEPARTMENT
URBAN DESIGN & DEVELOPMENT DIVISION
600 2nd Street NW, 3rd Floor, Albuquerque, NM 87102
P.O. Box 1293, Albuquerque, NM 87103
Office (505) 924-3860 Fax (505) 924-3339



OFFICIAL NOTIFICATION OF DECISION

August 15, 2024

City of Albuquerque
Aviation Department
2200 Sunport SE
Albuquerque, NM 87120

Project # PR-2018-001577
SI-2024-00994 – Site Plan – EPC

Dekker, agent for the City of Albuquerque Aviation Dept., requests an NR-SU Site Plan – EPC for the DEII Airport for all or a portion of Tract A-1, Plat of Tract A-1 & Tract L-1 Parcels 1-5, Double Eagle II Airport; Tract C and Parcel-1 Tract M Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; the Southeasterly Portion of Tract D-1-A-1, Tract D-1-A-2, and Tract S-1-A Plat of Tracts D-1-A-1, D-1-A-2, & S-1-A of Amended Bulk Land Plat for Aerospace Technology Park; Tract E, F, and K Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; Tract N-1 and Tract N-2 AKA Parcel 4 Bulk Land Plat of Tracts N-1, O-1, & N-2 Parcels 1, 2, 3, & 4 Double Eagle II Airport & Adjacent Lands; and Tract S-2 Bulk Land Plat for Aerospace Technology Park, Tracts D & S of Double Eagle II Airport & Adjacent Lands, located at 7401 Paseo Del Volcan NW, NW of the Atrisco Vista Blvd NW and Shooting Range Access Rd. intersection, approximately 4,200 acres.

(C-4, C-5, C-6, D-4, D-5, D-6, E-4, E-5, E-6, F-4, F-5, F-6, G-4, G-5, G-6)

Staff Planners: Catherine Heyne, Megan Jones

On August 15, 2024, the Environmental Planning Commission (EPC) voted to APPROVE PR-2018-001577 SI-2024-00994 – Site Plan – EPC, based on the following findings and subject to the following conditions of approval:

FINDINGS – SI-2024-00994 – Site Plan – EPC

1. This request is for a Site Plan – EPC for the Double Eagle II (“DEII”) Airport legally described as all or a portion of Tract A-1, Plat of Tract A-1 & Tract L-1, Parcels 1-5, Double Eagle II Airport; Tract C and Parcel-1 Tract M Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; the Southeasterly Portion of Tract D-1-A-1, Tract D-1-A-2, and Tract S-1-A Plat of Tracts D-1-A-1, D-1-A-2, & S-1-A of Amended Bulk Land Plat for Aerospace Technology Park; Tract E, F, and K Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; Tract N- 1 and Tract N-2 AKA Parcel 4 Bulk Land Plat of Tracts N-1, O-1, & N-2 Parcels 1, 2, 3, & 4 Double Eagle II Airport & Adjacent Lands; and Tract S-2 Bulk Land Plat for Aerospace Technology Park, Tracts D & S of Double Eagle

II Airport & Adjacent Lands, located at 7401 Paseo Del Volcan NW, NW of the Atrisco Vista Blvd NW and Shooting Range Access Rd intersection, containing approximately 4,200 acres (C-4, C-5, C-6, D-4, D-5, D-6, E-4, E-5, E-6, F-5, F-6, G-4, G-5, G-6).

2. The DEII Airport (the “subject site”) sits to the northwest of the Atrisco Vista Blvd. NW and Shooting Range Access Rd. intersection. This property is bounded by Major Public Open Space (MPOS), the City of Rio Rancho, and unincorporated Bernalillo County land and within a ¼ mile west of the Petroglyph National Monument. The subject site is about 8.5 miles northwest of Downtown Albuquerque (around 18 miles driving).
3. The subject site is zoned NR-SU and is within the DEII Employment Center, an Area of Change (most undeveloped land), and an Area of Consistency (airport areas) as designated by the Comprehensive Plan. Major Public Open Space (MPOS) is adjacent to DEII on its eastern and western boundaries with eastern portions within 330 ft of the Petroglyph National Monument.
4. The subject site is governed by the 2019 Double Eagle II Airport Master Plan. Master Plans and land uses are voluntary for the facility, but the Master Plan and Site Plan are meant to be complementary. As it stands, the Master Plan only includes an “Airport Layout Drawing.” There have been several Site Plans for the airport which have been approved administratively, but they are not consist with the IDO nor the NR-SU zone district.
5. The request documents the existing, as-built airport facilities and defines the associated NR-SU Design Standards required for the DEII Airport under the NR-SU zone district. The applicant is required to document the existing development in an “as-built” Site Plan to establish a baseline for any proposed future development and determinations for Design Standards. The NR-SU Site Plan establishes specific uses, site standards, and regulations for the Airport subject site, which are to be negotiated at the EPC. A site plan is required for any development to occur under the NR-SU zone district for the Airport Land Use.
6. The As-Built site plan is in compliance with the associated Master Plan for DEII (PR #2018-001577_SI-2024-00321).
7. The EPC is hearing this request (SI-2024-00994) pursuant to IDO §14-16-2-5(E) Non-Residential – Sensitive Use (NR-SU) zone district and IDO §14-16-6-6(I) Site Plan – EPC. The NR-SU zone district requires that a Site Plan is reviewed and decided by the EPC. The existing, as-built site conditions depicted in the proposed Site Plan shall remain valid until they are replaced (see IDO §14-16-1-10), which will require that any new development be in compliance with the IDO. The EPC shall review the uses, standards, and regulations as negotiated by the applicant.
8. The Albuquerque/Bernalillo County Comprehensive Plan and the City of Albuquerque Integrated Development Ordinance (IDO) are incorporated herein by reference and made part of the record for all purposes.
9. IDO §14-16-6-6(I)(3)(c) states, “The Site Plan complies with all applicable provisions of this IDO, the DPM, other adopted City regulations, and any terms and conditions specifically applied to development of the property in a prior permit or approval affecting the property.”

The proposed As-Built Site Plan has been evaluated for conformance with applicable goals and policies in the Comprehensive Plan and other applicable Plans. The approximately 4,200-acre site is being submitted as an “As-Built” Site Plan because the subject site area was developed pre-IDO and does not have an existing Site Plan on file with the City. Current uses and standards shall be allowed pursuant to IDO §14-16-1-10(A) Pre-IDO Approvals and IDO §14-16-4-1(E) Previously Allowed Uses.

Future development and amendments for the subject site would be required to follow all design standards set forth in the Site Plan for the DEII Airport and where silent, applicable IDO provisions, Site Plan and IDO Design Standards for the NR-SU zone district pursuant to IDO §14-16-2-5(E), as well as with all applicable provisions of the DPM, other adopted City regulations, and any other terms and conditions specific to the subject site. Anywhere the Design Standards are silent, the IDO regulations shall apply. Existing development is allowed to continue operating, even if nonconforming, pursuant to IDO §14-16-1-10(A) Pre-IDO Approvals and IDO §14-16-4-1(E) Previously Allowed Uses.

The 300-acre portion of the site governed by the Eclipse Aviation Campus Site Plan for Subdivision (PR 1003125 & 06DRB-00401) and the Aerospace Technology Park Site Development Plan for Subdivision (PR 1003125 03EPC-02054 & 05EPC-00566) is not a part of the request (Tract F Bulk Land Plat of Double Eagle II Airport and Adjacent Lands; Tract D-1-A-1, Tract S-1-A, and the Southwesterly Portion of Tract D-1-A-1, Plat of Tracts D-1-A-1, D-1-A-2 & S-1-A of amended bulk land plat for Aerospace Technology Park; Tract D-1-A-2 and the Southeasterly portion of Tract D-1-A-1, Plat of Tracts D-1-A-1, D-1-A-2 & S-1-A of amended bulk land plat for Aerospace Technology Park; and Lots 1-15 Bulk Land Plat of lots 1 - 15 Aerospace Technology Park, Unit 1).

10. The request is generally consistent with GOAL 4.2 Process: Engage communities to identify and plan for their distinct character and needs from Comprehensive Plan Chapter 4: Community Identity:

Although not required by IDO regulations, City Aviation reached out to Neighborhood Associations/Coalitions and other interested parties for a Public Open House held June 28, 2024 to engage communities and help identify and plan for the distinct character and needs of the DEII Airport Facilities. Solicited feedback regarding the proposed as-built Site Plan and Design Standards contributed to proposed Design Standards revisions.

11. The request is partly consistent with the following Policies from Comprehensive Plan Chapter 4: Community Identity:

- A. POLICY 4.1.5 NATURAL RESOURCES: Encourage high-quality development and redevelopment that responds appropriately to the natural setting and ecosystem functions.

This request is for an as-built Site Plan that also documents proposed Design Standards. The site is surrounded to the east and west by MPOS that includes the Petroglyph National Monument. As the Applicant stated, the “DEII Design Standards are integral to establishing an aesthetic character compatible with the surrounding context, particularly the City of Albuquerque Open Space and Petroglyph National Monument.”

The subject site is located in a generally undeveloped, desert grassland with expansive views to the mountains to the north (Sangre de Cristos), east (Petroglyph National Monument volcanoes,

Sandias), southeast (Manzanos), and west (Mt. Taylor). NR-SU zone district Design Standards may be varied in the NR-SU approval process (see IDO Table 2-5-9: Other Applicable IDO Sections) with Dimensional Standards determined in the approval of a Site Plan – EPC (see IDO §14-16-5-1(E)(2)(b)). Proposed DEII Airport Design Standards generally conform to IDO design standards (e.g., Access and Connectivity, Subdivision of Land, Parking and Loading, Solar access, and Signs) that work to help reduce development impact on Major Public Open Space (MPOS), such as suggesting a color palette and light reflective values (LRV) recommended for Sensitive Lands (see IDO §14-16-5-2) for buildings within 250 feet of MPOS. It also incorporates active and passive water harvesting, passive heating and cooling structure design, permeable pavements, less linear landscape lines, and using vegetation to help slow and filter runoff. However, any significant development to the scale proposed will increase light, noise, traffic, pollution, temperatures, and habitat fragmentation as well as impact natural drainage and views to culturally important landscape features to and from the Petroglyphs National Monument and Major Public Open Space.

Greater allowances for building setbacks (150 feet from MPOS) and requiring screened parking areas, should help lower development impact in terms of views from MPOS to the subject site, however, staff disagrees that the proposed building height maximums of 60 feet within a 150-foot setback from Atrisco Vista Blvd and 85 foot maximum for all other areas respond appropriately to the natural setting despite the addition of sustainable caveats such as orienting building to overlook MPOS in combination with passive and cooling. The proposed heights seem out of scale with area open space features and appears to be designed to compete with the volcanos themselves.

Adding trees to the Atrisco Vista Blvd ROW also seems out of character with the natural setting and ecosystem functions.

- B. POLICY 4.2.2 COMMUNITY ENGAGEMENT: Facilitate meaningful engagement opportunities and respectful interactions in order to identify and address the needs of all residents.

According to the Office of Neighborhood Coordination (ONC), there are no designated Neighborhood Associations within the required notification buffer areas; however, after the general required notification of property owners within 100 feet of the proposed Site Plan – EPC request, City Aviation reached out to Neighborhood Associations/Coalitions and other interested parties for a Public Open House held June 28, 2024. At this time, City Aviation provided information to solicit feedback regarding the proposed as-built Site Plan and Design Standards being brought before the EPC. Attendance included representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation, tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature (see Attachments). Comments collected during the meeting resulted in some Design Standards revisions. Updates of project description and were also resent to adjacent property owners and emailed to neighboring Neighborhood Associations/Coalitions.

However, the fact that this request is less than 330 ft from the Petroglyph National Monument, which is widely known as a significant sacred site for most of the area's Pueblos and Tribes, combined with the fact that only Pueblo of Laguna attended the Open House, this one Open

House would not be consistent with meaningful engagement and respectful interactions to identify and address the needs of all residents.

12. The request is consistent with the following Goals and Policies regarding Centers, Corridors and Development areas from Comprehensive Plan Chapter 5: Land Use:

- A. **GOAL 5.1 CENTERS AND CORRIDORS:** Grow as a community of strong centers connected by a multi-modal network of Corridors.

The subject site is located within a Developing Employment Center along the Regional Principal Arterial and Commuter Corridor, Atrisco Vista Blvd NW that prioritizes automobile travel in street design and improvements by allowing higher traffic speed and volume, connecting to I-40 about 4.5 miles to the south. It also is surrounded to the east and west by Major Public Open Space and includes the Petroglyph National Monument within 330 feet. The request codifies an as-built DEII Airport Site Plan and Design Standards to allow for future site development at the subject site. Adding potential employment sites to the West Side of the City would allow for the addition of jobs and employment opportunities west of the Rio Grande and could help improve the job-housing balance on that side of town.

- B. **POLICY 5.1.1 DESIRED GROWTH:** Capture regional growth in Centers and Corridors to help shape the built environment into a sustainable development pattern.

The subject site is located within a Developing Employment Center and along a Regional Principal Arterial/ Commuter Corridor of Atrisco Vista Blvd NW. Allowing growth within a Developing Employment Center promotes a more sustainable development patterns and could reduce the job-housing imbalance west of the river as articulated in the ABC Comp Plan, especially as the City's West Side continues to sprawl.

- C. **POLICY 5.1.2: DEVELOPMENT AREAS:** Direct more intense growth to Centers and Corridors and use Development Areas to establish and maintain appropriate density and scale of development within areas that should be more stable.

The subject site is located mostly within an ABC Comp Plan designated Area of Change, with the Areas of Consistency consisting of integral DEII Airport functions. The request will result in an as-built Site Plan documenting existing airport facilities and codifying Design Standards which will help guide growth and establish development density and scale for an area intended for higher intensity uses.

- D. **POLICY 5.1.5 EMPLOYMENT CENTERS:** Create Centers that prioritize employment and foster synergy among businesses.

The subject site is located within a Developing Employment Center and along a Regional Principal Arterial/ Commuter Corridor of Atrisco Vista Blvd NW. Allowing growth within a Developing Employment Center helps prioritize employment and foster synergy among businesses in an area with good automobile access and good connectivity for freight routes. The proposed DEII design standards also consider pedestrian and bicycle access within the Developing Employment Center.

- E. **GOAL 5.3 EFFICIENT DEVELOPMENT PATTERNS:** Promote development patterns that maximize the utility of existing infrastructure and public facilities and the efficient use of land to support the public good.

The proposed request promotes internal development patterns that maximize the utility of existing infrastructure and public facilities by documenting as-built DEII Airport structures and associated facilities, as well as compiling Design Standards for future proposed development at the subject site within a Developing Employment Center to better maintain a more cohesive future development. As this request does not propose any development, there will be no changes to the subject site layout, which preserves current efficiencies in land use to support the public good.

- F. **POLICY 5.7.2 REGULATORY ALIGNMENT:** Update regulatory frameworks to support desired growth, high quality development, economic development, housing, a variety of transportation modes, and quality of life priorities.

There is no DEII Airport Site Plan or Design Standards relative to the NR-SU zone district that is associated with the current Airport Master Plan. The proposed subject site Design Standards should provide a clearer regulatory framework and processes for any proposed future development to better support desired growth and both high quality employment and economic development.

13. The request is generally consistent with **POLICY 5.6.2 AREAS OF CHANGE:** Direct growth and more intense development to Centers, Corridors, industrial and business parks, and Metropolitan Redevelopment Areas where change is encouraged, from Comprehensive Plan Chapter 5: Land Use:

The proposed Design Standards being submitted as part of this request for an as-built Site Plan outlines potential future growth and more intense development in Areas of Change within a Developing Employment Center. This could encourage development that expands employment opportunities and job creation within the facility boundaries. At this stage of development of the west side of the Petroglyph National Monument, there will be minimal impacts of development on existing residential uses regarding noise, stormwater runoff, contaminants, lighting, air quality, and traffic. However, with the planned growth development impacts in general will increase in what are currently undeveloped natural habitats.

14. The request is partly consistent with **GOAL 5.6 CITY DEVELOPMENT AREAS:** Encourage and direct growth to Areas of Change where it is expected and desired and ensure that development in and near Areas of Consistency reinforces the character and intensity of the surrounding area, from Comprehensive Plan Chapter 5: Land Use:

Although the subject site is already developed as a Regional General Aviation airport—a reliever for the Sunport—that primarily focuses on business activity, lease areas within the airport tracts lie an Area of Change where development is expected and desired. The proposed Design Standards encourage and direct growth to Areas of Change where it is expected and desired and generally ensure that development in and near Areas of Consistency reinforces the character and intensity of the surrounding area. Proposed building height maximums and proposed trees along public ROW however, would change the character and intensity of land use considerably.

15. The request is not consistent with **POLICY 5.3.4 CONSERVATION DEVELOPMENT**: Encourage conservation development to promote private open space and preserve natural landscape, agricultural lands, and other features of the natural environment to encourage development that is sensitive to the open, natural character of the area and the geological and cultural conditions, from Comprehensive Plan Chapter 5: Land Use:

Staff agrees with the Applicant that given the large area of DEII and the need to prioritize airport operations, the DEII Design Standards should concentrate new development within a prescribed area and preserve the surrounding natural landscape which consists of a wide-open landscape with expansive views in all directions.

This request proposes landscaping, screening, and buffering standards, especially along parking lots, in areas with necessary but “less attractive” development, such as, loading, service, mechanical equipment, parking areas, and refuse facilities which should minimize visual impact from MPOS and Atrisco Vista Blvd. The suggested continuous line of 3-to-4-foot evergreen shrubbery with a minimum of 6-foot-wide buffer could harmonize with available native species (e.g., *Atriplex* sp) and preserve the natural landscape and local views; however, a dark green evergreen, such as decorative juniper will be out of place and inappropriate for the surrounding landscape. The addition of trees in landscape that emphasizes visitor and employee centric spaces and walkways can potentially be appropriate in certain areas, but not e along Atrisco Vista Blvd, especially on the east side since it will potentially change the character of the landscape.

Because the addition of fencing impacts view distances, view fencing should retain the feeling of Open Space and the viewshed, while helping maintain the built aesthetic. Proposed color schemes and light reflectivity standards that echo the surrounding natural environment shall reduce the visual impacts of new development. Again, the subject site is located in a generally flat, gradually sloped and undeveloped, desert grassland with expansive views to the north (Sangre de Cristos), east (Petroglyph National Monument volcanoes, Sandias), southeast (Manzanos), and west (Mt. Taylor), and greater building setback allowances should help lower development impact in terms of views from MPOS to the subject site as this will bar structures and buildings at the property lines. Staff disagrees that the proposed building height maximums of 60 feet within a 150-foot setback from Atrisco Vista Blvd and 85 foot maximum for all other areas responds appropriately to the natural setting. Nevertheless, proposed Design Standards encourage tenants to orient buildings to allow for views, especially of the Petroglyph National Monument. Any significant development to the scale proposed will increase light, noise, traffic, temperature, and habitat fragmentation as well as impact views to culturally important landscape features.

16. The request is partly consistent with the following Policies regarding Centers, Corridors and Development areas from Comprehensive Plan Chapter 7: Urban Design:
- A. **POLICY 7.3.1 NATURAL AND CULTURAL FEATURES**: Preserve, enhance, and leverage natural features and views of cultural landscapes, from Comprehensive Plan Chapter 7: Urban Design:

The proposed site designs address development issues to preserve and leverage natural features and views of cultural landscapes with development setback provisions, specifying appropriate color choices and building materials to blend in with the natural landscape, adding low landscape buffers and appropriate screening to less aesthetically pleasing development, requiring more

subdued and consolidated signage, burying all electric distribution lines, and eliminating light trespass beyond the property line or allowing light to be visible from MPOS except where FAA regulations may vary.

However, staff disagrees that the proposed Design Standards with building height maximums of 60 feet within a 150-foot setback from Atrisco Vista Blvd and an 85-foot maximum for all other areas preserve, enhance, and leverage natural features. Such heights will compete with the elevations of the nearby volcanos and would probably be visible from the City's east side, similar to the current warehouse fulfillment center to the south of the DEII Airport just outside of the City. Such heights would also be the first impression on entry from the west into the City on the highway and would be the features versus leveraging the neighboring volcano features.

Added trees to public ROWs, especially along Atrisco Vista Blvd would not maintain the natural conditions of the natural landscapes within the DEII parcels and would not provide an appropriate transition to adjoining Major Public Open Space.

- B. POLICY 7.5.1 LANDSCAPE DESIGN: Encourage landscape treatments that are consistent with the high desert climate to enhance our sense of place.

Some of the proposed landscape treatments are consistent with the high desert climate. For example, where not in conflict with FAA safety regulations or aviation functionality, the DEII Design Standards encourage active and passive water harvesting techniques to slow and capture stormwater on site as well as to supplement watered landscaped areas as natural drainage is modified.

Although the Design Standards encourage the use of native plant materials that enhance our sense of place, regionally adapted plants that will thrive in local conditions with less fertilizer, water, and maintenance are also proposed, the latter of which do not match adjacent landscapes and could be invasive.

The addition of trees along the public ROW would not be consistent with the microclimate of the site location or the dramatic setting of the West Mesa, but if restricted to visitor and employee centric spaces to create moderated microclimates, trees could help allow for the high desert climate in adjoining spaces, and with that, our sense of place.

17. The request is not consistent with GOAL 7.5 CONTEXT-SENSITIVE SITE DESIGN: Design sites, buildings, and landscape elements to respond to the high desert environment, from Comprehensive Plan Chapter 7 Urban Design:

Staff agrees that the natural terrain and open landscapes of arid grassland of the DEII Airport and surrounding West Mesa offers spectacular open views in all directions from the Petroglyph National Monument Volcanos to distant mountains (e.g., Sangre de Cristos, Sandias, Manzanos, Mt Taylor). Because of this, it is notable that the proposed DEII Design Standards prioritizes maintaining a natural landscape wherever possible. The fact that the proposed Design Standards center sustainable practices, including Green Stormwater Infrastructure (GSI) and Low Impact Development (LID) practices for site design, buildings, and landscape elements to respond to the high desert environment are also important. For the most part, sustainable, context-sensitive Site Design has been proposed. However, Staff contends that the proposed building heights and tree-lined ROWs

will negatively impact views to existing landscape elements. Buildings reaching maximum heights of 60 or 85 feet, although in effect represent denser development, will not preserve the open vistas. Within recent history, trees do not naturally contribute to the area's native vegetation and will block views to the natural high desert environment.

18. The request is not consistent with the following Policies from Comprehensive Plan Chapter 11 Heritage Conservation:

A. **GOAL 11.3 CULTURAL LANDSCAPES:** Protect, reuse, and/or enhance significant cultural landscapes as important contributors to our heritage and rich and complex identities.

The proposed DEII Airport Design Standards aim to protect the significant cultural landscapes that surround the airport. These critical landscapes include Major Public Open Space and the striking volcano cones and cultural remains protected by the Petroglyph National Monument, all of which contribute to the area's heritage as well as our rich and complex identities. Accordingly, all future development shall be pursuant to Archaeological Sites regulations of the IDO Site Design and Sensitive Lands (see §14-16-5-2(D)(1)).

Although the proposed DEII Design Standards take into consideration these remarkable westside viewsheds and exceed IDO standards, where feasible, additional measures can be taken to minimize the impact of development, such as:

- 1) Reduce building heights,
- 2) Increase building density,
- 3) Constrain development to only the west side of Atrisco Vista,
- 4) Eliminate the ROW tree requirement along Atrisco Vista and most ROW area; plant trees only in areas with dense development and regular outdoor use or to minimize heat as part of hardscapes,
- 5) Encourage shade structures,
- 6) Require shading over parking,
- 7) Require solar hookups, and
- 8) When developing land that has not already been bladed, remove plants prior to ground breaking to reuse in landscaping.

Proposed protective DEII Design Standards include minimizing the visibility of structures through color and building material restrictions, minimum building and parking setbacks, adding screening, wall, and fencing requirements. Additionally, building setbacks have been added for MPOS and Atrisco Vista Blvd that are greater than those required by the IDO to minimize impacts to MPOS and the Petroglyph National Monument.

B. **POLICY 11.3.4 PETROGLYPH NATIONAL MONUMENT:** Regulate adjacent development to protect and preserve the Petroglyph National Monument – its volcanoes, petroglyphs, and Northwest Mesa Escarpment – as a priceless cultural landscape and community resource that provides physical, cultural, and economic benefits.

The West Side is expanding, and the DEII Airport is within a designated Employment Center. The proposed DEII Design Standards attempt to protect the Monument from growth and development pressures by concentrating leased land development primarily to the southwest

corner of the property. Additional protective DEII Design Standards include minimizing the visibility of structures through color and building material restrictions, minimum building and parking setbacks, adding screening, wall, and fencing requirements. Additionally, building setbacks have been added for MPOS and Atrisco Vista Blvd that are greater than those required by the IDO to minimize impacts to MPOS and the Petroglyph National Monument.

However, the proposed structure height maximums of 60 feet between 150 and 250 feet from the Atrisco Vista ROW and up to 85 feet in all other areas (provided that they comply with the FFA FAR Part 7 regulations) are incongruent with the area's open grasslands and the Petroglyph National Monument. With these building heights, views to and from those spaces would not be preserved. Trees added to the Atrisco Vista Blvd ROW would also change the character of the landscape and not preserve the natural open spaces that characterize the area. Instead of the volcanos, any future proposed development would act as a visual reference point, rendering edge treatments and transitions as inappropriate.

19. The request meets the Site Plan – EPC Review & Decision Criteria in IDO §14-16-6-6(I)(3) as follows:

- A. 6-6(I)(3)(a) The site plan is consistent with the ABC Comp Plan, as amended. As demonstrated by the policy-based analysis of the proposed Site Plan, the request is generally consistent with applicable Comprehensive Plan Goals and policies.
- B. 6-6(I)(3)(b) The Site Plan is consistent with any applicable terms and conditions in any previously approved NR-SU or PD zoning covering the subject property and any related development agreements and/or regulations.

The subject site is zoned NR-SU. There is not an NR-SU Site Plan on file for the DEII Airport Property in its entirety. There is a Site Development Plan for Subdivision with Design standards that is valid for a 300-acre portion of the airport property which includes the Site Plan for Eclipse Aviation. This SDP for Subdivision governs these specific tracts within the Airport, which shall be noted on the NR-SU site plan: Tract F Bulk Land Plat of Double Eagle II Airport and Adjacent Lands; Tract D-1-A-1, Tract S-1-A, and the SW'LY Portion of Tract D-1-A-1, Plat of Tracts D-1-A- 1, D-1-A-2 &S-1-A of amended bulk land plat for Aerospace Technology Park; Tract D-1-A-2 and the Southeasterly portion of Tract D-1-A-1, Plat of Tracts D-1-A-1, D- 1-A-2 & S-1-A of amended bulk land plat for Aerospace Technology Park; and LTS 1-15 Bulk Land Plat of lots 1 - 15 Aerospace Technology Park, Unit 1. No other related development agreements and/or regulations are known to be in place for the project site.

- C. 6-6(I)(3)(c) The Site Plan complies with all applicable provisions of this IDO, the DPM, other adopted City regulations, and any terms and conditions specifically applied to development of the property in a prior permit or approval affecting the property.

Staff has reviewed the proposed as-built Site Plan for documentation purposes. The existing development for the airport use at the subject site was established prior to the adoption of the 2018 IDO; therefore, current, as-built development will be held to Pre-IDO Approvals pursuant to IDO §14-16-1-10 and IDO §14-16-4-1(E). Current land uses, buildings, lots, signs, and site features may become nonconforming due to the adoption of the IDO and will be pursuant to IDO §14-16-6-8 Nonconformities.

The subject site lies adjacent to Major Public Open Space (MPOS) and is within 330 feet of the Petroglyph National Monument. Under current IDO regulations, this location requires compliance to IDO §14-16-5-2 Site Design and Sensitive Lands, which prompts a Cumulative Impact Analysis review for any future development. MPOS adjacency also necessitates an analysis of MPOS Edges requirements (see IDO §14-16-5-2(J)). This has been completed as part of the staff report.

Although there is not any proposed new development at this time, any future proposed development will be required to comply with all applicable provisions and Design Standards for the NR-SU zone district pursuant to IDO 14-16-2-5(E) as well with all applicable provisions of the IDO, the DPM, other adopted City regulations, and any other terms and conditions specific to the subject site.

- D. 6-6(I)(3)(d) The City's existing infrastructure and public improvements, including but not limited to its street, trail, drainage, and sidewalk systems, have adequate capacity to serve the proposed development, and any burdens on those systems have been mitigated to the maximum extent practicable.

The City's existing infrastructure and public improvements should have adequate capacity to continue to serve the existing development. At this time, there is no proposed changes are being proposed to the existing use, access points, circulation, parking, or traffic patterns, noise, or hazardous material use; this application is to document the as-built physical environment at the DEII Airport.

- E. 6-6(I)(3)(e) The application mitigates any significant adverse impacts on the project site and the surrounding area to the maximum extent practicable.

The current manufacturing plant is a development pre-existing to the 2018 IDO and does not have a controlling Site Plan. Current uses shall be allowed pursuant to IDO §14-16-4-1(E) Previously Allowed Uses. The requested as-built Site Plan for the subject site establishes a baseline for allowable uses, design standards, and other development regulations for current and any future development.

- F. 6-6(I)(3)(f) If the subject property is within an approved Master Development Plan, the Site Plan meets any relevant standards in the Master Development Plan in addition to any standards applicable in the zone district the subject property is in.

The subject site is not within an approved Master Development Plan area.

- G. 6-6(I)(3)(g) If a cumulative impact analysis is required in the Railroad and Spur Small Area pursuant to §14-16-5-2(E) (Cumulative Impacts) and §14-16-6-4(H) (Cumulative Impacts Analysis Requirements), the Site Plan incorporates mitigation for all identified cumulative impacts. The proposed development will not create material adverse impacts on water quality or other land in the surrounding area through increases in traffic congestion, parking congestion, noise, vibration, light spillover, or other nuisances without sufficient mitigation or civic or environmental benefits that outweigh the expected impacts.

The request is for an as-built Site Plan under the NR-SU zone district for the DEII Airport subject site, which establishes a baseline for allowable uses, design standards, and other development regulations for current and any future development.

20. A member of the Santa Fe Village Neighborhood Association (SFVNA) wrote a letter of opposition dated August 5, 2024. The concerned citizen is opposed to the proposed 85-foot building height standard outlined in the Design Standards for the DEII Site Plan.
21. The National Park Service (NPS), Petroglyph National Monument, submitted a letter to the Planning Department on August 5, 2024 expressing concerns about the proposed building heights included in the Design Standards. They state that they “cannot support building heights as currently included in the design guidelines and request the City of Albuquerque Aviation Department reconsider and institute a maximum building height of 36 feet in order to protect the values of the West Mesa and Petroglyph National Monument.”
22. Solar power or other forms of renewable energy shall be encouraged for all new development.
23. Existing wall-mounted and free-standing signs may not be in compliance with current IDO regulations. If nonconforming, all future changes shall comply with IDO regulations pursuant to IDO §14-16-1-10(A) pre-IDO Approvals, §14-16-6-4(Z) Amendments of Pre-IDO Approvals, and §14-16-6-8(F) Nonconforming Signs, and any new development will comply with IDO and/or Double Eagle II design standards.
24. For future site plan applications, the Environmental Planning Commission expressed a desire to review profiles showing building height impacts as viewed from the Petroglyph National Monument.

Conditions of Approval

Project #: 2018-001577 / Case #: 2024-00994 - Site Plan – EPC

1. The applicant shall coordinate with the staff planner to ensure that all Conditions of Approval are met and then submit a vetted, final version to the staff planner for filing at the Planning Department.
2. After approval by the Environmental Planning Commission (EPC), the applicant shall submit the site plan that meets conditions of approval to the Staff Planner or relevant Urban Design and Development Staff for final sign-off. The reviewer will be responsible for ensuring that all EPC Conditions have been satisfied and that the IDO, DPM, and all other applicable City requirements have been met.
3. Pursuant to IDO §14-16-6-6(I)(2)(m) Site Plans shall be reviewed administratively for compliance with conditions of approval, DPM, and zoning standards prior to the issuance of a building permit.
4. Pursuant to IDO §14-16-6-4(P)(4) any conditions shall be met within 1 year of the approval, unless stated otherwise in the approval. If any conditions are not met within that time, the approval is void. The Planning Director may extend the time limit up to an additional 1 year.
5. Site Plan:
 - A. G5 shall be added to the Zone Atlas description for all pages (e.g., SDP1.2).
 - B. Zoning descriptions shall be indicated for each parcel outside of the DEII boundary on Sheet SDP1.1.

- C. A note shall be added to explain the Obstacle Free Zone on Sheets SDP1.2 and 2.1.
- D. A keyed note shall be added for the diagonal (NW-SE) solid line running from the area with buildings Q1-4 and road on Sheet SDP1.2.
- E. “Building” shall be added to state “Building Setbacks” to describe the first section of the DEII Dimensional Standards Summary Table in Section C (Site Design) on Sheet SD 4.1.
- F. Add sidewalks to Site Plan where appropriate with dimensions.
- G. The existing fencing east and south of Building Z shall be added to Sheet SDP 2.1.
- H. The depicted parking spaces shall match the number of parking spaces provided in the Facility Index Table on Sheet SDP1.2.
- I. Building S (Shade Cover) and Area/ Building Y (Vehicle Fueling Facility) shall be added to elevation drawings with complete dimensions.
- J. Existing motorcycle spaces and bicycle parking shall be documented as provided parking, if applicable.
- K. The DEII Airport Site Plan cover page, Sheet Index page titles for SDP 1.1 and SDP 1.2 shall be updated to reflect that the request is for an As-Built Site Plan and not an existing Site Plan.
- L. The DEII Airport Site Plan cover page, Sheet Index shall include Building identifiers.
- M. The DEII Airport Site Plan cover page, Sheet Index page titles shall be updated to match sheet titles for AE 3.22 (Site Amenities) and AE 3.26 (Walls, Fencing and Screening).
- N. The spelling inconsistency of “DE-II” in the Table, Wall and Fences row of Table of Applicable Standards, Sheet SDP1.1 shall be updated to DEII to reduce confusion.

6. Design Standards:

- A. For the western (and northwestern) boundaries with MPOS, no development shall be allowed within 150 feet except view fencing on the perimeter and seeding with native grassland mix appropriate for the area. Between 150 and 250 feet from the western and northwestern boundaries with MPOS, the maximum building height shall be 36 feet.

For the eastern boundary along Atrisco Vista Blvd, a setback of 150 feet shall be required. View fencing shall be allowed on the perimeter. Parking lots shall be located at least 50 feet from the eastern perimeter lot line. Landscaping shall be located at least 50 feet from the eastern perimeter lot line except for seeding with native grassland mix appropriate for the area. Between 150 and 250 feet from the lot line abutting Atrisco Vista Blvd., the maximum building height shall be 48 ft.

More than 250 feet from any perimeter lot line, the maximum building height shall be 65 feet.
- B. The ROW tree requirement along Atrisco Vista shall be removed; trees shall only be planted in areas with dense development and regular outdoor use or to minimize heat as part of hardscapes.
- C. Where not in conflict with FAA safety regulations or aviation functionality, Design Standards regarding clustered building (i.e., Conservation Design), shade structures, and shading over parking shall be added to Design Standards with regulations to be agreed upon as future amendments come forward to promote integrated site design that is considerate to the natural features and topography.
- D. The text 14-6-5-11 under Design Standards, section H. Architecture – Building Design, 1.: shall be updated to the correct IDO §14-16-5-11.

- E. The text “14-16-5-4-3” under Design Standards, section H. Architecture – Building Design, 7.: shall be updated to the correct IDO §14-16-6-5(D).
 - F. Building Height Section text “Within 250’ of Atrisco Vista Blvd ROW shall be updated to “From 150’ to 250’ of Atrisco Vista Blvd ROW” on Sheet SD 4.1.
 - G. Text shall be added that describes the proposed “50’ Required screen zone” in Section I.2. (Screening) on Sheet SD 4.1.
 - H. The Design Standard text of Sheet SD4.1, “The DEII Design Standards will also help provide a framework for review of proposed site developments. Subsequent site development plan requirements for developments on lease areas of 5 acres or less or that contain buildings of 50,000 sq. ft or less shall be delegated to the City of Albuquerque’s Development Facilitation Team (DFT) as a Minor Amendment. Development greater than these thresholds shall be delegated to the Environmental Planning Commission (EPC).” shall be moved to Section B (Process).
7. Elevations
- A. The storage building west of Building R shall be added to the proposed Site Plan, Sheet SDP1.2 with a Keyed Note.
 - B. Dimensions for western and northern structure features of Building X shall be added to AE3.20 as seen on SDP1.2.
 - C. The building height for the single-story portion of Building Z shall be added to Sheet AE3.21.
 - D. Both (2) Utility Structures shall be depicted with dimensions on Sheet AE3.23 (Site Amenities).
 - E. The symbols next to the Utility Structure to the southwest of Building B shall be added to the legend on Sheet SDP2.1.
8. Development shall not occur east of the right-of-way (ROW) of Atrisco Vista Blvd.
9. The Applicant shall coordinate with NMDOT District 3 Assistant Engineer Margaret Haynes to discuss the Facility Plan's potential impacts to Atrisco Vista Blvd and verify the DEII Airport address.
10. The Site Plan shall comply with the General Regulations of the IDO and all other applicable design regulations, except as specifically approved by the EPC.

APPEAL: If you wish to appeal this decision, you must do so within 15 days of the EPC’s decision or by **August 30, 2024**. The date of the EPC’s decision is not included in the 15-day period for filing an appeal, and if the 15th day falls on a Saturday, Sunday or Holiday, the next working day is considered as the deadline for filing the appeal.

For more information regarding the appeal process, please refer to §14-16-6-4(V) of the Integrated Development Ordinance (IDO), Administration and Enforcement. A non-refundable filing fee will be charged and is required to be paid at the time the appeal is filed.

You will receive notification if any person files an appeal. If there is no appeal, you can receive Building Permits at any time after the appeal deadline quoted above, provided all conditions imposed at the time of

approval have been met. Successful applicants are reminded that other regulations of the IDO must be complied with, even after approval of the referenced application(s).

Sincerely,



for Alan M. Varela,
Planning Director

AV/MRW/CH/MJ

CC:

Legal, aconon@cabq.gov
City of Albuquerque Aviation Department, Manny Manriquez, mmanriquez@cabq.gov
Dekker, Will Gleason, WillG@dpsdesign.org
Dekker, Jessica Lawlis, JessicaL@dpsdesign.org
Dekker, Rebecca Shank, RebeccaS@dpsdesign.org
DEII, Christina Sandoval, cmsandoval@cabq.gov
Bernadette Hardy, Bernadette@naeva.org
Alysia Coriz, alysia@naeva.org
Baxter Sosebee, mb422@comcast.net
Elizabeth Haley, elizabethkayhaley@gmail.com
Evelyn Rivera, rioreal@earthlink.net
Jane Baechle, jane.baechle@gmail.com
Santa Fe Village Neighborhood Association, SfVNA2014@gmail.com
Lan Sena, lan@civicpolicy.com
Rene' Horvath, aboard111@gmail.com
Mike Vorhees, mike@cyonic.com
NPS, Nancy Hendricks, nancy_hendricks@nps.gov
Pueblo of Laguna, Kenneth Bobroff, kbobroff@pol-nsn.org
CABQ OEI, Terry Sloan, terrysloan@cabq.gov
EPC file

PLANNING DEPARTMENT
URBAN DESIGN & DEVELOPMENT DIVISION
600 2nd Street NW, 3rd Floor, Albuquerque, NM 87102
P.O. Box 1293, Albuquerque, NM 87103
Office (505) 924-3860 Fax (505) 924-3339



OFFICIAL NOTIFICATION OF DECISION

August 15, 2024

City of Albuquerque
Aviation Department
2200 Sunport SE
Albuquerque, NM 87120

Project # PR-2018-001577
SI-2024-00994 – Site Plan – EPC

Dekker, agent for the City of Albuquerque Aviation Dept., requests an NR-SU Site Plan – EPC for the DEII Airport for all or a portion of Tract A-1, Plat of Tract A-1 & Tract L-1 Parcels 1-5, Double Eagle II Airport; Tract C and Parcel-1 Tract M Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; the Southeasterly Portion of Tract D-1-A-1, Tract D-1-A-2, and Tract S-1-A Plat of Tracts D-1-A-1, D-1-A-2, & S-1-A of Amended Bulk Land Plat for Aerospace Technology Park; Tract E, F, and K Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; Tract N-1 and Tract N-2 AKA Parcel 4 Bulk Land Plat of Tracts N-1, O-1, & N-2 Parcels 1, 2, 3, & 4 Double Eagle II Airport & Adjacent Lands; and Tract S-2 Bulk Land Plat for Aerospace Technology Park, Tracts D & S of Double Eagle II Airport & Adjacent Lands, located at 7401 Paseo Del Volcan NW, NW of the Atrisco Vista Blvd NW and Shooting Range Access Rd. intersection, approximately 4,200 acres.

(C-4, C-5, C-6, D-4, D-5, D-6, E-4, E-5, E-6, F-4, F-5, F-6, G-4, G-5, G-6)

Staff Planners: Catherine Heyne, Megan Jones

On August 15, 2024, the Environmental Planning Commission (EPC) voted to APPROVE PR-2018-001577 SI-2024-00994 – Site Plan – EPC, based on the following findings and subject to the following conditions of approval:

FINDINGS – SI-2024-00994 – Site Plan – EPC

1. This request is for a Site Plan – EPC for the Double Eagle II (“DEII”) Airport legally described as all or a portion of Tract A-1, Plat of Tract A-1 & Tract L-1, Parcels 1-5, Double Eagle II Airport; Tract C and Parcel-1 Tract M Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; the Southeasterly Portion of Tract D-1-A-1, Tract D-1-A-2, and Tract S-1-A Plat of Tracts D-1-A-1, D-1-A-2, & S-1-A of Amended Bulk Land Plat for Aerospace Technology Park; Tract E, F, and K Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; Tract N- 1 and Tract N-2 AKA Parcel 4 Bulk Land Plat of Tracts N-1, O-1, & N-2 Parcels 1, 2, 3, & 4 Double Eagle II Airport & Adjacent Lands; and Tract S-2 Bulk Land Plat for Aerospace Technology Park, Tracts D & S of Double Eagle

II Airport & Adjacent Lands, located at 7401 Paseo Del Volcan NW, NW of the Atrisco Vista Blvd NW and Shooting Range Access Rd intersection, containing approximately 4,200 acres (C-4, C-5, C-6, D-4, D-5, D-6, E-4, E-5, E-6, F-5, F-6, G-4, G-5, G-6).

2. The DEII Airport (the “subject site”) sits to the northwest of the Atrisco Vista Blvd. NW and Shooting Range Access Rd. intersection. This property is bounded by Major Public Open Space (MPOS), the City of Rio Rancho, and unincorporated Bernalillo County land and within a ¼ mile west of the Petroglyph National Monument. The subject site is about 8.5 miles northwest of Downtown Albuquerque (around 18 miles driving).
3. The subject site is zoned NR-SU and is within the DEII Employment Center, an Area of Change (most undeveloped land), and an Area of Consistency (airport areas) as designated by the Comprehensive Plan. Major Public Open Space (MPOS) is adjacent to DEII on its eastern and western boundaries with eastern portions within 330 ft of the Petroglyph National Monument.
4. The subject site is governed by the 2019 Double Eagle II Airport Master Plan. Master Plans and land uses are voluntary for the facility, but the Master Plan and Site Plan are meant to be complementary. As it stands, the Master Plan only includes an “Airport Layout Drawing.” There have been several Site Plans for the airport which have been approved administratively, but they are not consist with the IDO nor the NR-SU zone district.
5. The request documents the existing, as-built airport facilities and defines the associated NR-SU Design Standards required for the DEII Airport under the NR-SU zone district. The applicant is required to document the existing development in an “as-built” Site Plan to establish a baseline for any proposed future development and determinations for Design Standards. The NR-SU Site Plan establishes specific uses, site standards, and regulations for the Airport subject site, which are to be negotiated at the EPC. A site plan is required for any development to occur under the NR-SU zone district for the Airport Land Use.
6. The As-Built site plan is in compliance with the associated Master Plan for DEII (PR #2018-001577_SI-2024-00321).
7. The EPC is hearing this request (SI-2024-00994) pursuant to IDO §14-16-2-5(E) Non-Residential – Sensitive Use (NR-SU) zone district and IDO §14-16-6-6(I) Site Plan – EPC. The NR-SU zone district requires that a Site Plan is reviewed and decided by the EPC. The existing, as-built site conditions depicted in the proposed Site Plan shall remain valid until they are replaced (see IDO §14-16-1-10), which will require that any new development be in compliance with the IDO. The EPC shall review the uses, standards, and regulations as negotiated by the applicant.
8. The Albuquerque/Bernalillo County Comprehensive Plan and the City of Albuquerque Integrated Development Ordinance (IDO) are incorporated herein by reference and made part of the record for all purposes.
9. IDO §14-16-6-6(I)(3)(c) states, “The Site Plan complies with all applicable provisions of this IDO, the DPM, other adopted City regulations, and any terms and conditions specifically applied to development of the property in a prior permit or approval affecting the property.”

The proposed As-Built Site Plan has been evaluated for conformance with applicable goals and policies in the Comprehensive Plan and other applicable Plans. The approximately 4,200-acre site is being submitted as an “As-Built” Site Plan because the subject site area was developed pre-IDO and does not have an existing Site Plan on file with the City. Current uses and standards shall be allowed pursuant to IDO §14-16-1-10(A) Pre-IDO Approvals and IDO §14-16-4-1(E) Previously Allowed Uses.

Future development and amendments for the subject site would be required to follow all design standards set forth in the Site Plan for the DEII Airport and where silent, applicable IDO provisions, Site Plan and IDO Design Standards for the NR-SU zone district pursuant to IDO §14-16-2-5(E), as well as with all applicable provisions of the DPM, other adopted City regulations, and any other terms and conditions specific to the subject site. Anywhere the Design Standards are silent, the IDO regulations shall apply. Existing development is allowed to continue operating, even if nonconforming, pursuant to IDO §14-16-1-10(A) Pre-IDO Approvals and IDO §14-16-4-1(E) Previously Allowed Uses.

The 300-acre portion of the site governed by the Eclipse Aviation Campus Site Plan for Subdivision (PR 1003125 & 06DRB-00401) and the Aerospace Technology Park Site Development Plan for Subdivision (PR 1003125 03EPC-02054 & 05EPC-00566) is not a part of the request (Tract F Bulk Land Plat of Double Eagle II Airport and Adjacent Lands; Tract D-1-A-1, Tract S-1-A, and the Southwesterly Portion of Tract D-1-A-1, Plat of Tracts D-1-A-1, D-1-A-2 & S-1-A of amended bulk land plat for Aerospace Technology Park; Tract D-1-A-2 and the Southeasterly portion of Tract D-1-A-1, Plat of Tracts D-1-A-1, D-1-A-2 & S-1-A of amended bulk land plat for Aerospace Technology Park; and Lots 1-15 Bulk Land Plat of lots 1 - 15 Aerospace Technology Park, Unit 1).

10. The request is generally consistent with GOAL 4.2 Process: Engage communities to identify and plan for their distinct character and needs from Comprehensive Plan Chapter 4: Community Identity:

Although not required by IDO regulations, City Aviation reached out to Neighborhood Associations/Coalitions and other interested parties for a Public Open House held June 28, 2024 to engage communities and help identify and plan for the distinct character and needs of the DEII Airport Facilities. Solicited feedback regarding the proposed as-built Site Plan and Design Standards contributed to proposed Design Standards revisions.

11. The request is partly consistent with the following Policies from Comprehensive Plan Chapter 4: Community Identity:

- A. POLICY 4.1.5 NATURAL RESOURCES: Encourage high-quality development and redevelopment that responds appropriately to the natural setting and ecosystem functions.

This request is for an as-built Site Plan that also documents proposed Design Standards. The site is surrounded to the east and west by MPOS that includes the Petroglyph National Monument. As the Applicant stated, the “DEII Design Standards are integral to establishing an aesthetic character compatible with the surrounding context, particularly the City of Albuquerque Open Space and Petroglyph National Monument.”

The subject site is located in a generally undeveloped, desert grassland with expansive views to the mountains to the north (Sangre de Cristos), east (Petroglyph National Monument volcanoes,

Sandias), southeast (Manzanos), and west (Mt. Taylor). NR-SU zone district Design Standards may be varied in the NR-SU approval process (see IDO Table 2-5-9: Other Applicable IDO Sections) with Dimensional Standards determined in the approval of a Site Plan – EPC (see IDO §14-16-5-1(E)(2)(b)). Proposed DEII Airport Design Standards generally conform to IDO design standards (e.g., Access and Connectivity, Subdivision of Land, Parking and Loading, Solar access, and Signs) that work to help reduce development impact on Major Public Open Space (MPOS), such as suggesting a color palette and light reflective values (LRV) recommended for Sensitive Lands (see IDO §14-16-5-2) for buildings within 250 feet of MPOS. It also incorporates active and passive water harvesting, passive heating and cooling structure design, permeable pavements, less linear landscape lines, and using vegetation to help slow and filter runoff. However, any significant development to the scale proposed will increase light, noise, traffic, pollution, temperatures, and habitat fragmentation as well as impact natural drainage and views to culturally important landscape features to and from the Petroglyphs National Monument and Major Public Open Space.

Greater allowances for building setbacks (150 feet from MPOS) and requiring screened parking areas, should help lower development impact in terms of views from MPOS to the subject site, however, staff disagrees that the proposed building height maximums of 60 feet within a 150-foot setback from Atrisco Vista Blvd and 85 foot maximum for all other areas respond appropriately to the natural setting despite the addition of sustainable caveats such as orienting building to overlook MPOS in combination with passive and cooling. The proposed heights seem out of scale with area open space features and appears to be designed to compete with the volcanos themselves.

Adding trees to the Atrisco Vista Blvd ROW also seems out of character with the natural setting and ecosystem functions.

- B. POLICY 4.2.2 COMMUNITY ENGAGEMENT: Facilitate meaningful engagement opportunities and respectful interactions in order to identify and address the needs of all residents.

According to the Office of Neighborhood Coordination (ONC), there are no designated Neighborhood Associations within the required notification buffer areas; however, after the general required notification of property owners within 100 feet of the proposed Site Plan – EPC request, City Aviation reached out to Neighborhood Associations/Coalitions and other interested parties for a Public Open House held June 28, 2024. At this time, City Aviation provided information to solicit feedback regarding the proposed as-built Site Plan and Design Standards being brought before the EPC. Attendance included representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation, tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature (see Attachments). Comments collected during the meeting resulted in some Design Standards revisions. Updates of project description and were also resent to adjacent property owners and emailed to neighboring Neighborhood Associations/Coalitions.

However, the fact that this request is less than 330 ft from the Petroglyph National Monument, which is widely known as a significant sacred site for most of the area's Pueblos and Tribes, combined with the fact that only Pueblo of Laguna attended the Open House, this one Open

House would not be consistent with meaningful engagement and respectful interactions to identify and address the needs of all residents.

12. The request is consistent with the following Goals and Policies regarding Centers, Corridors and Development areas from Comprehensive Plan Chapter 5: Land Use:

- A. **GOAL 5.1 CENTERS AND CORRIDORS:** Grow as a community of strong centers connected by a multi-modal network of Corridors.

The subject site is located within a Developing Employment Center along the Regional Principal Arterial and Commuter Corridor, Atrisco Vista Blvd NW that prioritizes automobile travel in street design and improvements by allowing higher traffic speed and volume, connecting to I-40 about 4.5 miles to the south. It also is surrounded to the east and west by Major Public Open Space and includes the Petroglyph National Monument within 330 feet. The request codifies an as-built DEII Airport Site Plan and Design Standards to allow for future site development at the subject site. Adding potential employment sites to the West Side of the City would allow for the addition of jobs and employment opportunities west of the Rio Grande and could help improve the job-housing balance on that side of town.

- B. **POLICY 5.1.1 DESIRED GROWTH:** Capture regional growth in Centers and Corridors to help shape the built environment into a sustainable development pattern.

The subject site is located within a Developing Employment Center and along a Regional Principal Arterial/ Commuter Corridor of Atrisco Vista Blvd NW. Allowing growth within a Developing Employment Center promotes a more sustainable development patterns and could reduce the job-housing imbalance west of the river as articulated in the ABC Comp Plan, especially as the City's West Side continues to sprawl.

- C. **POLICY 5.1.2: DEVELOPMENT AREAS:** Direct more intense growth to Centers and Corridors and use Development Areas to establish and maintain appropriate density and scale of development within areas that should be more stable.

The subject site is located mostly within an ABC Comp Plan designated Area of Change, with the Areas of Consistency consisting of integral DEII Airport functions. The request will result in an as-built Site Plan documenting existing airport facilities and codifying Design Standards which will help guide growth and establish development density and scale for an area intended for higher intensity uses.

- D. **POLICY 5.1.5 EMPLOYMENT CENTERS:** Create Centers that prioritize employment and foster synergy among businesses.

The subject site is located within a Developing Employment Center and along a Regional Principal Arterial/ Commuter Corridor of Atrisco Vista Blvd NW. Allowing growth within a Developing Employment Center helps prioritize employment and foster synergy among businesses in an area with good automobile access and good connectivity for freight routes. The proposed DEII design standards also consider pedestrian and bicycle access within the Developing Employment Center.

- E. **GOAL 5.3 EFFICIENT DEVELOPMENT PATTERNS:** Promote development patterns that maximize the utility of existing infrastructure and public facilities and the efficient use of land to support the public good.

The proposed request promotes internal development patterns that maximize the utility of existing infrastructure and public facilities by documenting as-built DEII Airport structures and associated facilities, as well as compiling Design Standards for future proposed development at the subject site within a Developing Employment Center to better maintain a more cohesive future development. As this request does not propose any development, there will be no changes to the subject site layout, which preserves current efficiencies in land use to support the public good.

- F. **POLICY 5.7.2 REGULATORY ALIGNMENT:** Update regulatory frameworks to support desired growth, high quality development, economic development, housing, a variety of transportation modes, and quality of life priorities.

There is no DEII Airport Site Plan or Design Standards relative to the NR-SU zone district that is associated with the current Airport Master Plan. The proposed subject site Design Standards should provide a clearer regulatory framework and processes for any proposed future development to better support desired growth and both high quality employment and economic development.

13. The request is generally consistent with **POLICY 5.6.2 AREAS OF CHANGE:** Direct growth and more intense development to Centers, Corridors, industrial and business parks, and Metropolitan Redevelopment Areas where change is encouraged, from Comprehensive Plan Chapter 5: Land Use:

The proposed Design Standards being submitted as part of this request for an as-built Site Plan outlines potential future growth and more intense development in Areas of Change within a Developing Employment Center. This could encourage development that expands employment opportunities and job creation within the facility boundaries. At this stage of development of the west side of the Petroglyph National Monument, there will be minimal impacts of development on existing residential uses regarding noise, stormwater runoff, contaminants, lighting, air quality, and traffic. However, with the planned growth development impacts in general will increase in what are currently undeveloped natural habitats.

14. The request is partly consistent with **GOAL 5.6 CITY DEVELOPMENT AREAS:** Encourage and direct growth to Areas of Change where it is expected and desired and ensure that development in and near Areas of Consistency reinforces the character and intensity of the surrounding area, from Comprehensive Plan Chapter 5: Land Use:

Although the subject site is already developed as a Regional General Aviation airport—a reliever for the Sunport—that primarily focuses on business activity, lease areas within the airport tracts lie an Area of Change where development is expected and desired. The proposed Design Standards encourage and direct growth to Areas of Change where it is expected and desired and generally ensure that development in and near Areas of Consistency reinforces the character and intensity of the surrounding area. Proposed building height maximums and proposed trees along public ROW however, would change the character and intensity of land use considerably.

15. The request is not consistent with POLICY 5.3.4 CONSERVATION DEVELOPMENT: Encourage conservation development to promote private open space and preserve natural landscape, agricultural lands, and other features of the natural environment to encourage development that is sensitive to the open, natural character of the area and the geological and cultural conditions, from Comprehensive Plan Chapter 5: Land Use:

Staff agrees with the Applicant that given the large area of DEII and the need to prioritize airport operations, the DEII Design Standards should concentrate new development within a prescribed area and preserve the surrounding natural landscape which consists of a wide-open landscape with expansive views in all directions.

This request proposes landscaping, screening, and buffering standards, especially along parking lots, in areas with necessary but “less attractive” development, such as, loading, service, mechanical equipment, parking areas, and refuse facilities which should minimize visual impact from MPOS and Atrisco Vista Blvd. The suggested continuous line of 3-to-4-foot evergreen shrubbery with a minimum of 6-foot-wide buffer could harmonize with available native species (e.g., *Atriplex* sp) and preserve the natural landscape and local views; however, a dark green evergreen, such as decorative juniper will be out of place and inappropriate for the surrounding landscape. The addition of trees in landscape that emphasizes visitor and employee centric spaces and walkways can potentially be appropriate in certain areas, but not e along Atrisco Vista Blvd, especially on the east side since it will potentially change the character of the landscape.

Because the addition of fencing impacts view distances, view fencing should retain the feeling of Open Space and the viewshed, while helping maintain the built aesthetic. Proposed color schemes and light reflectivity standards that echo the surrounding natural environment shall reduce the visual impacts of new development. Again, the subject site is located in a generally flat, gradually sloped and undeveloped, desert grassland with expansive views to the north (Sangre de Cristos), east (Petroglyph National Monument volcanoes, Sandias), southeast (Manzanos), and west (Mt. Taylor), and greater building setback allowances should help lower development impact in terms of views from MPOS to the subject site as this will bar structures and buildings at the property lines. Staff disagrees that the proposed building height maximums of 60 feet within a 150-foot setback from Atrisco Vista Blvd and 85 foot maximum for all other areas responds appropriately to the natural setting. Nevertheless, proposed Design Standards encourage tenants to orient buildings to allow for views, especially of the Petroglyph National Monument. Any significant development to the scale proposed will increase light, noise, traffic, temperature, and habitat fragmentation as well as impact views to culturally important landscape features.

16. The request is partly consistent with the following Policies regarding Centers, Corridors and Development areas from Comprehensive Plan Chapter 7: Urban Design:
 - A. POLICY 7.3.1 NATURAL AND CULTURAL FEATURES: Preserve, enhance, and leverage natural features and views of cultural landscapes, from Comprehensive Plan Chapter 7: Urban Design:

The proposed site designs address development issues to preserve and leverage natural features and views of cultural landscapes with development setback provisions, specifying appropriate color choices and building materials to blend in with the natural landscape, adding low landscape buffers and appropriate screening to less aesthetically pleasing development, requiring more

subdued and consolidated signage, burying all electric distribution lines, and eliminating light trespass beyond the property line or allowing light to be visible from MPOS except where FAA regulations may vary.

However, staff disagrees that the proposed Design Standards with building height maximums of 60 feet within a 150-foot setback from Atrisco Vista Blvd and an 85-foot maximum for all other areas preserve, enhance, and leverage natural features. Such heights will compete with the elevations of the nearby volcanos and would probably be visible from the City's east side, similar to the current warehouse fulfillment center to the south of the DEII Airport just outside of the City. Such heights would also be the first impression on entry from the west into the City on the highway and would be the features versus leveraging the neighboring volcano features.

Added trees to public ROWs, especially along Atrisco Vista Blvd would not maintain the natural conditions of the natural landscapes within the DEII parcels and would not provide an appropriate transition to adjoining Major Public Open Space.

- B. POLICY 7.5.1 LANDSCAPE DESIGN: Encourage landscape treatments that are consistent with the high desert climate to enhance our sense of place.

Some of the proposed landscape treatments are consistent with the high desert climate. For example, where not in conflict with FAA safety regulations or aviation functionality, the DEII Design Standards encourage active and passive water harvesting techniques to slow and capture stormwater on site as well as to supplement watered landscaped areas as natural drainage is modified.

Although the Design Standards encourage the use of native plant materials that enhance our sense of place, regionally adapted plants that will thrive in local conditions with less fertilizer, water, and maintenance are also proposed, the latter of which do not match adjacent landscapes and could be invasive.

The addition of trees along the public ROW would not be consistent with the microclimate of the site location or the dramatic setting of the West Mesa, but if restricted to visitor and employee centric spaces to create moderated microclimates, trees could help allow for the high desert climate in adjoining spaces, and with that, our sense of place.

17. The request is not consistent with GOAL 7.5 CONTEXT-SENSITIVE SITE DESIGN: Design sites, buildings, and landscape elements to respond to the high desert environment, from Comprehensive Plan Chapter 7 Urban Design:

Staff agrees that the natural terrain and open landscapes of arid grassland of the DEII Airport and surrounding West Mesa offers spectacular open views in all directions from the Petroglyph National Monument Volcanos to distant mountains (e.g., Sangre de Cristos, Sandias, Manzanos, Mt Taylor). Because of this, it is notable that the proposed DEII Design Standards prioritizes maintaining a natural landscape wherever possible. The fact that the proposed Design Standards center sustainable practices, including Green Stormwater Infrastructure (GSI) and Low Impact Development (LID) practices for site design, buildings, and landscape elements to respond to the high desert environment are also important. For the most part, sustainable, context-sensitive Site Design has been proposed. However, Staff contends that the proposed building heights and tree-lined ROWs

will negatively impact views to existing landscape elements. Buildings reaching maximum heights of 60 or 85 feet, although in effect represent denser development, will not preserve the open vistas. Within recent history, trees do not naturally contribute to the area's native vegetation and will block views to the natural high desert environment.

18. The request is not consistent with the following Policies from Comprehensive Plan Chapter 11 Heritage Conservation:

A. **GOAL 11.3 CULTURAL LANDSCAPES:** Protect, reuse, and/or enhance significant cultural landscapes as important contributors to our heritage and rich and complex identities.

The proposed DEII Airport Design Standards aim to protect the significant cultural landscapes that surround the airport. These critical landscapes include Major Public Open Space and the striking volcano cones and cultural remains protected by the Petroglyph National Monument, all of which contribute to the area's heritage as well as our rich and complex identities. Accordingly, all future development shall be pursuant to Archaeological Sites regulations of the IDO Site Design and Sensitive Lands (see §14-16-5-2(D)(1)).

Although the proposed DEII Design Standards take into consideration these remarkable westside viewsheds and exceed IDO standards, where feasible, additional measures can be taken to minimize the impact of development, such as:

- 1) Reduce building heights,
- 2) Increase building density,
- 3) Constrain development to only the west side of Atrisco Vista,
- 4) Eliminate the ROW tree requirement along Atrisco Vista and most ROW area; plant trees only in areas with dense development and regular outdoor use or to minimize heat as part of hardscapes,
- 5) Encourage shade structures,
- 6) Require shading over parking,
- 7) Require solar hookups, and
- 8) When developing land that has not already been bladed, remove plants prior to ground breaking to reuse in landscaping.

Proposed protective DEII Design Standards include minimizing the visibility of structures through color and building material restrictions, minimum building and parking setbacks, adding screening, wall, and fencing requirements. Additionally, building setbacks have been added for MPOS and Atrisco Vista Blvd that are greater than those required by the IDO to minimize impacts to MPOS and the Petroglyph National Monument.

B. **POLICY 11.3.4 PETROGLYPH NATIONAL MONUMENT:** Regulate adjacent development to protect and preserve the Petroglyph National Monument – its volcanoes, petroglyphs, and Northwest Mesa Escarpment – as a priceless cultural landscape and community resource that provides physical, cultural, and economic benefits.

The West Side is expanding, and the DEII Airport is within a designated Employment Center. The proposed DEII Design Standards attempt to protect the Monument from growth and development pressures by concentrating leased land development primarily to the southwest

corner of the property. Additional protective DEII Design Standards include minimizing the visibility of structures through color and building material restrictions, minimum building and parking setbacks, adding screening, wall, and fencing requirements. Additionally, building setbacks have been added for MPOS and Atrisco Vista Blvd that are greater than those required by the IDO to minimize impacts to MPOS and the Petroglyph National Monument.

However, the proposed structure height maximums of 60 feet between 150 and 250 feet from the Atrisco Vista ROW and up to 85 feet in all other areas (provided that they comply with the FFA FAR Part 7 regulations) are incongruent with the area's open grasslands and the Petroglyph National Monument. With these building heights, views to and from those spaces would not be preserved. Trees added to the Atrisco Vista Blvd ROW would also change the character of the landscape and not preserve the natural open spaces that characterize the area. Instead of the volcanos, any future proposed development would act as a visual reference point, rendering edge treatments and transitions as inappropriate.

19. The request meets the Site Plan – EPC Review & Decision Criteria in IDO §14-16-6-6(I)(3) as follows:

- A. 6-6(I)(3)(a) The site plan is consistent with the ABC Comp Plan, as amended. As demonstrated by the policy-based analysis of the proposed Site Plan, the request is generally consistent with applicable Comprehensive Plan Goals and policies.
- B. 6-6(I)(3)(b) The Site Plan is consistent with any applicable terms and conditions in any previously approved NR-SU or PD zoning covering the subject property and any related development agreements and/or regulations.

The subject site is zoned NR-SU. There is not an NR-SU Site Plan on file for the DEII Airport Property in its entirety. There is a Site Development Plan for Subdivision with Design standards that is valid for a 300-acre portion of the airport property which includes the Site Plan for Eclipse Aviation. This SDP for Subdivision governs these specific tracts within the Airport, which shall be noted on the NR-SU site plan: Tract F Bulk Land Plat of Double Eagle II Airport and Adjacent Lands; Tract D-1-A-1, Tract S-1-A, and the SW'LY Portion of Tract D-1-A-1, Plat of Tracts D-1-A- 1, D-1-A-2 &S-1-A of amended bulk land plat for Aerospace Technology Park; Tract D-1-A-2 and the Southeasterly portion of Tract D-1-A-1, Plat of Tracts D-1-A-1, D- 1-A-2 & S-1-A of amended bulk land plat for Aerospace Technology Park; and LTS 1-15 Bulk Land Plat of lots 1 - 15 Aerospace Technology Park, Unit 1. No other related development agreements and/or regulations are known to be in place for the project site.

- C. 6-6(I)(3)(c) The Site Plan complies with all applicable provisions of this IDO, the DPM, other adopted City regulations, and any terms and conditions specifically applied to development of the property in a prior permit or approval affecting the property.

Staff has reviewed the proposed as-built Site Plan for documentation purposes. The existing development for the airport use at the subject site was established prior to the adoption of the 2018 IDO; therefore, current, as-built development will be held to Pre-IDO Approvals pursuant to IDO §14-16-1-10 and IDO §14-16-4-1(E). Current land uses, buildings, lots, signs, and site features may become nonconforming due to the adoption of the IDO and will be pursuant to IDO §14-16-6-8 Nonconformities.

The subject site lies adjacent to Major Public Open Space (MPOS) and is within 330 feet of the Petroglyph National Monument. Under current IDO regulations, this location requires compliance to IDO §14-16-5-2 Site Design and Sensitive Lands, which prompts a Cumulative Impact Analysis review for any future development. MPOS adjacency also necessitates an analysis of MPOS Edges requirements (see IDO §14-16-5-2(J)). This has been completed as part of the staff report.

Although there is not any proposed new development at this time, any future proposed development will be required to comply with all applicable provisions and Design Standards for the NR-SU zone district pursuant to IDO 14-16-2-5(E) as well with all applicable provisions of the IDO, the DPM, other adopted City regulations, and any other terms and conditions specific to the subject site.

- D. 6-6(I)(3)(d) The City's existing infrastructure and public improvements, including but not limited to its street, trail, drainage, and sidewalk systems, have adequate capacity to serve the proposed development, and any burdens on those systems have been mitigated to the maximum extent practicable.

The City's existing infrastructure and public improvements should have adequate capacity to continue to serve the existing development. At this time, there is no proposed changes are being proposed to the existing use, access points, circulation, parking, or traffic patterns, noise, or hazardous material use; this application is to document the as-built physical environment at the DEII Airport.

- E. 6-6(I)(3)(e) The application mitigates any significant adverse impacts on the project site and the surrounding area to the maximum extent practicable.

The current manufacturing plant is a development pre-existing to the 2018 IDO and does not have a controlling Site Plan. Current uses shall be allowed pursuant to IDO §14-16-4-1(E) Previously Allowed Uses. The requested as-built Site Plan for the subject site establishes a baseline for allowable uses, design standards, and other development regulations for current and any future development.

- F. 6-6(I)(3)(f) If the subject property is within an approved Master Development Plan, the Site Plan meets any relevant standards in the Master Development Plan in addition to any standards applicable in the zone district the subject property is in.

The subject site is not within an approved Master Development Plan area.

- G. 6-6(I)(3)(g) If a cumulative impact analysis is required in the Railroad and Spur Small Area pursuant to §14-16-5-2(E) (Cumulative Impacts) and §14-16-6-4(H) (Cumulative Impacts Analysis Requirements), the Site Plan incorporates mitigation for all identified cumulative impacts. The proposed development will not create material adverse impacts on water quality or other land in the surrounding area through increases in traffic congestion, parking congestion, noise, vibration, light spillover, or other nuisances without sufficient mitigation or civic or environmental benefits that outweigh the expected impacts.

The request is for an as-built Site Plan under the NR-SU zone district for the DEII Airport subject site, which establishes a baseline for allowable uses, design standards, and other development regulations for current and any future development.

20. A member of the Santa Fe Village Neighborhood Association (SFVNA) wrote a letter of opposition dated August 5, 2024. The concerned citizen is opposed to the proposed 85-foot building height standard outlined in the Design Standards for the DEII Site Plan.
21. The National Park Service (NPS), Petroglyph National Monument, submitted a letter to the Planning Department on August 5, 2024 expressing concerns about the proposed building heights included in the Design Standards. They state that they “cannot support building heights as currently included in the design guidelines and request the City of Albuquerque Aviation Department reconsider and institute a maximum building height of 36 feet in order to protect the values of the West Mesa and Petroglyph National Monument.”
22. Solar power or other forms of renewable energy shall be encouraged for all new development.
23. Existing wall-mounted and free-standing signs may not be in compliance with current IDO regulations. If nonconforming, all future changes shall comply with IDO regulations pursuant to IDO §14-16-1-10(A) pre-IDO Approvals, §14-16-6-4(Z) Amendments of Pre-IDO Approvals, and §14-16-6-6-8(F) Nonconforming Signs, and any new development will comply with IDO and/or Double Eagle II design standards.
24. For future site plan applications, the Environmental Planning Commission expressed a desire to review profiles showing building height impacts as viewed from the Petroglyph National Monument.

Conditions of Approval

Project #: 2018-001577 / Case #: 2024-00994 - Site Plan – EPC

1. The applicant shall coordinate with the staff planner to ensure that all Conditions of Approval are met and then submit a vetted, final version to the staff planner for filing at the Planning Department.
2. After approval by the Environmental Planning Commission (EPC), the applicant shall submit the site plan that meets conditions of approval to the Staff Planner or relevant Urban Design and Development Staff for final sign-off. The reviewer will be responsible for ensuring that all EPC Conditions have been satisfied and that the IDO, DPM, and all other applicable City requirements have been met.
3. Pursuant to IDO §14-16-6-6(I)(2)(m) Site Plans shall be reviewed administratively for compliance with conditions of approval, DPM, and zoning standards prior to the issuance of a building permit.
4. Pursuant to IDO §14-16-6-4(P)(4) any conditions shall be met within 1 year of the approval, unless stated otherwise in the approval. If any conditions are not met within that time, the approval is void. The Planning Director may extend the time limit up to an additional 1 year.
5. Site Plan:
 - A. G5 shall be added to the Zone Atlas description for all pages (e.g., SDP1.2).
 - B. Zoning descriptions shall be indicated for each parcel outside of the DEII boundary on Sheet SDP1.1.

- C. A note shall be added to explain the Obstacle Free Zone on Sheets SDP1.2 and 2.1.
 - D. A keyed note shall be added for the diagonal (NW-SE) solid line running from the area with buildings Q1-4 and road on Sheet SDP1.2.
 - E. “Building” shall be added to state “Building Setbacks” to describe the first section of the DEII Dimensional Standards Summary Table in Section C (Site Design) on Sheet SD 4.1.
 - F. Add sidewalks to Site Plan where appropriate with dimensions.
 - G. The existing fencing east and south of Building Z shall be added to Sheet SDP 2.1.
 - H. The depicted parking spaces shall match the number of parking spaces provided in the Facility Index Table on Sheet SDP1.2.
 - I. Building S (Shade Cover) and Area/ Building Y (Vehicle Fueling Facility) shall be added to elevation drawings with complete dimensions.
 - J. Existing motorcycle spaces and bicycle parking shall be documented as provided parking, if applicable.
 - K. The DEII Airport Site Plan cover page, Sheet Index page titles for SDP 1.1 and SDP 1.2 shall be updated to reflect that the request is for an As-Built Site Plan and not an existing Site Plan.
 - L. The DEII Airport Site Plan cover page, Sheet Index shall include Building identifiers.
 - M. The DEII Airport Site Plan cover page, Sheet Index page titles shall be updated to match sheet titles for AE 3.22 (Site Amenities) and AE 3.26 (Walls, Fencing and Screening).
 - N. The spelling inconsistency of “DE-II” in the Table, Wall and Fences row of Table of Applicable Standards, Sheet SDP1.1 shall be updated to DEII to reduce confusion.
6. Design Standards:
- A. For the western (and northwestern) boundaries with MPOS, no development shall be allowed within 150 feet except view fencing on the perimeter and seeding with native grassland mix appropriate for the area. Between 150 and 250 feet from the western and northwestern boundaries with MPOS, the maximum building height shall be 36 feet.

For the eastern boundary along Atrisco Vista Blvd, a setback of 150 feet shall be required. View fencing shall be allowed on the perimeter. Parking lots shall be located at least 50 feet from the eastern perimeter lot line. Landscaping shall be located at least 50 feet from the eastern perimeter lot line except for seeding with native grassland mix appropriate for the area. Between 150 and 250 feet from the lot line abutting Atrisco Vista Blvd., the maximum building height shall be 48 ft.

More than 250 feet from any perimeter lot line, the maximum building height shall be 65 feet.
 - B. The ROW tree requirement along Atrisco Vista shall be removed; trees shall only be planted in areas with dense development and regular outdoor use or to minimize heat as part of hardscapes.
 - C. Where not in conflict with FAA safety regulations or aviation functionality, Design Standards regarding clustered building (i.e., Conservation Design), shade structures, and shading over parking shall be added to Design Standards with regulations to be agreed upon as future amendments come forward to promote integrated site design that is considerate to the natural features and topography.
 - D. The text 14-6-5-11 under Design Standards, section H. Architecture – Building Design, 1.: shall be updated to the correct IDO §14-16-5-11.

- E. The text “14-16-5-4-3” under Design Standards, section H. Architecture – Building Design, 7.: shall be updated to the correct IDO §14-16-6-5(D).
 - F. Building Height Section text “Within 250’ of Atrisco Vista Blvd ROW shall be updated to “From 150’ to 250’ of Atrisco Vista Blvd ROW” on Sheet SD 4.1.
 - G. Text shall be added that describes the proposed “50’ Required screen zone” in Section I.2. (Screening) on Sheet SD 4.1.
 - H. The Design Standard text of Sheet SD4.1, “The DEII Design Standards will also help provide a framework for review of proposed site developments. Subsequent site development plan requirements for developments on lease areas of 5 acres or less or that contain buildings of 50,000 sq. ft or less shall be delegated to the City of Albuquerque’s Development Facilitation Team (DFT) as a Minor Amendment. Development greater than these thresholds shall be delegated to the Environmental Planning Commission (EPC).” shall be moved to Section B (Process).
7. Elevations
- A. The storage building west of Building R shall be added to the proposed Site Plan, Sheet SDP1.2 with a Keyed Note.
 - B. Dimensions for western and northern structure features of Building X shall be added to AE3.20 as seen on SDP1.2.
 - C. The building height for the single-story portion of Building Z shall be added to Sheet AE3.21.
 - D. Both (2) Utility Structures shall be depicted with dimensions on Sheet AE3.23 (Site Amenities).
 - E. The symbols next to the Utility Structure to the southwest of Building B shall be added to the legend on Sheet SDP2.1.
8. Development shall not occur east of the right-of-way (ROW) of Atrisco Vista Blvd.
9. The Applicant shall coordinate with NMDOT District 3 Assistant Engineer Margaret Haynes to discuss the Facility Plan's potential impacts to Atrisco Vista Blvd and verify the DEII Airport address.
10. The Site Plan shall comply with the General Regulations of the IDO and all other applicable design regulations, except as specifically approved by the EPC.

APPEAL: If you wish to appeal this decision, you must do so within 15 days of the EPC’s decision or by **August 30, 2024**. The date of the EPC’s decision is not included in the 15-day period for filing an appeal, and if the 15th day falls on a Saturday, Sunday or Holiday, the next working day is considered as the deadline for filing the appeal.

For more information regarding the appeal process, please refer to §14-16-6-4(V) of the Integrated Development Ordinance (IDO), Administration and Enforcement. A non-refundable filing fee will be charged and is required to be paid at the time the appeal is filed.

You will receive notification if any person files an appeal. If there is no appeal, you can receive Building Permits at any time after the appeal deadline quoted above, provided all conditions imposed at the time of

approval have been met. Successful applicants are reminded that other regulations of the IDO must be complied with, even after approval of the referenced application(s).

Sincerely,

for Alan M. Varela,
Planning Director

AV/MRW/CH/MJ

CC:

Legal, aconon@cabq.gov
City of Albuquerque Aviation Department, Manny Manriquez, mmanriquez@cabq.gov
Dekker, Will Gleason, WillG@dpsdesign.org
Dekker, Jessica Lawlis, JessicaL@dpsdesign.org
Dekker, Rebecca Shank, RebeccaS@dpsdesign.org
DEII, Christina Sandoval, cmsandoval@cabq.gov
Bernadette Hardy, Bernadette@naeva.org
Alysia Coriz, alysia@naeva.org
Baxter Sosebee, mb422@comcast.net
Elizabeth Haley, elizabethkayhaley@gmail.com
Evelyn Rivera, rioreal@earthlink.net
Jane Baechle, jane.baechle@gmail.com
Santa Fe Village Neighborhood Association, SfVNA2014@gmail.com
Lan Sena, lan@civicpolicy.com
Rene' Horvath, aboard111@gmail.com
Mike Vorhees, mike@cyonic.com
NPS, Nancy Hendricks, nancy_hendricks@nps.gov
Pueblo of Laguna, Kenneth Bobroff, kbobroff@pol-nsn.org
CABQ OEI, Terry Sloan, terrysloan@cabq.gov
EPC file



Environmental Planning Commission

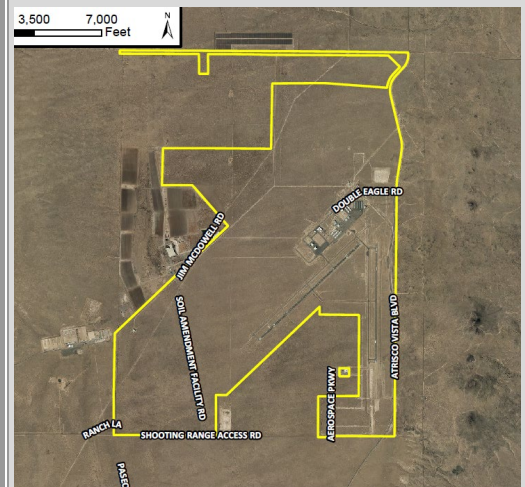
Agenda Number: 3
Project #: 2018-001577
Case #: SI-2024-00994
Hearing Date: August 15, 2024

Staff Report

Agent	Dekker
Applicant	City of Albuquerque Aviation Department
Request	Double Eagle II Airport Site Plan – EPC
Legal Description	All or a portion of Tract A-1, Plat of Tract A-1 & Tract L-1, Parcels 1-5, Double Eagle II Airport; Tract C and Parcel-1 Tract M Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; the Southeasterly Portion of Tract D-1-A-1, Tract D-1-A-2, and Tract S-1-A Plat of Tracts D-1-A-1, D-1-A-2, & S-1-A of Amended Bulk Land Plat for Aerospace Technology Park; Tract E, F, and K Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; Tract N-1 and Tract N-2 AKA Parcel 4 Bulk Land Plat of Tracts N-1, O-1, & N-2 Parcels 1, 2, 3, & 4 Double Eagle II Airport & Adjacent Lands; and Tract S-2 Bulk Land Plat for Aerospace Technology Park, Tracts D & S of Double Eagle II Airport & Adjacent Lands
Location	7401 Paseo Del Volcan NW, NW of the Atrisco Vista Blvd NW and Shooting Range Access Rd intersection
Size	Approximately 4,200 acres
Zoning	NR-SU (Non-residential - Sensitive use)

Staff Recommendation

APPROVAL of Project # 2018-001577, SI-2024-00994 based on the Findings 1-21 and subject to the Conditions of Approval 1-10 within this report.



Staff Planners:

Megan Jones, Principal Planner
Catherine Heyne, Planner

Summary of Analysis

This request is for an as-built Site Plan for the Double Eagle II (DEII) Airport that did not have a previously approved plan, which is required under the NR-SU zone district. DEII is classified as a Regional Reliever Airport for the Albuquerque International Sunport. DEII property is owned and operated by the City of Albuquerque Aviation Department.

The subject site is within the DEII Employment Center, an Area of Change (most undeveloped land), and an Area of Consistency (airport areas) per the Comprehensive Plan. Major Public Open Space (MPOS) is adjacent to DEII to the east and west. Much of the eastern boundary is within 330 ft. of the Petroglyph National Monument.

There are no affected neighborhood organizations. Property owners within a 100' buffer were notified as required. An Open House was held for area representatives on June 28, 2024. Two Agencies were in support of the Site Plan. The National Parks Service (NPS) Petroglyph National Monument and a member of the Santa Fe Village Neighborhood Association (SFVNA) are in opposition to the proposed 85 ft and 60 ft building heights in the proposed Design Standards.

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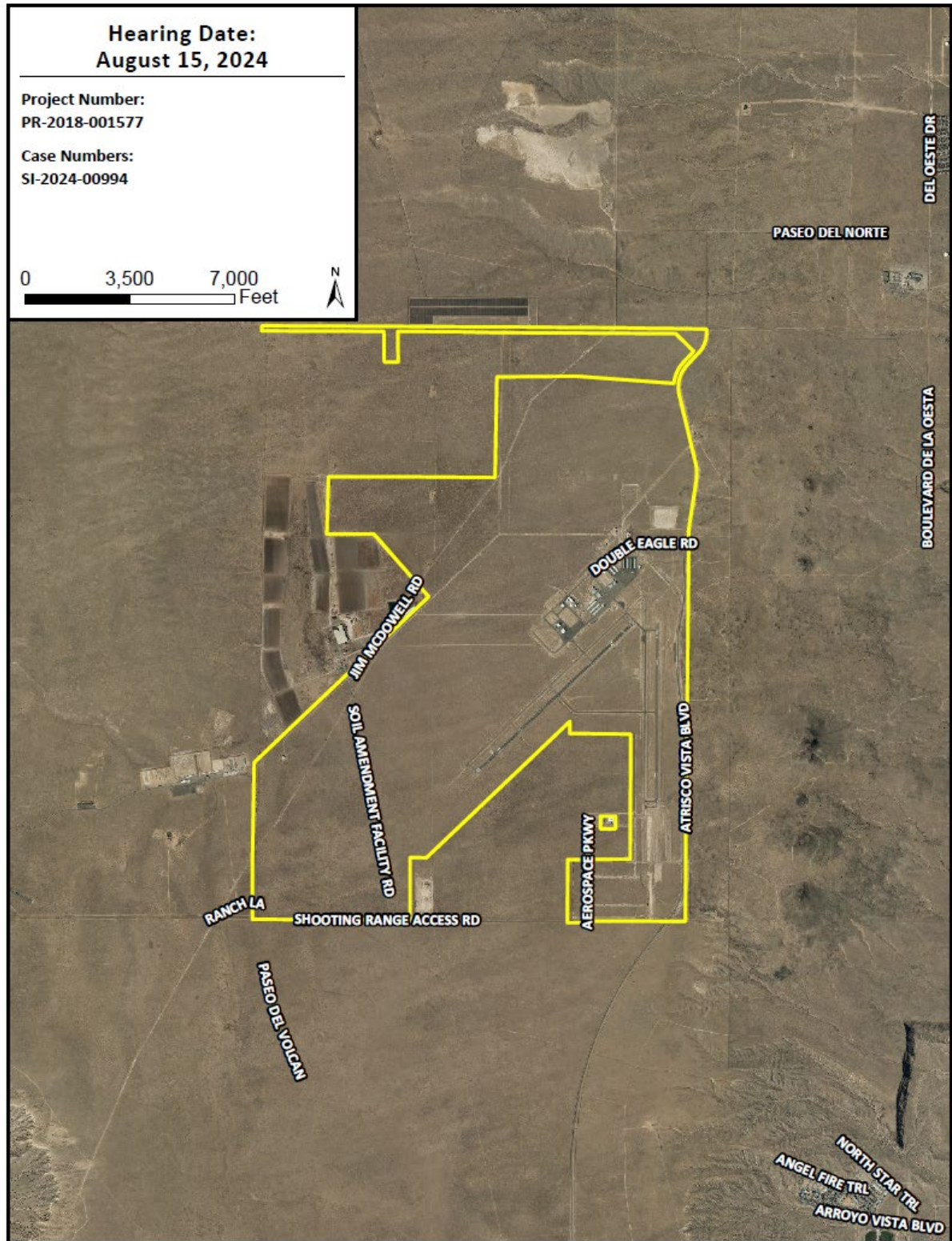
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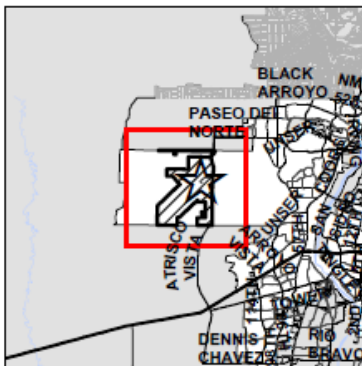
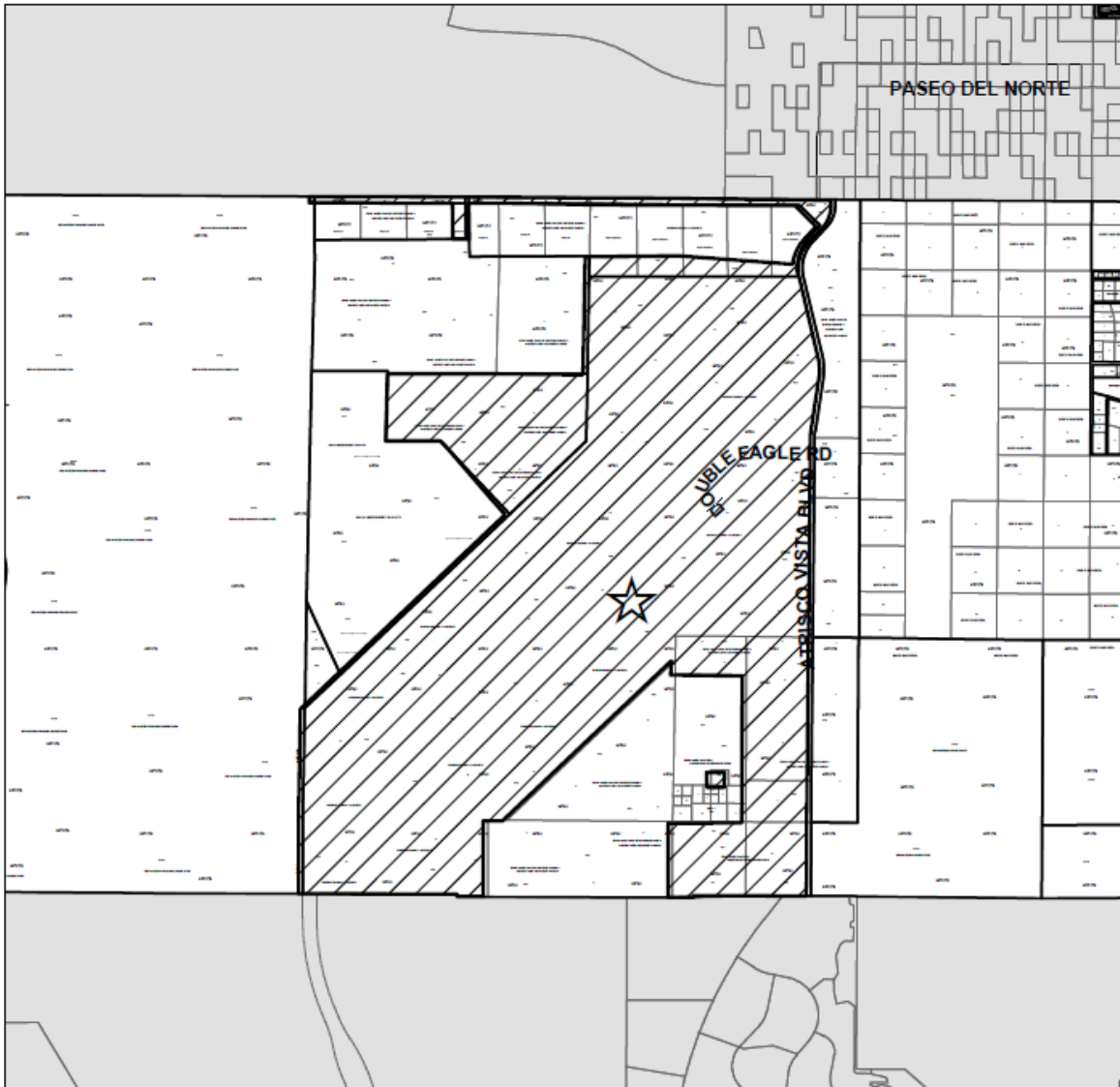
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I. Maps

Aerial Map



IDO Zoning Map



IDO ZONING MAP

Note: Gray shading
Indicates County.



1 inch = 4,000 feet

Hearing Date:

8/15/2024

Project Number:

PR-2018-001577

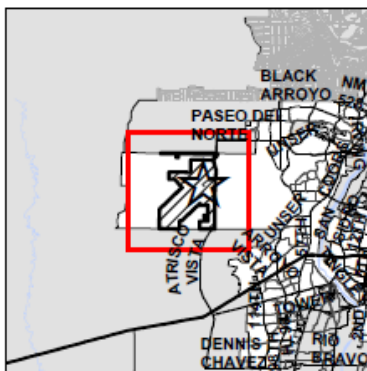
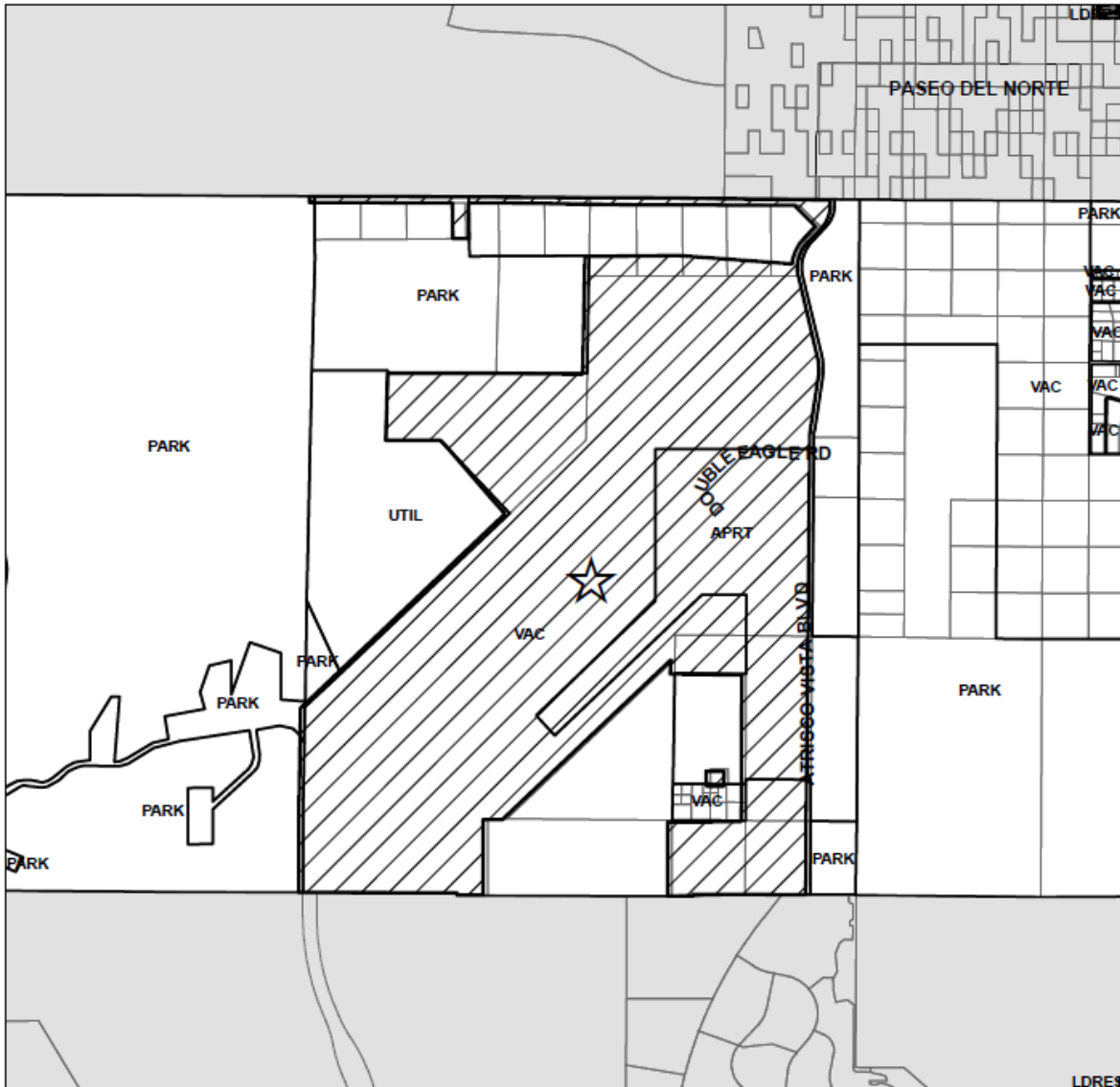
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C4-6, D4-6, E4-6 & F4-6

Land Use Map



LAND USE MAP

Note: Gray shading
Indicates County.

Key to Land Use Abbreviations	
LDRES Low-density Residential	APRT Airport
MULT Multi-family	TRANS Transportation
COMM Commercial Retail	AGRI Agriculture
CMSV Commercial Services	PARK Parks and Open Space
OFC Office	DRNG Drainage
IND Industrial	VAC Vacant
INSMED Institutional / Medical	UTIL Utilities
ED Educational	CMTY Community
	KAFB Kirtland Air Force Base



1 inch = 4,000 feet

Hearing Date:

8/15/2024

Project Number:

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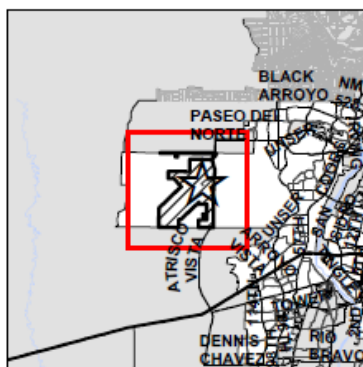
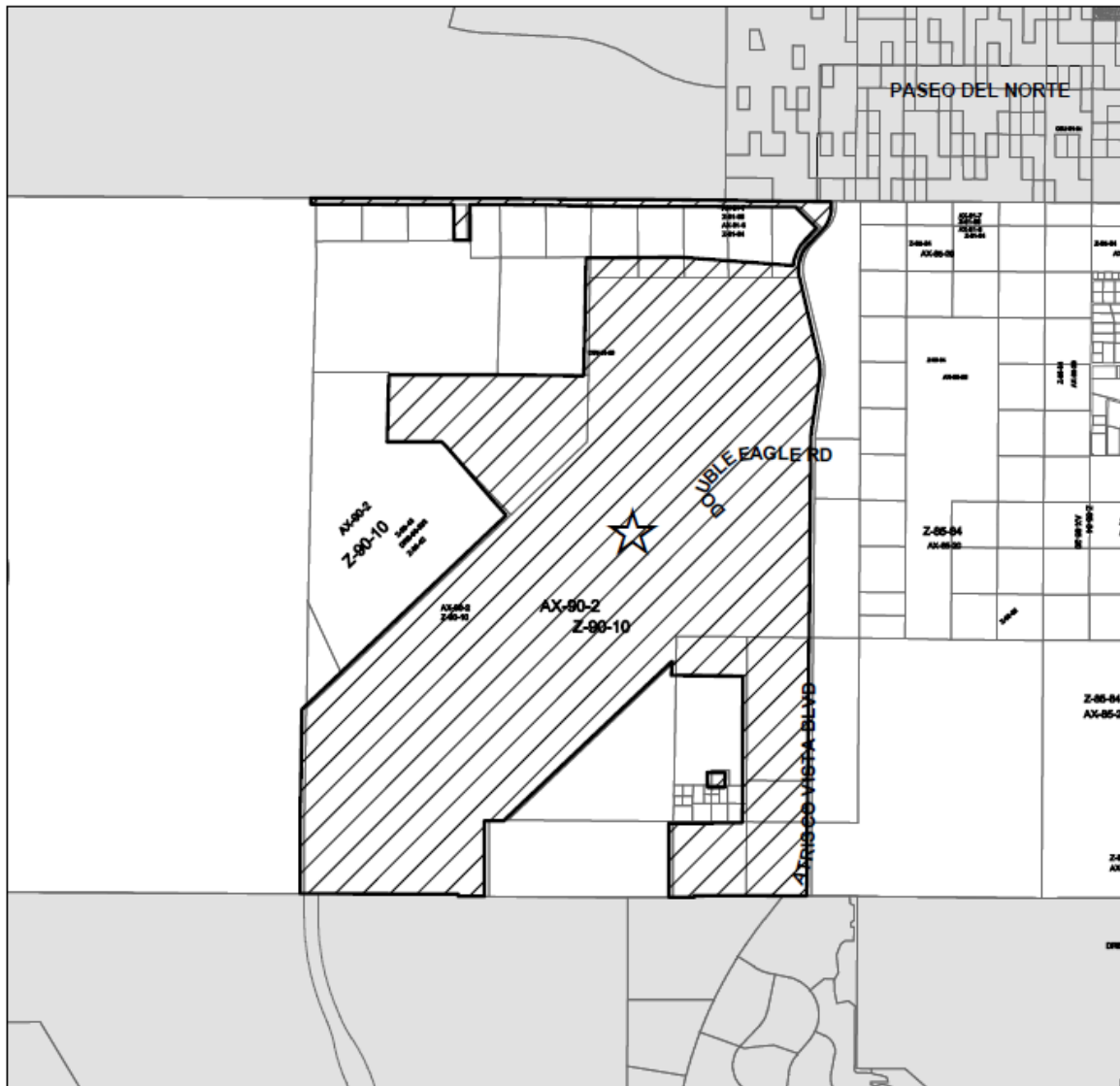
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History Map



HISTORY MAP

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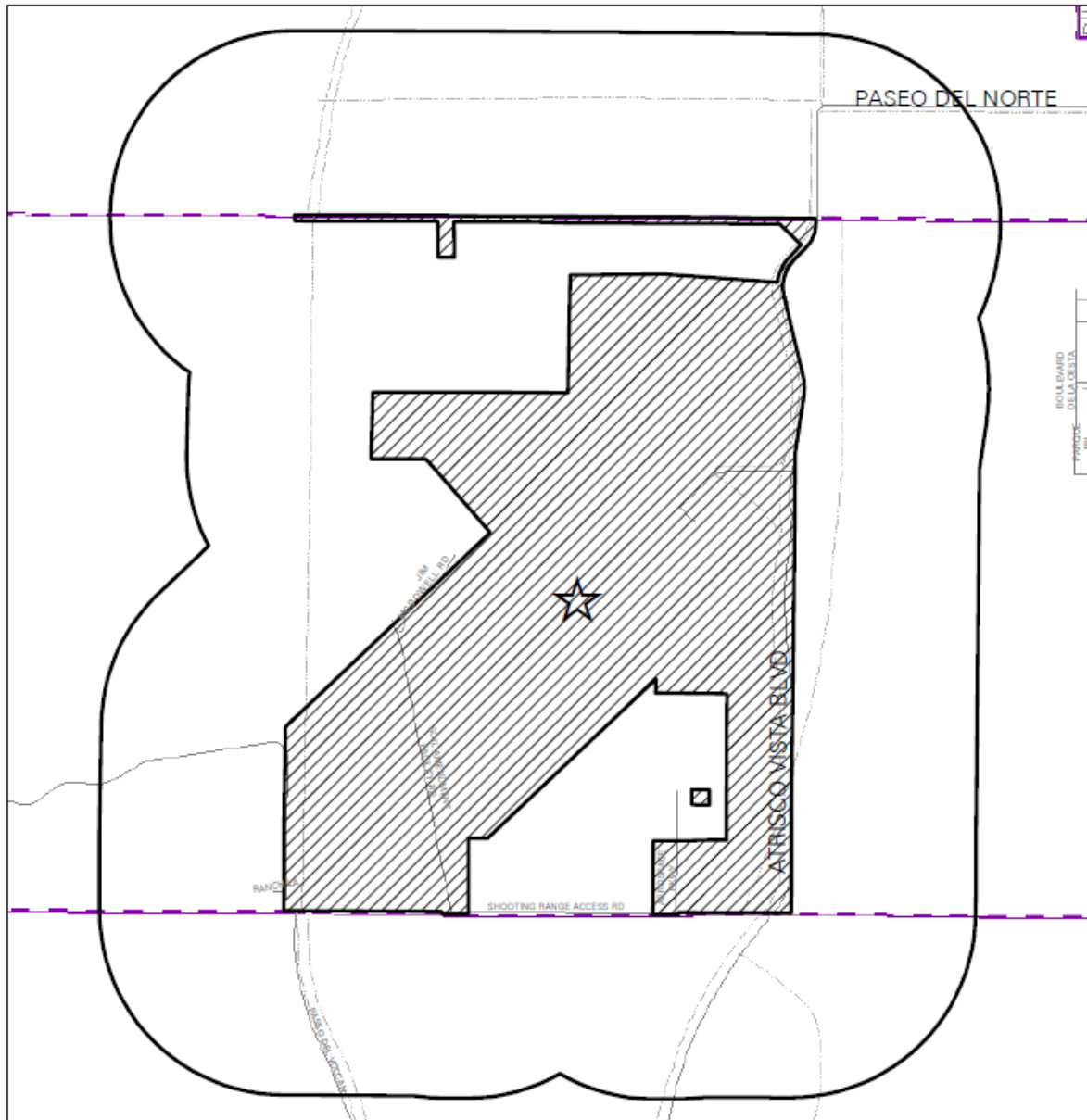


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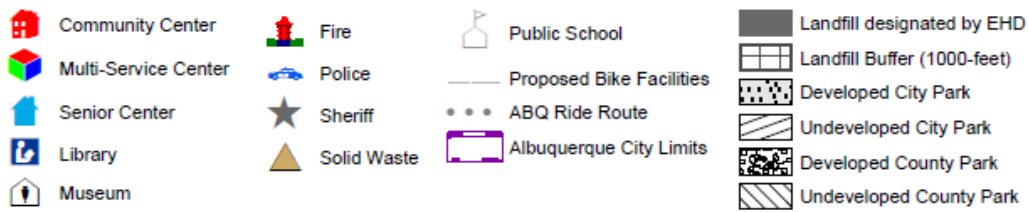
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Public Facilities / Community Services



Public Facilities Map with One-Mile Buffer



Project Number: PR-2018-001577

0 0.5 1 Miles

II. Introduction

	<i>Zoning</i>	<i>Comprehensive Plan Area</i>	<i>Land Use</i>
<i>Site</i>	NR-SU	Area of Change Area of Consistency	Airport and related uses Airfields, Open Space
North	NR-PO-B NR-PO-C N/A	Area of Consistency Area of Change Outside CABQ Limits	Parks & Open Space, Vacant, Airport related Parks & Open Space, Vacant Unincorporated Bernalillo County
South	NR-SU N/A	Area of Change Outside CABQ Limits	Vacant, Airport related City of Rio Rancho
East	NR-PO-B NR-SU	Area of Consistency Area of Consistency	Parks & Open Space Airport related
West	NR-PO-B	Area of Consistency	Utilities, Parks & Open Space

Request

This request is for a Site Plan – EPC for the Double Eagle II (DEII) Airport. The DEII Airport is an approximately 4,200-acre site located at 7401 Paseo Del Volcan NW, which sits to the NW of the Atrisco Vista Blvd NW and Shooting Range Access Rd intersection. This property is bounded by Major Public Open Space (MPOS), the City of Rio Rancho, and unincorporated Bernalillo County land (the “subject site”) and within a ¼ mile west of the Petroglyph National Monument. The subject site is about 8.5 miles northwest of Downtown Albuquerque (around 18 miles driving).

The request documents the existing, as-built airport facilities and proposes Design Standards required for the DEII Airport under the NR-SU zone district. The applicant is required to document the existing development in an “as-built” Site Plan to establish a baseline for any proposed future development and determinations for Design Standards. The NR-SU Site Plan would establish specific uses, site standards, and regulations for the Airport which are to be negotiated at the EPC through the EPC application and the Site Plan. A site plan is required for any development to occur under the NR-SU zone district for the Airport Land Use. The site plan shall be in compliance with the associated Master Plan for DEII.

EPC Role

The EPC is hearing this request (SI-2024-00994) pursuant to IDO §14-16-2-5(E) Non-Residential – Sensitive Use Zone District (NR-SU) and §14-16-6-6(I) Site Plan – EPC. The NR-SU zone district requires that a Site Plan is reviewed and decided by the EPC. The existing, as-built site conditions depicted in the proposed Site Plan shall remain valid until they are replaced (see IDO §14-16-1-10), which will require that any new development be

in compliance with the IDO. The EPC shall review the uses, standards, and regulations as negotiated by the applicant. This case is a quasi-judicial matter.

The EPC is the final decision-making body unless the EPC decision is appealed. If so, the Land Use Hearing Officer (LUHO) would hear the appeal and make a recommendation to City Council. The City Council would then make the final decision.

Context

The subject site consists of the Double Eagle II airport, a reliever airport for the Albuquerque International Sunport and classified as a Regional General Aviation Airport. The general purpose of this airport is to primarily serve general aviation activity with a focus on business activity. The property is governed by the Double Eagle II (DEII) Airport Master Plan and largely zoned NR-SU. The surrounding land use characterized primarily by open grasslands that are designated as vacant land.

Neighboring uses include the City of Albuquerque Shooting Range facility and Soils Amendment Facility to the west and soon to be one of the largest combined solar and storage projects in the United States adjacent and to the north of the subject site in Rio Rancho. Just to the south on Bernalillo County lands between the DEII and I-40, is a multi-story warehouse fulfillment center and other manufacturing facilities.

In general, the DEII Airport is situated between Major Public Open Space to the east, west, and due to the subject site's irregular shape, on portions in the north. The boundaries for the Petroglyph National Monument lie within 330 feet to the east of the subject site. The City of Rio Rancho abuts airport land on the north and unincorporated Bernalillo County lands borders the subject site on the south.

The primary access to the subject site is from Airport Rd NW off of Atrisco Vista Blvd NW, which is considered a critical link in the transportation system for the northwest subarea of metropolitan Albuquerque. The north-south Atrisco Vista Blvd NW falls within the DEII property designation. No facilities currently exist for pedestrian or alternative modes of transportation.

Roadway System

The Long-Range Roadway System (LRRS) map, produced by the Mid-Region Metropolitan Region Planning Organization (MRMPO), identifies the functional classification of roadways.

Atrisco Vista Blvd is an existing Regional Principal Arterial that currently exists as a two-lane facility extending from Central Avenue north to Paseo del Norte, and where Bernalillo County has undertaken the Atrisco Vista Boulevard study to evaluate roadway safety improvements and addition of a multi-use trail. The east-west Shooting Range Access Road is a Proposed Regional Principal Arterial.

The east-west Shooting Range Access Rd is a Proposed Regional Principal Arterial that changes to the Proposed Regional Principal Arterial of Paseo Del Volcan at the point it turns north to access the Shooting Range Park west of the airport. The Soil Amendment Facility Rd is a proposed Major Collector.

Airport Rd, which directs traffic into the airport, is an Existing Major Collector.

The MRCOG's 2040 Limited Access Facilities Map shows adjacent Paseo del Volcan roadway as a Limited Access Arterial.

The MRCOG's 2040 Primary Freight Corridors and Truck Restrictions Map does not designate roadways adjacent to or through the subject site as either a Primary Freight Corridor or a roadway with Truck Restrictions.

Bikeways / Trails

The Long-Range Bikeway System (LRBS) map, produced by the Mid-Region Metropolitan Planning Organization (MRMPO), identifies existing and proposed routes and trails.

A bicycle lane exists along Arroyo Vista Blvd north from Central Ave to the Double Eagle Rd NW, entrance to the airport. A proposed bike lane and paved trail is proposed for along both Paseo Del Volcan and Shooting Range Access Rd. Access Rd is a proposed paved trail and potential bike facility

Transit

The subject site is not served by public transit.

III. Airport Planning & the Federal Aviation Administration (FAA)

Most of the nation's airports were constructed and/or funded by the Federal government and developed based on Federal policy that promotes civil aviation and air commerce. The FAA's strategic plan, Destination 2025, contains goals regarding safety, efficiency, access, and sustainability. The National Plan of Integrated Airport Systems (NPIAS) supports the goals of Destination 2025 and is used by the FAA to administer its aviation programs. The DEII Airport is included in the NPIAS as a Reliever-Regional Airport. The purpose of a reliever airport is to provide an alternative to congested hub airport for general aviation activity.

The DEII Airport is also included in the New Mexico Airport Systems Plan (2009), a state document. The systems plan provides a tool for the New Mexico Department of Transportation (NMDOT) to monitor performance metrics, assess airport needs, justify funding for improvements, and provide information regarding the State's public use airports. The DEII Airport is classified as a Regional General Aviation Airport, the purposes of which are to primarily serve general aviation activity, with a focus on business activity.

FAA Standards

An airport master plan must be developed in accordance with FAA requirements. The FAA requires specific components within an airport master plan, which allow for a systematic and technical approach to facilities planning. Basically, these components include an inventory of existing facilities, a forecast of aviation demand, airport facility needs, development of alternative concepts, preferred concept, and implementation. The 2018 DEII Airport Master Plan includes these and more.

Airport Improvement Program

The FAA's Airport Improvement Program (AIP) is available to provide assistance to airports to help them develop sustainability plans. The City of Albuquerque received an AIP grant that helped support preparation of the *Albuquerque International Sunport Sustainable Airport Master Plan (2018)*. However, the AIP was not used for the 2018 DEII Airport Master Plan.

History & Background

Early History

The concept for the Double Eagle II (DEII) Airport was first conceived by the City of Albuquerque in 1969 in address concerns about future aviation demands in the Albuquerque area. The Metropolitan Airport Evaluation Committee (Metropolitan Airport Development Committee) was created to study the need for additional facilities through a joint City and County effort in 1970. A statewide Airport System Plan developed by consultants Bucher and Willis in 1972, again identified the necessity to develop additional aviation related facilities for the Albuquerque area.

Accordingly, Herkenhoff and Parsons conducted a master plan study in 1972 for the then named "West Mesa Airport" that would "service all segments of Albuquerque's air transportation requirements", including commercial and general aviation. The West Mesa Airport's initial site was four miles north of Interstate-40 and was intended to be a reliever airport for the Sunport. Although negotiations with the landowner of that site failed, an agreement was made with the then landowners of the current location, which at that time was part of the Bond Ranch.

An Environmental Assessment (EA) was completed in 1980 for the location of the DEII Airport, and the Federal Aviation Administration (FAA) found no significant impact. Construction for the new airport began in 1982 and was completed in 1983 with one runway. A second was added several years later.

The DEII Airport was categorized as a Regional General Aviation airport—airports that primarily serve general aviation activity with a focus on business activity including jet and turboprop aircraft—a reliever for the Sunport airport. The DEII Airport replaced several smaller, now closed, airports around Albuquerque, and was named for the first balloon to cross the Atlantic Ocean (Double Eagle II).

A second EA was published for the on-airport access road in 1994 that would connect the airport's landside facilities to an off-airport access road to the north. Paseo del Volcan Rd NW, the road bordering the airport on the west, was constructed in 1997.

In 1997, the EPC approved a request for annexation and rezoning of an approximate 11,756-acre area that included the lands of the DEII Airport, the Volcano Ranch Park, and holdings within the Petroglyph National Monument (Z-96-57/AX-96-8/EPC 6-20-96; see attached;). Zoning was changed from A-1 to SU-1 to be used for Airport and Related Facilities and Major Open Space.

DEII Airport Master Plan

The original DEII Airport Master Plan document was reviewed by the EPC and approved by City Council in 2002.

This document was created during a time of national growth, technology advancements, and increased private investments. Because of this, the 2002 Master Plan included suggestions for several ambitious improvements, such as the construction of a mid-field hangar area, runway renovations, taxiway reconstructions, building of an air traffic control tower, replacement of the electrical vault, and various infrastructure improvements. Since then, improvements at DEII include a connector taxiway between runways and construction of general aviation hangars (PR # 1001656; see attached).

The 2018 DEII Master Plan was reviewed by the EPC and then adopted by City Council in 2019 (R-19-169; see attached). The Master Plan outlines a 20-year development vision and included a facilities inventory, a needs assessment, a discussion of development alternatives, a forecast, an evaluation of development alternatives, and a Capital Implementation Plan (CIP) (PR # 1001656).

In 2023, a text amendment to incorporate Wireless Telecommunications Facilities (WTF) as a "Support Area Requirement" to the 2018 DEII Airport Master Plan was adopted by City Council (see PR-2018-00157/SI-2023-00537). WTFs are a Permissive Accessory use in the NR-SU zone district, and are proposed to be located outside of the runway protection sub area (IDO § 14-16-3-3).

On April 18, 2024 the EPC voted to forward a recommendation of approval for a text amendment to the DEII Airport Master Plan to allow non-aeronautical land uses on the airport property (PR #2018-001577_SI-2024-00321). On August 5, 2024 the City Council voted to accept the EPC's recommendation of approval (see R-24-71 attached; a signed copy is unavailable at the time of staff report posting).

Eclipse Aviation

Eclipse Aviation, a start-up aircraft manufacturer, announced in May of 2000 that it had chosen Albuquerque for its offices and as the site to produce its airplanes. Operations moved from the Sunport to DEII in 2006.

In January of 2002, the City Aviation Department was a party in a land swap between the City Open Space Division, the State Land Office, and the Black family. This land transaction, annexation, and associated rezoning consisted of 1,987 acres and is considered to be the start of preparations to bring Eclipse Aviation to DEII (# 01114-01874/ 01110-01875, finalized by City Council in July 2002).

The land exchange occurred in two phases. The first phase involved 899 acres that were requested for annexation and SU-1 zoning: 727 acres were to be zoned SU-1 for Airport and Related Uses, 172 acres were to be SU-1 for Major Public Open Space. The second phase consisted of annexing 1,088 acres, also with a request for a zone change. Of that, 724 acres were to be changed from SU-1 for Major Public Open Space to SU-1 for Airport and Related Uses and 364 acres were to be changed from SU-1 for Airport Uses to SU-1 for Major Public Open Space. The final land ownership configuration has created a buffer strip of open space along the eastern edge of the airport that abuts the Petroglyph National Monument.

The administration and the City Council agreed to serve the airport with water and waste water lines, which was part of the lease agreement with Eclipse Aviation, which was anticipated to be a major employer by 2007. It was evident from bond actions and a property tax abatement from the administration and the City Council mid-2004 that further development of DEII was a priority.

Other Related Projects

In February 2002, the EPC recommended approval to the City Council of a request for annexation of approximately 900 acres of land, zoned County A-1, to create a portion of the present-day site of the Double Eagle II airport (#1001656/ 01114-01874 and 01110-01875). At this time SU-1 zoning for Airport and Related Facilities and SU-1 zoning for Major Public Open Space was established.

In February 2003, the EPC approved a site development plan for a building permit for the DEII Airport to allow the construction of a water and waste water facility to serve the airport. The water system consisted of facilities in three different locations of the airport (#1001656/02EPC-01956; see attached).

In July 2003, the EPC approved a zone map amendment (zone change) and a site development plan for subdivision of approximately 590 acres located northwest of the DEII Airport on Major Public Open Space. The City's Open Space Division and the State Land Office entered into an agreement to exchange land (#1001656/03EPC-00915 and 00916; see attached).

On January 15, 2004 the EPC approved a request for a Site Development Plan for Subdivision with design standards for a 300-acre site, including Tracts D and a portion of Tract S. The site was intended to be the location of Eclipse Aviation on 160-acres with the other 150-acres subdivided into 1-10 acre lots. On December 8, 2004 the DRB signed

off on the Site Plan for subdivision for the Aerospace Technology Park (PR 1003125_03EPC-02054)

On May 5, 2005 the EPC approved a Site Plan for Subdivision for the Aerospace Technology Park for Tract D-1 within the site plan for subdivision property. This site plan is still valid and includes approximately 150-acres originally zoned SU-1 for airport and related facilities. The Site plan for Subdivision includes design standards for the area. This parcel became the location of Eclipse Aviation (PR # 1003125_05EPC-00566).

Several administrative amendments have been approved for the Eclipse Aviation Campus for a public access easement and to change some design standards, a subdivision of land.

In April 2008, zoning approved the Alltel co-location on an existing KOB-tv tower in (#1003376; see attached).

In 2018 an Archeological Certificate was requested for the subject site (PR-2018-001577).

IV. Analysis of City Plans and Ordinances

Albuquerque / Bernalillo County Comprehensive Plan (Rank 1)

Comprehensive Plan Designations

Approximately one-quarter of the subject site is in an Area of Consistency and the remaining, approximately three-quarters of the undeveloped land surrounding the subject site is in an Area of Change as designated by the Comprehensive Plan. Most of the vacant land within the DEII Airport area is characterized as an Area of Change. Areas of Change are intended to be the focus of urban-scale development that benefits job growth and housing opportunities.

Areas of Consistency have policies to protect and enhance the character of existing single-family neighborhoods, areas outside of Centers and Corridors, parks, and Major Public Open Space, and in this case, helps preserve such critical airport facilities as the runways and other critical infrastructure.

The Comprehensive Plan designates the DEII Airport subject site (and the Atrisco Vista Shooting range to the south) as a Developing Employment Center. Employment Centers were designated to offer a range of employment opportunities and remain predominately industrial, business, and retail centers with industrial, commercial, and office activity. Employment Centers are imagined as auto-oriented with excellent access for trucks and freight connections located near major intersections, along highways, or major arterials.

Atrisco Vista Blvd is designated as a Commuter Corridor. Commuter Corridors prioritize automobile travel in street design and improvements by allowing higher traffic speed and volume, and maintaining a high level of service for automobiles so they can make long

distance trips to regional destinations (i.e., limited-access roadways). Access for pedestrians is managed by signalized intersections.

The subject site lies within the western portion of the West Mesa Community Planning Area (CPA) which is bounded by I-40 on the south, the Rio Grande to the east, Atrisco Vista Boulevard and the city edge on the west, and Montañó Rd and Gila Rd to the north. It is characterized by its proximity to the Northwest Mesa Escarpment, Petroglyph National Monument, and the Bosque.

Applicable Goals & Policies

CHAPTER 4: COMMUNITY IDENTITY

POLICY 4.1.5 NATURAL RESOURCES: Encourage high-quality development and redevelopment that responds appropriately to the natural setting and ecosystem functions.

This request is for an as-built Site Plan that also documents proposed Design Standards. The site is surrounded to the east and west by MPOS and includes the Petroglyph National Monument. As the Applicant stated, the “DEII Design Standards are integral to establishing an aesthetic character compatible with the surrounding context, particularly the City of Albuquerque Open Space and Petroglyph National Monument.”

The subject site is located in a generally undeveloped, desert grassland with expansive views to the mountains to the north (Sangre de Cristos), east (Petroglyph National Monument volcanoes, Sandias), southeast (Manzanos), and west (Mt Taylor). NR-SU zone district Design Standards may be varied in the NR-SU approval process (see IDO Table 2-5-9: Other Applicable IDO Sections, p. 54) with Dimensional Standards determined in the approval of a Site Plan – EPC (see IDO §14-16-5-1(E)(2)(b)). Proposed DEII Airport Design Standards generally conform to IDO design standards (e.g., Access and Connectivity, Subdivision of Land, Parking and Loading, Solar access, and Signs) that work to help reduce development impact on Major Public Open Space (MPOS), such as suggesting a color palette and light reflective values (LRV) recommended for Sensitive Lands (see IDO §14-16-5-2) for buildings within 250 feet of MPOS. It also incorporates active and passive water harvesting, passive heating and cooling structure design, permeable pavements, less linear landscape lines, and using vegetation to help slow and filter runoff. However, any significant development to the scale proposed will increase light, noise, traffic, pollution, temperatures, and habitat fragmentation as well as impact natural drainage and views to culturally important landscape features to and from the Petroglyphs National Monument and Major Public Open Space.

Greater allowances for building setbacks (150 feet from MPOS) and requiring screened parking areas, should help lower development impact in terms of views from MPOS to the subject site, however, staff disagrees that the proposed building height maximums of 60 feet within a 150-foot setback from Atrisco Vista Blvd and 85 foot maximum for

all other areas respond appropriately to the natural setting despite the addition of sustainable caveats such as orienting building to overlook MPOS in combination with passive and cooling. The proposed heights seem out of scale with area open space features and appears to be designed to compete with the volcanos themselves.

Adding trees to the Atrisco Vista Blvd ROW also seems out of character with the natural setting and ecosystem functions.

The request is partly consistent with Policy 4.1.5 Natural Resources.

POLICY 4.2.2 COMMUNITY ENGAGEMENT: Facilitate meaningful engagement opportunities and respectful interactions in order to identify and address the needs of all residents.

According to the Office of Neighborhood Coordination (ONC), there are no designated Neighborhood Associations within the required notification buffer areas. However, after the general required notification of property owners within 100 feet of the proposed Site Plan – EPC request, City Aviation reached out to Neighborhood Associations/Coalitions and other interested parties for a Public Open House held June 28, 2024. At this time, City Aviation provided information to solicit feedback regarding the proposed as-built Site Plan and Design Standards being brought before the EPC. Attendance included representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation, tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature (see Attachments). Comments collected during the meeting resulted in some Design Standards revisions. Updates of project description and were also resent to adjacent property owners and emailed to neighboring Neighborhood Associations/Coalitions.

However, the fact that this request is less than 330 ft from the Petroglyphs which is widely known as a significant sacred site for most of the area's Pueblos and Tribes, combined with the fact that only Pueblo of Laguna attended the Open House, would not be consistent to meaningful engagement and respectful interactions to identify and address the needs of all residents.

The request is partly consistent with Policy 4.2.2 Community Engagement

GOAL 4.2 PROCESS: Engage communities to identify and plan for their distinct character and needs.

Although not required by IDO regulations, City Aviation reached out to Neighborhood Associations/Coalitions and other interested parties for a Public Open House held June 28, 2024 to engage communities and help identify and plan for the distinct character and needs of the DEII Airport Facilities. Solicited feedback regarding the proposed as-built Site Plan and Design Standards contributed to proposed Design Standards revisions. **The request is generally consistent with GOAL 4.2 Process.**

CHAPTER 5: LAND USE

GOAL 5.1 CENTERS AND CORRIDORS: Grow as a community of strong centers connected by a multi-modal network of Corridors.

The subject site is located within a Developing Employment Center along the Regional Principal Arterial and Commuter Corridor, Atrisco Vista Blvd NW that prioritizes automobile travel in street design and improvements by allowing higher traffic speed and volume, connecting to I-40 about 4.5 miles to the south. It also is surrounded to the east and west by Major Public Open Space and includes the Petroglyph National Monument within 330 feet. The request codifies an as-built DEII Airport Site Plan and Design Standards to allow for future site development at the subject site. Adding potential employment sites to the West Side of the City would allow for the addition of jobs and employment opportunities west of the Rio Grande and could help improve the job-housing balance on that side of town. **The request is consistent with Goal 5.1 Centers and Corridors.**

POLICY 5.1.1 DESIRED GROWTH: Capture regional growth in Centers and Corridors to help shape the built environment into a sustainable development pattern.

The subject site is located within a Developing Employment Center and along a Regional Principal Arterial/ Commuter Corridor of Atrisco Vista Blvd NW. Allowing growth within a Developing Employment Center promotes a more sustainable development patterns and could reduce the job-housing imbalance west of the river as articulated in the ABC Comp Plan, especially as the City's West Side continues to sprawl. **The request is consistent with Policy 5.1.1 Desired Growth.**

POLICY 5.1.2: DEVELOPMENT AREAS: Direct more intense growth to Centers and Corridors and use Development Areas to establish and maintain appropriate density and scale of development within areas that should be more stable.

The subject site is located mostly within an ABC Comp Plan designated Area of Change, with the Areas of Consistency consisting of integral DEII Airport functions. The request will result in an as-built Site Plan documenting existing airport facilities and codifying Design Standards which will help guide growth and establish development density and scale for an area intended for higher intensity uses. **The request is generally consistent with Policy 5.1.2 Development Areas.**

POLICY 5.1.5 EMPLOYMENT CENTERS: Create Centers that prioritize employment and foster synergy among businesses.

The subject site is located within a Developing Employment Center and along a Regional Principal Arterial/ Commuter Corridor of Atrisco Vista Blvd NW. Allowing growth within a Developing Employment Center helps prioritize employment and foster synergy among businesses in an area with good automobile access and good connectivity for freight routes. The proposed DEII design standards also consider pedestrian and bicycle

access within the Developing Employment Center. **The request is consistent with Policy 5.1.5 Employment Centers.**

GOAL 5.3 EFFICIENT DEVELOPMENT PATTERNS: Promote development patterns that maximize the utility of existing infrastructure and public facilities and the efficient use of land to support the public good.

The proposed request promotes internal development patterns that maximize the utility of existing infrastructure and public facilities by documenting as-built DEII Airport structures and associated facilities, as well as compiling Design Standards for future proposed development at the subject site within a Developing Employment Center to better maintain a more cohesive future development. As this request does not propose any development, there will be no changes to the subject site layout which preserves current efficiencies in land use to support the public good. The request is generally consistent with Goal 5.3- Efficient Development Patterns. **The request is consistent with Policy 5.1.5 Employment Centers.**

POLICY 5.3.4 CONSERVATION DEVELOPMENT: Encourage conservation development to promote private open space and preserve natural landscape, agricultural lands, and other features of the natural environment to encourage development that is sensitive to the open, natural character of the area and the geological and cultural conditions.

Staff agrees with the Applicant that given the large area of DEII and the need to prioritize airport operations, the DEII Design Standards should concentrate new development within a prescribed area and preserve the surrounding natural landscape which consists of a wide-open landscape with expansive views in all directions.

This request proposes landscaping, screening, and buffering standards, especially along parking lots, in areas with necessary but “less attractive” development, such as, loading, service, mechanical equipment, parking areas, and refuse facilities which should minimize visual impact from MPOS and Atrisco Vista Blvd. The suggested continuous line of 3-to-4-foot evergreen shrubbery with a minimum of 6-foot-wide buffer could harmonize with available native species (e.g., Atriplex sp) and preserve the natural landscape and local views, however, a dark green evergreen, such as decorative juniper will be out of place and inappropriate for the surrounding landscape. The addition of trees in landscape that emphasizes visitor and employee centric spaces and walkways can potentially be appropriate in certain areas, but not e along Atrisco Vista Blvd, especially on the east side since it will potentially change the character of the landscape.

Because the addition of fencing impacts view distances, opaque fencing should retain the feeling of Open Space and the viewshed, while helping maintain the built aesthetic.

Proposed color schemes and light reflectivity standards that echo the surrounding natural environment shall reduce the visual impacts of new development. Again, the subject site is located in a generally flat, gradually sloped and undeveloped, desert

grassland with expansive views to the north (Sangre de Cristos), east (Petroglyph National Monument volcanoes, Sandias), southeast (Manzanos), and west (Mt Taylor), and greater building setback allowances should help lower development impact in terms of views from MPOS to the subject site as this will bar structures and buildings at the property lines. Staff disagrees that the proposed building height maximums of 60 feet within a 150-foot setback from Atrisco Vista Blvd and 85 foot maximum for all other areas responds appropriately to the natural setting. Nevertheless, proposed Design Standards encourage tenants to orient buildings to allow for views, especially of the Petroglyph National Monument.

Any significant development to the scale proposed will increase light, noise, traffic, temperature, and habitat fragmentation as well as impact views to culturally important landscape features. **The request is not consistent with Policy 5.3.4 Conservation Development.**

GOAL 5.6 CITY DEVELOPMENT AREAS: Encourage and direct growth to Areas of Change where it is expected and desired and ensure that development in and near Areas of Consistency reinforces the character and intensity of the surrounding area.

Although the subject site is already developed as a Regional General Aviation airport—a reliever for the Sunport airport—that primarily focuses on business activity, lease areas within the airport tracts lie an Area of Change where development is expected and desired. The proposed Design Standards encourage and direct growth to Areas of Change where it is expected and desired and generally ensure that development in and near Areas of Consistency reinforces the character and intensity of the surrounding area. Proposed building height maximums and proposed trees along public ROW however, would change the character and intensity of land use considerably. **The request is partly consistent with Goal 5.6- City Development Areas.**

POLICY 5.6.2 AREAS OF CHANGE: Direct growth and more intense development to Centers, Corridors, industrial and business parks, and Metropolitan Redevelopment Areas where change is encouraged.

The proposed Design Standards being submitted as part of this request for an as-built Site Plan outlines potential future growth and more intense development in Areas of Change within a Developing Employment Center. This could encourage development that expands employment opportunities and job creation within the facility boundaries. At this stage of development of the west side of the Petroglyph National Monument, there will be minimal impacts of development on existing residential uses regarding noise, stormwater runoff, contaminants, lighting, air quality, and traffic. However, with the planned growth development impacts in general will increase in what are currently undeveloped natural habitats. **The request is generally consistent with Policy 5.6.2 Area of Change.**

POLICY 5.7.2 REGULATORY ALIGNMENT: Update regulatory frameworks to support desired growth, high quality development, economic development, housing, a variety of transportation modes, and quality of life priorities.

There is no DEII Airport Site Plan or Design Standards relative to the NR-SU zone district that is associated with the current Airport Master Plan. The proposed subject site Design Standards should provide a clearer regulatory framework and processes for any proposed future development to better support desired growth and both high quality employment and economic development. **The request is consistent with Policy 5.7.2 Regulatory Alignment.**

CHAPTER 7: URBAN DESIGN

POLICY 7.3.1 NATURAL AND CULTURAL FEATURES: Preserve, enhance, and leverage natural features and views of cultural landscapes.

The proposed site designs address development issues to preserve and leverage natural features and views of cultural landscapes with development setback provisions, specifying appropriate color choices and building materials to blend in with the natural landscape, adding low landscape buffers and appropriate screening to less aesthetically pleasing development, requiring more subdued and consolidated signage, burying all electric distribution lines, and eliminating light trespass beyond the property line or allowing light to be visible from MPOS except where FAA regulations may vary.

However, staff disagrees that the proposed Design Standards with building height maximums of 60 feet within a 150-foot setback from Atrisco Vista Blvd and an 85-foot maximum for all other areas preserve, enhance, and leverage natural features. As such heights will compete with the elevations of the nearby volcanos, and would probably be visible from the City's east side, similar to the current warehouse fulfillment center to the south of the DEII Airport just outside of the City. Such heights would also be the first impression on entry from the west into the City on the highway and would be the features versus leveraging the neighboring volcano features.

Added trees to public ROWs, especially along Atrisco Vista Blvd would not maintain the natural conditions of the natural landscapes within the DEII parcels and would not provide an appropriate transition to adjoining Major Public Open Space. **The request is partly consistent with Policy 7.3.1 Natural and Cultural Features.**

GOAL 7.5 CONTEXT-SENSITIVE SITE DESIGN: Design sites, buildings, and landscape elements to respond to the high desert environment.

Staff agrees that the natural terrain and open landscapes of arid grassland of the DEII Airport and surrounding West Mesa offers spectacular open views in all directions from the Petroglyph National Monument Volcanos to distant mountains (e.g., Sangre de Cristos, Sandias, Manzanos, Mt Taylor). Because of this, it is notable that the proposed DEII Design Standards prioritizes maintaining a natural landscape wherever possible.

The fact that the proposed Design Standards center sustainable practices, including Green Stormwater Infrastructure (GSI) and Low Impact Development (LID) practices for site design, buildings, and landscape elements to respond to the high desert environment are also important. For the most part, sustainable, context-sensitive Site Design has been proposed. However, Staff contends that the proposed building heights and tree-lined ROWs will negatively impact views to existing landscape elements. Buildings reaching maximum heights of 60 or 85 feet, although in effect represent denser development, will not preserve the open vistas. Within recent history, trees do not naturally contribute to the area's native vegetation and will block views to the natural high desert environment. **The request is not consistent with Goal 7.5 Context-Sensitive Site Design.**

POLICY 7.5.1 LANDSCAPE DESIGN: Encourage landscape treatments that are consistent with the high desert climate to enhance our sense of place.

Some of the proposed landscape treatments are consistent with the high desert climate. For example, where not in conflict with FAA safety regulations or aviation functionality, the DEII Design Standards encourage active and passive water harvesting techniques to slow and capture stormwater on site as well as to supplement watered landscaped areas as natural drainage is modified.

Although the Design Standards encourage the use of native plant materials that enhance our sense of place, regionally adapted plants that will thrive in local conditions with less fertilizer, water, and maintenance are also proposed, the latter of which do not match adjacent landscapes and could be invasive.

The addition of trees along the public ROW would not be consistent with the microclimate of the site location or the dramatic setting of the West Mesa, but if restricted to visitor and employee centric spaces to create moderated microclimates, trees could help allow for the high desert climate in adjoining spaces, and with that, our sense of place.

The request is partly consistent with Policy 7.5.1 Landscape Design.

CHAPTER 11: HERITAGE CONSERVATION

GOAL 11.3 CULTURAL LANDSCAPES: Protect, reuse, and/or enhance significant cultural landscapes as important contributors to our heritage and rich and complex identities.

The proposed DEII Airport Design Standards aim to protect the significant cultural landscapes that surround the airport. These critical landscapes include Major Public Open Space and the striking volcano cones and cultural remains protected by the Petroglyph National Monument, all of which contribute to the area's heritage as well as our rich and complex identities. Accordingly, all future development shall be pursuant to Archaeological Sites regulations of the IDO Site Design and Sensitive Lands (see §14-16-5-2(D)(1)).

And although the proposed DEII Design Standards take into consideration these remarkable westside viewsheds and exceed IDO standards, where feasible, additional measure can be taken to minimize the impact of development:

1) Reduce building heights, 2), Increase building density, 3) Constrain development to only the west side of Atrisco Vista, 4) Eliminate the ROW tree requirement along Atrisco Vista and most ROW area; plant trees only in areas with dense development and regular outdoor use or to minimize heat as part of hardscapes, 5) Encourage shade structures, 6) Require shading over parking, 6) Require solar hookups, and 6) When developing land that has not already been bladed, remove plants prior to ground breaking to reuse in landscaping.

Proposed protective DEII Design Standards include minimizing the visibility of structures through color and building material restrictions, minimum building and parking setbacks, adding screening, wall, and fencing requirements. Additionally, building setbacks have been added for MPOS and Atrisco Vista Blvd that are greater than those required by the IDO to minimize impacts to MPOS and the Petroglyph National Monument.

The request is not consistent with Goal 11.3 Cultural Landscapes.

POLICY 11.3.4 PETROGLYPH NATIONAL MONUMENT: Regulate adjacent development to protect and preserve the Petroglyph National Monument – its volcanoes, petroglyphs, and Northwest Mesa Escarpment – as a priceless cultural landscape and community resource that provides physical, cultural, and economic benefits.

The West Side is expanding and the DEII Airport is within a designated Employment Center. The proposed DEII Design Standards attempt to protect the Monument from growth and development pressures by concentrating leased land development primarily to the southwest corner of the property. Additional protective DEII Design Standards include minimizing the visibility of structures through color and building material restrictions, minimum building and parking setbacks, adding screening, wall, and fencing requirements. Additionally, building setbacks have been added for MPOS and Atrisco Vista Blvd that are greater than those required by the IDO to minimize impacts to MPOS and the Petroglyph National Monument.

However, the proposed structure height maximums of 60 feet between 150 and 250 feet from the Atrisco Vista ROW and up to 85 feet in all other areas (provided that they comply with the FFA FAR Part 7 regulations) are incongruent with the area's open grasslands and the Petroglyph National Monument. With these building heights, views to and from those spaces would not be preserved. Trees added to the Atrisco Vista Blvd ROW would also change the character of the landscape and not preserve the natural open spaces that characterize the area. Instead of the volcanos, any future proposed development would act as a visual reference point, rendering edge treatments and

transitions as inappropriate. **The request is not consistent with Goal 11.3 Cultural Landscapes.**

Integrated Development Ordinance (IDO)

The application for this request was submitted subsequent to the effective date on May 17, 2018 of the Integrated Development Ordinance, and is therefore, subject to its regulations and processes. However, since this Site Plan request is to document pre-IDO development, that development will continue to operate as nonconforming structures and are not subject to the requirements of the effective IDO until any future development is proposed (see IDO §14-16-1-10, §14-16-6-4(Z), §14-16-6-8).

Pursuant to IDO §14-16-4-1(A)(4)(b), the NR-SU zone district allows primary uses not allowed in any other zone district as indicated in IDO §14-16-2-5(E)(2) (Use and Development Standards). Uses approved for each property and any related standards are documented in the approved Site Plan for that property on file with the City Planning Department.

IDO Zoning

The subject site is zoned NR-SU (Non-Residential – Sensitive Use Zone District). Prior to the adoption of the IDO the subject site was zoned SU-1 for Airport and Related Facilities. The purpose of the NR-SU zone district is to accommodate highly specialized public, civic, institutional, or natural resource-related uses that require additional review of location, site design, and impact mitigation to protect the safety and character of surrounding properties. Pursuant to IDO §14-16-2-5(E)(2) Airports require an NR-SU zone designation. District Standards can be found in IDO §14-16-2-5(E)(3).

Lots Adjacent to Major Public Open Space

The subject site is adjacent to Major Public Open Space along much of its borders and shall follow the requirements outlined in IDO §14-16-5-2(J) Major Public Open Space Edges. These protections include additional regulations relating to but not limited to building height bonuses, access and connectivity, landscaping, buffering, and screening, outdoor lighting, colors, and signing.

Overlay Zones

A large portion of the subject site falls within the boundaries of the Airport Protection Overlay Zone (IDO 14-16-3-3), which includes the Air Space Protection Sub-area, Runway Protection Sub-area, and the Noise Contour Sub-area.

The purpose of the Airport Protection Overlay (APO) zone is to require that land use and development at or around public airport facilities comply with the regulations of the Federal Aviation Administration (FAA) that protect the public from noise, vibration, and hazard impacts of airport operations and that preserve the safety of aircraft operators. In the case of a conflict between the provisions of a Zone District and the provisions of

an Overlay Zone, the provisions of the Overlay Zone shall prevail. Where an Overlay Zone is silent, IDO requirements apply.

Definitions

Abut: To touch or share a property line.

Adjacent: Those properties that are abutting or separated only by a street, alley, trail, or utility easement, whether public or private.

Airport: The area of land used or intended to be used for the landing and takeoff of aircraft, passenger and cargo loading areas, and related uses.

Development: Any activity that alters the ground or lot lines on a property. Development may include subdivision of land; construction of buildings, structures, or streets; installation of landscaping, infrastructure, utilities, or site features; and/or activities to prepare land for such construction or installation, such as grading. For the purposes of the IDO, this term includes new development and redevelopment on existing lots.

Employment Center (EC): An area designated as an Employment Center in the ABC Comp Plan, as amended.

Major Public Open Space: City-owned or managed property that is zoned NR-PO-B or City-managed property that is zoned NR-PO-C, including the Rio Grande State Park (i.e. the Bosque), Petroglyph National Monument, and Sandia foothills. These are typically greater than 5 acres and may include natural and cultural resources, preserves, low-impact recreational facilities, dedicated lands, arroyos, or trail corridors. The Rank 2 Major Public Open Space Facility Plan guides the management of these areas. For the purposes of this IDO, Major Public Open Space located outside the city municipal boundary that is mapped as Open Space in the ABC Comp Plan still triggers Major Public Open Space Edge requirements for properties within the city adjacent to or within the specified distance of Major Public Open Space.

Site Plan: An accurate plan that includes all information required for that type of application, structure, or development.

Relevant Rank 3 Plan

The subject site is governed by the 2019 Double Eagle II Airport Master Plan. Master Plans and land uses are voluntary for the facility, but the Master Plan and Site Plan are meant to be complimentary. As it stands the Master Plan only includes an "Airport Layout Drawing." There have been several Administrative Site Plans for the airport which have been approved, but this is not consistent with the IDO nor the NR-SU zone district.

V. Site Plan – EPC

The request is for a Site Plan – EPC to document the DEII Airport (a pre-2018 IDO, as-built development). Pursuant to IDO §14-16-6-6(l)(3) of the Integrated Development Ordinance, Review and Decision Criteria, an application for a Site Plan – EPC will be approved if it meets all of the criteria presented in 6-6(l)(3)(a-g).

6-6(l)(3)(a) The site plan is consistent with the ABC Comp Plan, as amended

As demonstrated by the policy-based analysis of the proposed Site Plan, the request is generally consistent with applicable Comprehensive Plan Goals and policies.

6-6(l)(3)(b) The Site Plan is consistent with any applicable terms and conditions in any previously approved NR-SU or PD zoning covering the subject property and any related development agreements and/or regulations.

The subject site is zoned NR-SU. There is not an NR-SU Site Plan on file for the DEII Airport Property in its entirety. There is a Site Development Plan for Subdivision with Design standards that is valid for a 300-acre portion of the airport property which includes the Site Plan for Eclipse Aviation. This SDP for Subdivision governs these specific tracts within the Airport, which is noted on the NR-SU site plan: Tract F Bulk Land Plat of Double Eagle II Airport and Adjacent Lands; Tract D-1-A-1, Tract S-1-A, and the SW'LY Portion of Tract D-1-A-1, Plat of Tracts D-1-A-1, D-1-A-2 & S-1-A of amended bulk land plat for Aerospace Technology Park; Tract D-1-A-2 and the Southeasterly portion of Tract D-1-A-1, Plat of Tracts D-1-A-1, D-1-A-2 & S-1-A of amended bulk land plat for Aerospace Technology Park; and LTS 1-15 Bulk Land Plat of lots 1 - 15 Aerospace Technology Park, Unit 1. No other related development agreements and/or regulations are known to be in place for the project site.

6-6(l)(3)(c) The Site Plan complies with all applicable provisions of this IDO, the DPM, other adopted City regulations, and any terms and conditions specifically applied to development of the property in a prior permit or approval affecting the property.

Staff has reviewed the proposed as-built Site Plan for documentation purposes. The existing development and uses for the airport at the subject site were established prior to the adoption of the 2018 IDO, therefore, current, as-built development will be held to Pre-IDO Approvals pursuant to IDO §14-16-1-10 and IDO §14-16-4-1(E). Current land uses, buildings, lots, signs, and site features may become nonconforming due to the adoption of the IDO and will be pursuant to IDO §14-16-6-8 Nonconformities.

The subject site lies adjacent to Major Public Open Space (MPOS) and is within 330 feet of the Petroglyph National Monument. Under current IDO regulations, this location requires compliance to IDO §14-16-5-2 Site Design and Sensitive Lands, which prompts a Cumulative Impact Analysis review for any future development. MPOS adjacency also necessitates an analysis of MPOS Edges requirements (see IDO §14-16-5-2(J)). This has been completed as part of the staff report.

Although there are not any proposed new development at this time, any future development will be required to comply with all applicable provisions and Design Standards for the NR-SU zone district pursuant to IDO 14-16-2-5(E) as well with all applicable provisions of the IDO, the DPM, other adopted City regulations, and any other terms and conditions specific to the subject site.

6-6(I)(3)(d) The City's existing infrastructure and public improvements, including but not limited to its street, trail, drainage, and sidewalk systems, have adequate capacity to serve the proposed development, and any burdens on those systems have been mitigated to the maximum extent practicable.

The City's existing infrastructure and public improvements should have adequate capacity to continue to serve the existing development. At this time, there is no proposed changes are being proposed to the existing use, access points, circulation, parking, or traffic patterns, noise, or hazardous material use; this application is to document the as-built physical environment at the DEII Airport.

6-6(I)(3)(e) The application mitigates any significant adverse impacts on the project site and the surrounding area to the maximum extent practicable.

The current manufacturing plant is a development pre-existing to the 2018 IDO and does not have a controlling Site Plan. Current uses shall be allowed pursuant to IDO §14-16-4-1(E) Previously Allowed Uses. The requested as-built Site Plan for the subject site establishes a baseline for allowable uses, design standards, and other development regulations for current and any future development.

6-6(I)(3)(f) If the subject property is within an approved Master Development Plan, the Site Plan meets any relevant standards in the Master Development Plan in addition to any standards applicable in the zone district the subject property is in.

The subject site is not within an approved Master Development Plan area.

6-6(I)(3)(g) If a cumulative impact analysis is required in the Railroad and Spur Small Area pursuant to Subsections 14-16-5-2(E) (Cumulative Impacts) and 14-16-6-4(H) (Cumulative Impacts Analysis Requirements), the Site Plan incorporates mitigation for all identified cumulative impacts. The proposed development will not create material adverse impacts on water quality or other land in the surrounding area through increases in traffic congestion, parking congestion, noise, vibration, light spillover, or other nuisances without sufficient mitigation or civic or environmental benefits that outweigh the expected impacts.

The request is for an as-built Site Plan under the NR-SU zone district for the DEII Airport subject site, which establishes a baseline for allowable uses, design standards, and other development regulations for current and any future development.

Site Plan Analysis

The request is for a Site Plan – EPC to document the existing Double Eagle II (DEII) Airport development that is adjacent to Major Public Open Space (MPOS) and within 330 ft of the Petroglyph National Monument. The proposed as-built Site Plan depicts all existing development in the Non-Residential – Sensitive Use (NR-SU) zone district. The subject site is located at 7401 Paseo del Volcan Blvd NW, an area located to the NW of the intersection of Atrisco Vista Blvd NW and Shooting Range Access Rd.

The approximately 4,200-acre site is being submitted as an “As-Built” Site Plan because the subject site area was developed pre-IDO and does not have an existing Site Plan on file with the City. Current uses and standards shall be allowed pursuant to IDO §14-16-1-10(A) Pre-IDO Approvals and IDO §14-16-4-1(E) Previously Allowed Uses.

Airport use is only permissive under the NR-SU zone district (IDO §2-5(E)(2)(a)). The NR-SU zone district is to accommodate highly specialized public, civic, institutional, or natural resource-related uses that require additional review of location, site design, and impact mitigation to protect the safety and character of surrounding properties. An as-built Site Plan for the existing development establishes the prevailing use, design standards, and other development regulations at the subject site and sets the precedence for allowable uses, design standards, and other development regulations as negotiated at the EPC.

The entire site is included with this Site Plan request to create clarity and consistency and to help ensure that any future development or amendments to the subject site are pursuant to any standards on the Site Plan and applicable IDO development standards for the NR-SU zone district. Upon approval of this request, the subject site would then be controlled by the Site Plan and Design Standards as submitted as the request here and as approved by the EPC.

Future development and amendments for the subject site would be required to follow all design standards set forth in the Site Plan for the DEII Airport and where silent, applicable IDO provisions and Design Standards for the NR-SU zone district pursuant to IDO §14-16-2-5(E), as well as with all applicable provisions of the DPM, other adopted City regulations, and any other terms and conditions specific to the subject site. Anywhere the Design Standards are silent, the IDO regulations shall apply. Existing development is allowed to continue operating, even if nonconforming, pursuant to IDO §14-16-1-10(A) Pre-IDO Approvals and IDO §14-16-4-1(E) Previously Allowed Uses.

Pursuant to IDO §14-16-6-4(Q), the decision-making body may impose conditions necessary to ensure compliance with the development standards of this IDO via the Site Plan – EPC Review and Decision Criteria of IDO §14-16-6-6(I).

The as-built Site Plan and related drawings were revised based on Staff input and any Agency Comments received by August 5, 2024. Staff has crafted Conditions of Approval to create compliance as needed and to provide clarity for the future. With the EPC’s

approval, the Site Plan would be signed off by the City's Zone Examining Officer to document the as built conditions and design standards approved by the EPC.

Site Plan Layout/ Configuration

The proposed Site Plan is located on an approximately 4,200-acre site which contains the Double Eagle II (DEII) Airport and support facilities on the Albuquerque West Mesa adjacent to Major Public Open Space (MPOS) and within 330 ft of the Petroglyph National Monument. The majority of the airport land is City-owned by the City, although there are few, privately held tracts within the DEII boundaries.

The airport facilities are concentrated in a rectangular arrangement angled NE to SE near the east central portion of the airport property. The main activity area with structures interspersed by undeveloped sectors encompasses approximately 130 acres and is located east of center of the subject site. Immediately to the south of this concentration of airport facilities are two runway areas. One runway runs NE-SW and the other N-S. The second developed area of approximately 5-acres lies about 1.4 miles to the south near the southeastern corner of the property includes a single building of the Southwest Aeronautical Mathematics and Science Academy, facilities that encompass an approximate 5-acre area.

Lease areas are proposed to the north (approximately 130 acres) and south of the as-built facilities (approximately 72.1 acres) as well as to the southeast (approximately 960 acres) and within or immediately southeast of existing development (approximately 41 acres) for a total of over 1200 acres. All of these tracts will be subject to the same Design Standards as set forth in this request.

The main portion of the airport includes 30 as-built structures, most of which were built pre-IDO. On-site buildings include: a Fixed-Base Operator (FBO) and a FBO Office Facility (Buildings F & B, respectively), 2 Executive hangars (Buildings U & V), 9 T-hangars (Buildings C, D, E, O1, O2, Q 1-4), 4 FBO hangars (Buildings G, H, L, N), 1 FBO Bay Hangar (Building M), an APD Air Support hangar (Building P), a Flying High hangar (Building T1), an Air Traffic Control Tower (Building X), an airport electrical vault (Building A), Bureau of Indian Affairs (BIA) facility (Building R), an Airfield Maintenance facility (Building W), a Vehicle fueling facility (Building Y), a NM Department of Public Safety building (Building Z), a shade cover (Building S), 2 Fuel Farms (Buildings I, J), and an AVGAS Island (Building K). Additional features are described in Sheet Keyed Notes 1 through 38. A total of 19 wall-mounted signs are photo-documented with the elevations of the respective structure (Buildings F, L, N, P, V, W, Z, Structure K). The proposed, as-built Site Plan show lease agreement areas (see SDP1.1). These locales are intended to be areas of future development.

There is a single main entrance via the Double Eagle Rd NW which is accessed to the west off of Atrisco Vista Blvd. A locked gate restricts access further west than the entry road leading south to the Fixed-Base Operator (FBO) building, various hangars (G, H, L, M, N,

O1, O2, P, Q1-4), and 2 Fuel Farms (I, J). The road south ends in an asphalted parking lot or “Airside Parking” (Keyed Note 21) with 78 typical and 4 handicapped parking spaces.

Double Eagle Rd continues west past the locked gate to the second area of airport development. This area includes the FBO Office Facility (B), the Airfield Maintenance Facility (W), the taller than 85 ft Air Traffic Control Tower (X), the NM Department of Public Safety Building (Z), various hangars (E, C, D, T1, U, V), a vehicle Fueling Facility, as well as the Airport Electrical Vault (A). The Flying High Hangar (T1) is one of a proposed total of 4 which all will be located near the center of this grouping. A dirt road runs just north of and almost parallel to the Double Eagle Rd and access the subject site water tank and utility area just north of the main facilities.

The 28,400 sq ft, landscaped Southwest Aeronautical Mathematics and Science Academy is located within the DEII but is not part of this request as it falls within Eclipse Aviation Campus Site Plan for Subdivision (PR 1003125 & 06DRB-00401). An unofficial dirt road entry east off of Aerospace Pkwy NW connects at the southern point of the access road for the eastern, north-south running airport runway.

Consolidated plans for Drainage and Grading, Utility, and Landscape Plans do not exist, but shall be submitted with any proposed, future development.

Dimensional Standards

At this time, there is no proposed development; this request is to document existing DEII Airport facilities and Design Standards.

Proposed Dimensional Design Standards are located in Sections B (Site design), D (Setbacks), G (Architecture), and I (Lighting), pages SD 4.1, SD 4.2. Maximum building heights are summarized in the DEII Dimensional Standards Summary Table under Section B (page SD 4.1).

Dimensional standards in NR-SU zone district shall be determined in the approval process of a Site Plan – EPC, and, if not specified in the proposed Site Plan Design Standards, shall comply with the applicable IDO standards (e.g., as applicable to the most similar use or district as shown in Section 14-16-5-1). The regulations of the Overlay zone prevail regardless of whether the Overlay zone regulations are more or less restrictive than the other regulations (IDO §14-16- 1-8(A)(1)) in the IDO. The DEII Airport Air Space Protection Sub-area specifies a maximum height of construction or vegetation—no greater than 150 feet above the highest point of the usable landing area, or a Horizontal Surface of 6,028 feet in elevation. Six volcano cones lie to the east of the DEII in the Petroglyph National Monument. The height of the shortest volcano cone is 5,879 feet, which, is the second from the north. The tallest measures 6,033 feet in elevation and is the third from the south (The Volcanoes Quadrangle, Bernalillo and Sandoval Counties, New Mexico, USGS Geologic Map, <https://ngmdb.usgs.gov/mapview/?center=-106.766,35.139&zoom=15>).

Proposed Building Height Standards for the DEII Airport allow structures up to 85 feet tall, and such a height would be visible from many parts of the City. In fact, this proposed height maximum will be greater than the multi-story warehouse fulfillment center to the south of the DEII Airport and wholly within Bernalillo County. The County caps building height at 60 feet for “public, semipublic, or public service buildings, hospitals, institutions, or schools” and 75 feet for churches (Appendix A - Zoning, Section 22. - Supplementary Height and Area Regulations). A maximum building height of 60 feet would be allowed between 150 feet and 250 feet of the Atrisco Vista Blvd ROW.

The 85-foot height maximum would not be allowed within 250 feet from the Atrisco Vista ROW, and would not apply to critical aeronautical support infrastructure (e.g., Air Traffic control Towers).

Proposed setbacks are described in Section D and summarized in table form. All proposed Setback Standards for all primary and accessory buildings shall comply with IDO standards where the proposed Design Standards are silent.

As-built structure dimensions are documented on the proposed building Elevation Plan (pages AE3.1-3.22). Dimensions are also provided for existing Site Amenities; Refuse Enclosures; Free Standing Signs; Lighting; and Walls, Fencing, and Screening.

All as-built DEII Airport structures meet or exceed the proposed Site Plan height and setback standards.

See Lighting (Section I) for lighting design standards.

Where the proposed Design Standards are silent, IDO regulations shall apply.

Site Design and Sensitive Lands

No Sensitive Lands analysis is required as part of the As-Built Site Plan.

Access and Connectivity

Proposed Access and Circulation Design Standards are located in Sections C (Access and Circulation) and E (Taxilands), page SD 4.1.

Access and Circulation section acknowledges adjacency to Major Public Open Space (MPOS) and the close proximity to the Petroglyph National Monument. In addition, proposed Design Standards specify the need to designate 1) specific guest pickup and drop off areas as well as 2) pre-approved delivery routes.

Design standards specify a minimum 6-foot pedestrian pathway to building front doors. Otherwise, vehicle access and circulation shall be located and designed pursuant to the appropriate IDO regulations (e.g., 14-16-5-3 Access and Connectivity).

Aircraft circulation, taxi-lane setbacks, minimum separation standards throughout the DEII are proposed and summarized in the Taxi-lane Standards table found in Design Standards, Section E.

Pedestrian and bicycle access to the Major Public Open Space shall be provided consistent with the Rank 2 Bikeways and Trails Facility Plan and as acceptable to the Open Space Division of the City Parks and Recreation Department pursuant to IDO §14-16 5-2(J)(1).

Where the proposed Design Standards are silent, IDO regulations shall apply (e.g., IDO §14-16-5-3(E)(3)(c)).

Subdivision of Land

There are no specifications for Subdivision of Land within the Design Standards or the Site Plan.

Where the proposed Design Standards are silent, IDO regulations shall apply (e.g., IDO §14-16-5-4).

Parking and Loading

Proposed Parking and Loading Design Standards are located in Sections B (Site Design) and F (Parking), page SD 4.1.

A 25 ft parking area setback from Atrisco Vista Blvd NW is proposed. Atrisco Vista Blvd lies entirely within the DEII Airport property along this stretch of its path.

A parking area setback along all other boundaries adjacent to MPOS is proposed as a 100-foot setback. This standard primarily applies to areas located along the western edges of the DEII Airport property.

Bicycle parking is proposed at a ratio of one (1) bicycle rack per 50 vehicle parking spaces. At present, there are no bicycle parking facilities available. No motorcycle parking design standards are proposed, and shall comply to IDO development standards. Three motorcycle parking spaces are currently documented for Building X and shall be included in the Provided Parking Table as a Condition of Approval.

The number of parking spaces depicted on the proposed Site Plan do not match what is listed in the Facility Index table on page SDP1.2. Staff has added a Condition of Approval stating the applicant shall update the proposed Site Plan so that the depicted parking spaces match the number of parking spaces provided in the Facility Index Table (page SDP1.2).

Off-street parking shall be regulated by the IDO §14-16-5-5 and the DPM. Where other proposed Design Standards are silent, IDO regulations shall also apply. For example, the DEII Airport is within a Developing Employment Center where parking reductions may apply.

The City's Aviation Department shall also approve the design of all parking areas on DEII property, which is noted on the design standards.

Landscaping, Buffering, and Screening

Proposed Landscaping, Buffering, and Screening Design Standards are located in Sections H (Screening), K (Landscaping), L (Sustainability), and M (Utilities), pages SD 4.1, SD 4.2. Where the proposed Design Standards are silent, IDO regulations shall apply.

The proposed Design Standards include provisions that take into account the natural desert grassland environment and surrounding Open Space by focusing landscaping within visitor and employee centric spaces, an area that may be appropriate for trees. Although gravel mulch may be appropriate in some areas, large swaths of gravel mulch should be avoided to help reduce heat and better maintain the character of the landscape.

Street trees on public ROWs are required every 25 feet along the ROW pursuant to the IDO, which is what the design standards propose. This may not be appropriate for this locale and would change the character of the open grassland environment, especially as this is something that distinguishes the area's MPOS and the Petroglyph National Monument from other regions. Trees would also block open views, especially to the east of Atrisco Vista. Revegetation with a native seed mix is appropriate and important for this region and enhances DEII Airport sustainability goals.

Less attractive areas, such as parking lots, "loading, service, and/or mechanical area, including but not limited to exterior refuse facilities, mechanical equipment, storage yards, and loading docks" shall be screened using walls, fences, and/or landscape buffers. In particular, proposed Design Standards are specified for parking lots and perimeter walls. The proposed opaque fencing should allow for open views.

Walls and Fences

Proposed Walls and Fences Design Standards are located in Sections G (Architecture) and H (Screening), pages SD 4.1, SD 4.2).

Less attractive areas, such as parking lots, "loading, service, and/or mechanical area, including but not limited to exterior refuse facilities, mechanical equipment, storage yards, and loading docks" shall be screened using walls, fences, and/or landscape buffers. In particular, proposed Design Standards are specified for parking lots and perimeter walls.

In contrast to the IDO (see §14-16-5-7(E)(1)(c)) which will be visible from MPOS, however, According to the proposed Design Standards, , barbed tape, razor wire, barbed wire, or similar materials shall not be allowed within 150 feet of Major Public Open Space with opaque fencing proposed, which is more suitable for development adjacent to MPOS minimizing obstruction to open landscapes and the Petroglyph National Monument.

Current perimeter fencing at the DEII Airport consists of chain-linked fence topped with 3-stranded barbed wire. Existing DEII structures (e.g., Buildings A, V, Z) are also currently topped with barbed and/or razor wire above chain link fencing. The proposed opaque

fencing should not create a barrier that restricts the open views that are more consistent to MPOS and should be more pleasing within the viewshed.

Loading, service, and refuse areas located within 250 feet of Atrisco Vista Blvd, public streets, or Major Public Open Space shall be screened by a minimum 6-foot tall, opaque decorative wall, fence, or landscaping. Mechanical equipment shall be screened from Atrisco Vista Blvd pursuant to the City IDO §16-5-5-6.

Outdoor Lighting

Proposed Outdoor Lighting Design Standards are located in Section I (Lighting), page SD 4.2.

Existing outdoor free-standing and wall-mounted lighting types are documented in Light Detail page, page AE 3.25. Streetlights and parking lot lights are required to be a maximum 30-foot height. Pedestrian lighting is to be a 16-foot height maximum and lighting adjacent to MPOS is required to be no taller than 20-feet.

Outdoor lighting standards are proposed to comply with the IDO (e.g., §14-16-5-8) which should help protect against unwanted light pollution adjacent to MPOS. A table and illustration summarize the proposed outdoor site lighting. Proposed text further specifies that Atrisco vista Blvd lighting shall consider MPOS adjacency and airfield restrictions.

Where the proposed Design Standards are silent, IDO regulations shall apply.

Neighborhood Edges

There are no specifications proposed for Neighborhood Edges. Where the proposed Design Standards are silent, IDO regulations shall apply.

Solar Access

There are no specifications proposed for Solar Access. Where the proposed Design Standards are silent, IDO regulations shall apply.

Building Design

Proposed Building Design Standards are located in Section G (Architecture), page SD 4.1). Although IDO §14-16-5-11 would apply where the proposed Design Standards are silent, there are no applicable NR-SU zone district design standards specifically stated within IDO §14-16-5-11.

The proposed Site Plan Building Design standards mention scale, proportion, and composition of building façade fenestration, visual interest, and positioning of major entries that should connect with pedestrian pathways. Structural features shall be to human scale and provide visual interest with glass and metal elements complying to FAA regulations for reflectivity, although very general, these ideas could enhance safety and the visual appearance of development as well as strengthen the pedestrian environment as promoted by the IDO.

Signs

Proposed Sign Design Standards are located in Section J (Signage), pages SD 4.2).

Free-standing and wall-mounted sign height and square footage is proposed. Due to sensitivity of airport usage, colors, and electronic illumination and motion is controlled. Overall signage and locations shall be approved by the City Aviation's Department.

Existing free-standing signage at the subject site is documented on Site Plan page AE 3.25.

Existing wall-mounted signage is identified with an arrow and outline in individual building elevation photos.

Where the proposed Design Standards are silent, IDO regulations shall apply, which should help locate, design, and orient signage to minimize impact on adjacent Major Public Open Space.

Major Public Open Space Edges

No development is proposed as part of this request for any subject site parcels, although Design Standards as part of the NR-SU are being negotiated to accommodate highly specialized uses that require additional review of location, site design, and impact mitigation to protect the safety and character of surrounding properties.

The DEII Airport is immediately adjacent to Major Public Open Space (MPOS). MPOS (NR-PO-B abuts the DEII property along both the eastern and western boundaries, with the Petroglyph National Monument situated within 330 ft from and to the east of the airport boundary. NR-PO-C to the south of and adjacent to DEII parcels along the most norther extent of the property. Since DEII Airport property extends to both sides of Alta Vista Blvd NW, a Principal Arterial roadway, both IDO §14-16-5-2(J)(1) **Lots within 330 Feet of Major Public Open Space** and IDO §14-16-5-2(J)(2) **Lots Adjacent to Major Public Open Space** apply, each with specific standards governing development to enhance and protect Major Public Open Space.

MPOS is space that is protected as part of the City's goal to preserve and maintain natural resources that characterize Albuquerque. Uses and development standards specified in a Resource Management Plan or Master Plan approved or amended by the Open Space Division of the City Parks and Recreation Department for each facility or in the Rank 2 Major Public Open Space Facility Plan prevail over IDO standards and may be reflected in Site Plans approved pursuant to the IDO (§14-16-2-5(F)(3)(b) Sub-zone B: Major Public Open Space).

Current uses shall be allowed pursuant to IDO §14-16-1-10(A) Pre-IDO Approvals and 4-1(E) Previously Allowed Uses.

Any future proposed development will be required to comply with the Design Standards outlined within this request and come into compliance with the current IDO regulations where those design standards are silent. Approvals granted prior to the effective date of

this IDO may be amended as described in IDO §14-16-6-4(Z) Amendments of Pre-IDO Approvals. Staff recommends that development shall not occur adjacent to MPOS east of the Atrisco Vista Blvd. right-of way (ROW) or within 330-feet of the Petroglyph National Monument.

Agency & Neighborhood Concerns

Reviewing Agencies

Various Agencies replied to the request for comment: City Transportation Development Services, AMAFCA, Bernalillo County, MRMPO, NMDOT, and PNM. These comments mostly reinforced the need to comply with existing plans and associated development criteria.

Bernalillo County Planning and Development Services and MRMPO supports the amendment.

PNM requests that any existing and/or new PNM easements and facilities be reflected on any future Site Plan and any future Plat. MRMPO recommends that the applicant adopt parking management strategies to decrease parking requirements in activity centers and redevelopment areas and increase parking costs in high demand locations. They also suggest that the applicant adopt/update land use policies and building and site development design standards to promote walkability. NMDOT asks that the applicant coordinate with the NMDOT Engineer to discuss potential impacts to Atrisco Vista Blvd and verify the development's address.

The applicant gave a project overview to Petroglyph National Monument staff on April 11, 2024, and the design criteria was provided for their review in June. The National Park Service (NPS), Petroglyph National Monument, submitted a letter to the Planning Department on August 5, 2024 expressing concerns about the proposed building heights included in the Design Standards. They state that they "cannot support building heights as currently included in the design guidelines and request the City of Albuquerque Aviation Department reconsider and institute a maximum building height of 36 feet in order to protect the values of the West Mesa and Petroglyph National Monument." The NPS does acknowledge that many of their comments based on the design guidelines they were given were related to setbacks, lighting and the protection of dark skies, building materials and colors, and screening have been addressed and are included in the design guidelines (see attached letter).

Neighborhood/Public

Notification requirements are found in IDO Table 6-1-1 (Summary of Development Review Procedures) in Part 14-16-6 Administration and Enforcement, and are further explained in IDO §14-16-6-4(K) Public Notice section. According to the City Office of Neighborhood Coordination (ONC), there are no affected registered neighborhood organizations (see

attachment). Property owners within 100 feet of the subject site were notified by mail as required (see attachments).

The West Side Coalition of Neighborhood Association was notified and invited to a DEII Open House held June 20, 2024, although the boundaries of their member associations do not include the DEII Airport area.

A member of the Santa Fe Village Neighborhood Association (SFVNA) wrote a letter of opposition dated August 5, 2024. The concerned citizen is opposed to the proposed 85-foot building height standard outlined in the Design Standards for the DEII Site Plan.

As of this writing, Staff has not been contacted nor received any other comments regarding this request.

VI. Conclusion

This request is for a Site Plan – EPC for the Double Eagle II (DEII) Airport. The DEII Airport is an approximately 4,200-acre site located at 7401 Paseo Del Volcan NW, which sits to the NW of the Atrisco Vista Blvd NW and Shooting Range Access Rd intersection. This property is bounded by Major Public Open Space (MPOS), the City of Rio Rancho, and unincorporated Bernalillo County land (the “subject site”) and within a ¼ mile west of the Petroglyph National Monument. The subject site is about 8.5 miles northwest of Downtown Albuquerque (around 18 miles driving).

The request documents the existing, as-built airport facilities as well as and defines the proposes associated NR-SU Design Standards required for the DEII Airport under the NR-SU zone district. The applicant is required to document the existing development in an “as-built” Site Plan to establish a baseline for any proposed future development and determinations for Design Standards. The NR-SU Site Plan would establish specific uses, site standards, and regulations for the Airport subject site as which are to be negotiated at the EPC through EPC application and the Site Plan. A site plan is required for any development to occur under the NR-SU zone district for the Airport Land Use. The site plan shall be in compliance with the associated Master Plan for DEII.

The subject site is zoned NR-SU, and within the DEII Employment Center, an Area of Change (most undeveloped land), and an Area of Consistency (airport areas) per the Comprehensive Plan. Major Public Open Space (MPOS) is adjacent to the DEII on its eastern and western boundaries with eastern portions within 330 ft. of the Petroglyph National Monument.

There are no affected neighborhood organizations. Property owners within a 100’ buffer were notified as required. An Open House was held for area representatives June 28, 2024. Two Agencies were in support of the Site Plan. A member of the SFVNA and the NPS Petroglyph National Monument are in opposition to the building height of 85’ in the proposed design standards.

The applicant has partially justified the request pursuant to IDO review and decision criteria for a Site Plan-EPC. Staff Recommends Approval, subject to the Conditions of Approval in the Findings of the staff report created to create clarity, and consistency, and adequate compliance with pertinent Comprehensive Plan goals policies.

Findings, Site Plan-EPC

Project #: 2018-001577 / SI-2024-00994

1. This request is for a Site Plan – EPC for the Double Eagle II (DEII) Airport legally described as all or a portion of Tract A-1, Plat of Tract A-1 & Tract L-1, Parcels 1-5, Double Eagle II Airport; Tract C and Parcel-1 Tract M Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; the Southeasterly Portion of Tract D-1-A-1, Tract D-1-A-2, and Tract S-1-A Plat of Tracts D-1-A-1, D-1-A-2, & S-1-A of Amended Bulk Land Plat for Aerospace Technology Park; Tract E, F, and K Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; Tract N-1 and Tract N-2 AKA Parcel 4 Bulk Land Plat of Tracts N-1, O-1, & N-2 Parcels 1, 2, 3, & 4 Double Eagle II Airport & Adjacent Lands; and Tract S-2 Bulk Land Plat for Aerospace Technology Park, Tracts D & S of Double Eagle II Airport & Adjacent Lands, located at 7401 Paseo Del Volcan NW, NW of the Atrisco Vista Blvd NW and Shooting Range Access Rd intersection, containing approximately 4,200 acres.
2. The DEII Airport sits to the NW of the Atrisco Vista Blvd NW and Shooting Range Access Rd intersection. This property is bounded by Major Public Open Space (MPOS), the City of Rio Rancho, and unincorporated Bernalillo County land (the “subject site”) and within a ¼ mile west of the Petroglyph National Monument. The subject site is about 8.5 miles northwest of Downtown Albuquerque (around 18 miles driving).
3. The subject site is zoned NR-SU, and within the DEII Employment Center, an Area of Change (most undeveloped land), and an Area of Consistency (airport areas) per the Comprehensive Plan. Major Public Open Space (MPOS) is adjacent to DEII on its eastern and western boundaries with eastern portions within 330 ft of the Petroglyph National Monument.
4. The subject site is governed by the 2019 Double Eagle II Airport Master Plan. Master Plans and land uses are voluntary for the facility, but the Master Plan and Site Plan are meant to be complimentary. As it stands the Master Plan only includes an “Airport Layout Drawing.” There have been several Administrative Site Plans for the airport which have been approved, but this is not consist with the IDO nor the NR-SU zone district.
5. The request documents the existing, as-built airport facilities as well as and defines the proposes associated NR-SU Design Standards required for the DEII Airport under the NR-SU zone district. The applicant is required to document the existing development in an “as-built” Site Plan to establish a baseline for any proposed future development and determinations for Design Standards. The NR-SU Site Plan establishes specific uses, site

standards, and regulations for the Airport subject site as which are to be negotiated at the EPC. A site plan is required for any development to occur under the NR-SU zone district for the Airport Land Use.

6. The As-Built site plan is in compliance with the associated Master Plan for DEII (PR #2018-001577_SI-2024-00321).
7. The EPC is hearing this request (SI-2024-00994) pursuant to IDO §14-16-2-5(E) Non-Residential – Sensitive Use Zone District (NR-SU) and §14-16-6-6(I) Site Plan – EPC. The NR-SU zone district requires that a Site Plan is reviewed and decided by the EPC. The existing, as-built site conditions depicted in the proposed Site Plan shall remain valid until they are replaced (see IDO §14-16-1-10), which will require that any new development be in compliance with the IDO. The EPC shall review the uses, standards, and regulations as negotiated by the applicant.
8. The Albuquerque/Bernalillo County Comprehensive Plan and the City of Albuquerque Integrated Development Ordinance (IDO) are incorporated herein by reference and made part of the record for all purposes.
9. IDO §14-16-6-6(I)(3)(c) states, “The Site Plan complies with all applicable provisions of this IDO, the DPM, other adopted City regulations, and any terms and conditions specifically applied to development of the property in a prior permit or approval affecting the property.”

The proposed As-Built Site Plan has been evaluated for conformance with applicable goals and policies in the Comprehensive Plan, and other applicable Plans. The approximately 4,200-acre site is being submitted as an “As-Built” Site Plan because the subject site area was developed pre-IDO and does not have an existing Site Plan on file with the City. Current uses and standards shall be allowed pursuant to IDO §14-16-1-10(A) Pre-IDO Approvals and IDO §14-16-4-1(E) Previously Allowed Uses.

Future development and amendments for the subject site would be required to follow all design standards set forth in the Site Plan for the DEII Airport and where silent, applicable IDO provisions, Site Plan and IDO Design Standards for the NR-SU zone district pursuant to IDO §14-16-2-5(E), as well as with all applicable provisions of the DPM, other adopted City regulations, and any other terms and conditions specific to the subject site. Anywhere the Design Standards are silent, the IDO regulations shall apply. Existing development is allowed to continue operating, even if nonconforming, pursuant to IDO §14-16-1-10(A) Pre-IDO Approvals and IDO §14-16-4-1(E) Previously Allowed Uses.

The 300-acre portion of the site governed by the Eclipse Aviation Campus Site Plan for Subdivision (PR 1003125 & 06DRB-00401) and the Aerospace Technology Park Site Development Plan for Subdivision (PR 1003125 03EPC-02054 & 05EPC-00566) site is not a part of the request (Tract F Bulk Land Plat of Double Eagle II Airport and Adjacent Lands; Tract D-1-A-1, Tract S-1-A, and the SW'LY Portion of Tract D-1-A-1, Plat of Tracts D-1-A-1,

D-1-A-2 & S-1-A of amended bulk land plat for Aerospace Technology Park; Tract D-1-A-2 and the Southeasterly portion of Tract D-1-A-1, Plat of Tracts D-1-A-1, D-1-A-2 & S-1-A of amended bulk land plat for Aerospace Technology Park; and LTS 1-15 Bulk Land Plat of lots 1 - 15 Aerospace Technology Park, Unit 1).

10. The request is generally consistent with GOAL 4.2 Process: Engage communities to identify and plan for their distinct character and needs from Comprehensive Plan Chapter 4: Community Identity:

Although not required by IDO regulations, City Aviation reached out to Neighborhood Associations/Coalitions and other interested parties for a Public Open House held June 28, 2024 to engage communities and help identify and plan for the distinct character and needs of the DEII Airport Facilities. Solicited feedback regarding the proposed as-built Site Plan and Design Standards contributed to proposed Design Standards revisions.

11. The request is partly consistent with the following Policies from Comprehensive Plan Chapter 4: Community Identity:

- A. POLICY 4.1.5 NATURAL RESOURCES: Encourage high-quality development and redevelopment that responds appropriately to the natural setting and ecosystem functions.

This request is for an as-built Site Plan that also documents proposed Design Standards. The site is surrounded to the east and west by MPOS and includes the Petroglyph National Monument. As the Applicant stated, the "DEII Design Standards are integral to establishing an aesthetic character compatible with the surrounding context, particularly the City of Albuquerque Open Space and Petroglyph National Monument."

The subject site is located in a generally undeveloped, desert grassland with expansive views to the mountains to the north (Sangre de Cristos), east (Petroglyph National Monument volcanoes, Sandias), southeast (Manzanos), and west (Mt Taylor). NR-SU zone district Design Standards may be varied in the NR-SU approval process (see IDO Table 2-5-9: Other Applicable IDO Sections, p. 54) with Dimensional Standards determined in the approval of a Site Plan – EPC (see IDO §14-16-5-1(E)(2)(b)). Proposed DEII Airport Design Standards generally conform to IDO design standards (e.g., Access and Connectivity, Subdivision of Land, Parking and Loading, Solar access, and Signs) that work to help reduce development impact on Major Public Open Space (MPOS), such as suggesting a color palette and light reflective values (LRV) recommended for Sensitive Lands (see IDO §14-16-5-2) for buildings within 250 feet of MPOS. It also incorporates active and passive water harvesting, passive heating and cooling structure design, permeable pavements, less linear landscape lines, and using vegetation to help slow and filter runoff. However, any significant development to the scale proposed will increase light, noise, traffic, pollution, temperatures, and habitat

fragmentation as well as impact natural drainage and views to culturally important landscape features to and from the Petroglyphs National Monument and Major Public Open Space.

Greater allowances for building setbacks (150 feet from MPOS) and requiring screened parking areas, should help lower development impact in terms of views from MPOS to the subject site, however, staff disagrees that the proposed building height maximums of 60 feet within a 150-foot setback from Atrisco Vista Blvd and 85 foot maximum for all other areas respond appropriately to the natural setting despite the addition of sustainable caveats such as orienting building to overlook MPOS in combination with passive and cooling. The proposed heights seem out of scale with area open space features and appears to be designed to compete with the volcanos themselves.

Adding trees to the Atrisco Vista Blvd ROW also seems out of character with the natural setting and ecosystem functions.

- B. POLICY 4.2.2 COMMUNITY ENGAGEMENT: Facilitate meaningful engagement opportunities and respectful interactions in order to identify and address the needs of all residents.

According to the Office of Neighborhood Coordination (ONC), there are no designated Neighborhood Associations within the required notification buffer areas. However, after the general required notification of property owners within 100 feet of the proposed Site Plan – EPC request, City Aviation reached out to Neighborhood Associations/Coalitions and other interested parties for a Public Open House held June 28, 2024. At this time, City Aviation provided information to solicit feedback regarding the proposed as-built Site Plan and Design Standards being brought before the EPC. Attendance included representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation, tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature (see Attachments). Comments collected during the meeting resulted in some Design Standards revisions. Updates of project description and were also resent to adjacent property owners and emailed to neighboring Neighborhood Associations/Coalitions.

However, the fact that this request is less than 330 ft from the Petroglyphs which is widely known as a significant sacred site for most of the area's Pueblos and Tribes, combined with the fact that only Pueblo of Laguna attended the Open House, would not be consistent to meaningful engagement and respectful interactions to identify and address the needs of all residents.

12. The request is consistent with the following Goals and Policies regarding Centers, Corridors and Development areas from Comprehensive Plan Chapter 5: Land Use:

- A. GOAL 5.1 CENTERS AND CORRIDORS: Grow as a community of strong centers connected by a multi-modal network of Corridors.

The subject site is located within a Developing Employment Center along the Regional Principal Arterial and Commuter Corridor, Atrisco Vista Blvd NW that prioritizes automobile travel in street design and improvements by allowing higher traffic speed and volume, connecting to I-40 about 4.5 miles to the south. It also is surrounded to the east and west by Major Public Open Space and includes the Petroglyph National Monument within 330 feet. The request codifies an as-built DEII Airport Site Plan and Design Standards to allow for future site development at the subject site. Adding potential employment sites to the West Side of the City would allow for the addition of jobs and employment opportunities west of the Rio Grande and could help improve the job-housing balance on that side of town.

- B. POLICY 5.1.1 DESIRED GROWTH: Capture regional growth in Centers and Corridors to help shape the built environment into a sustainable development pattern.

The subject site is located within a Developing Employment Center and along a Regional Principal Arterial/ Commuter Corridor of Atrisco Vista Blvd NW. Allowing growth within a Developing Employment Center promotes a more sustainable development patterns and could reduce the job-housing imbalance west of the river as articulated in the ABC Comp Plan, especially as the City's West Side continues to sprawl.

- C. POLICY 5.1.2: DEVELOPMENT AREAS: Direct more intense growth to Centers and Corridors and use Development Areas to establish and maintain appropriate density and scale of development within areas that should be more stable.

The subject site is located mostly within an ABC Comp Plan designated Area of Change, with the Areas of Consistency consisting of integral DEII Airport functions. The request will result in an as-built Site Plan documenting existing airport facilities and codifying Design Standards which will help guide growth and establish development density and scale for an area intended for higher intensity uses.

- D. POLICY 5.1.5 EMPLOYMENT CENTERS: Create Centers that prioritize employment and foster synergy among businesses.

The subject site is located within a Developing Employment Center and along a Regional Principal Arterial/ Commuter Corridor of Atrisco Vista Blvd NW. Allowing growth within a Developing Employment Center helps prioritize employment and foster synergy among businesses in an area with good automobile access and good connectivity for freight routes. The proposed DEII design standards also consider pedestrian and bicycle access within the Developing Employment Center.

- E. GOAL 5.3 EFFICIENT DEVELOPMENT PATTERNS: Promote development patterns that maximize the utility of existing infrastructure and public facilities and the efficient use of land to support the public good.

The proposed request promotes internal development patterns that maximize the utility of existing infrastructure and public facilities by documenting as-built DEII Airport structures and associated facilities, as well as compiling Design Standards for future proposed development at the subject site within a Developing Employment Center to better maintain a more cohesive future development. As this request does not propose any development, there will be no changes to the subject site layout which preserves current efficiencies in land use to support the public good.

- F. POLICY 5.7.2 REGULATORY ALIGNMENT: Update regulatory frameworks to support desired growth, high quality development, economic development, housing, a variety of transportation modes, and quality of life priorities.

There is no DEII Airport Site Plan or Design Standards relative to the NR-SU zone district that is associated with the current Airport Master Plan. The proposed subject site Design Standards should provide a clearer regulatory framework and processes for any proposed future development to better support desired growth and both high quality employment and economic development.

13. The request is generally consistent with POLICY 5.6.2 AREAS OF CHANGE: Direct growth and more intense development to Centers, Corridors, industrial and business parks, and Metropolitan Redevelopment Areas where change is encouraged, from Comprehensive Plan Chapter 5: Land Use:

The proposed Design Standards being submitted as part of this request for an as-built Site Plan outlines potential future growth and more intense development in Areas of Change within a Developing Employment Center. This could encourage development that expands employment opportunities and job creation within the facility boundaries. At this stage of development of the west side of the Petroglyph National Monument, there will be minimal impacts of development on existing residential uses regarding noise, stormwater runoff, contaminants, lighting, air quality, and traffic. However, with the planned growth development impacts in general will increase in what are currently undeveloped natural habitats.

14. The request is partly consistent with GOAL 5.6 CITY DEVELOPMENT AREAS: Encourage and direct growth to Areas of Change where it is expected and desired and ensure that development in and near Areas of Consistency reinforces the character and intensity of the surrounding area, from Comprehensive Plan Chapter 5: Land Use:

Although the subject site is already developed as a Regional General Aviation airport—a reliever for the Sunport airport—that primarily focuses on business activity, lease areas

within the airport tracts lie an Area of Change where development is expected and desired. The proposed Design Standards encourage and direct growth to Areas of Change where it is expected and desired and generally ensure that development in and near Areas of Consistency reinforces the character and intensity of the surrounding area. Proposed building height maximums and proposed trees along public ROW however, would change the character and intensity of land use considerably.

15. The request is not consistent with POLICY 5.3.4 CONSERVATION DEVELOPMENT: Encourage conservation development to promote private open space and preserve natural landscape, agricultural lands, and other features of the natural environment to encourage development that is sensitive to the open, natural character of the area and the geological and cultural conditions, from Comprehensive Plan Chapter 5: Land Use:

Staff agrees with the Applicant that given the large area of DEII and the need to prioritize airport operations, the DEII Design Standards should concentrate new development within a prescribed area and preserve the surrounding natural landscape which consists of a wide-open landscape with expansive views in all directions.

This request proposes landscaping, screening, and buffering standards, especially along parking lots, in areas with necessary but “less attractive” development, such as, loading, service, mechanical equipment, parking areas, and refuse facilities which should minimize visual impact from MPOS and Atrisco Vista Blvd. The suggested continuous line of 3-to-4-foot evergreen shrubbery with a minimum of 6-foot-wide buffer could harmonize with available native species (e.g., Atriplex sp) and preserve the natural landscape and local views, however, a dark green evergreen, such as decorative juniper will be out of place and inappropriate for the surrounding landscape. The addition of trees in landscape that emphasizes visitor and employee centric spaces and walkways can potentially be appropriate in certain areas, but not e along Atrisco Vista Blvd, especially on the east side since it will potentially change the character of the landscape.

Because the addition of fencing impacts view distances, opaque fencing should retain the feeling of Open Space and the viewshed, while helping maintain the built aesthetic.

Proposed color schemes and light reflectivity standards that echo the surrounding natural environment shall reduce the visual impacts of new development. Again, the subject site is located in a generally flat, gradually sloped and undeveloped, desert grassland with expansive views to the north (Sangre de Cristos), east (Petroglyph National Monument volcanoes, Sandias), southeast (Manzanos), and west (Mt Taylor), and greater building setback allowances should help lower development impact in terms of views from MPOS to the subject site as this will bar structures and buildings at the property lines. Staff disagrees that the proposed building height maximums of 60 feet within a 150-foot setback from Atrisco Vista Blvd and 85 foot maximum for all other areas responds appropriately to the natural setting. Nevertheless, proposed Design Standards encourage

tenants to orient buildings to allow for views, especially of the Petroglyph National Monument.

Any significant development to the scale proposed will increase light, noise, traffic, temperature, and habitat fragmentation as well as impact views to culturally important landscape features.

16. The request is partly consistent with the following Policies regarding Centers, Corridors and Development areas from Comprehensive Plan Chapter 7: Urban Design:

- A. POLICY 7.3.1 NATURAL AND CULTURAL FEATURES: Preserve, enhance, and leverage natural features and views of cultural landscapes, from Comprehensive Plan Chapter 7: Urban Design:

The proposed site designs address development issues to preserve and leverage natural features and views of cultural landscapes with development setback provisions, specifying appropriate color choices and building materials to blend in with the natural landscape, adding low landscape buffers and appropriate screening to less aesthetically pleasing development, requiring more subdued and consolidated signage, burying all electric distribution lines, and eliminating light trespass beyond the property line or allowing light to be visible from MPOS except where FAA regulations may vary.

However, staff disagrees that the proposed Design Standards with building height maximums of 60 feet within a 150-foot setback from Atrisco Vista Blvd and an 85-foot maximum for all other areas preserve, enhance, and leverage natural features. As such heights will compete with the elevations of the nearby volcanos, and would probably be visible from the City's east side, similar to the current warehouse fulfillment center to the south of the DEII Airport just outside of the City. Such heights would also be the first impression on entry from the west into the City on the highway and would be the features versus leveraging the neighboring volcano features.

Added trees to public ROWs, especially along Atrisco Vista Blvd would not maintain the natural conditions of the natural landscapes within the DEII parcels and would not provide an appropriate transition to adjoining Major Public Open Space.

- B. POLICY 7.5.1 LANDSCAPE DESIGN: Encourage landscape treatments that are consistent with the high desert climate to enhance our sense of place.

Some of the proposed landscape treatments are consistent with the high desert climate. For example, where not in conflict with FAA safety regulations or aviation functionality, the DEII Design Standards encourage active and passive water harvesting techniques to slow and capture stormwater on site as well as to supplement watered landscaped areas as natural drainage is modified.

Although the Design Standards encourage the use of native plant materials that enhance our sense of place, regionally adapted plants that will thrive in local conditions with less fertilizer, water, and maintenance are also proposed, the latter of which do not match adjacent landscapes and could be invasive.

The addition of trees along the public ROW would not be consistent with the microclimate of the site location or the dramatic setting of the West Mesa, but if restricted to visitor and employee centric spaces to create moderated microclimates, trees could help allow for the high desert climate in adjoining spaces, and with that, our sense of place.

17. The request is not consistent with GOAL 7.5 CONTEXT-SENSITIVE SITE DESIGN: Design sites, buildings, and landscape elements to respond to the high desert environment, from Comprehensive Plan Chapter 7 Urban Design:

Staff agrees that the natural terrain and open landscapes of arid grassland of the DEII Airport and surrounding West Mesa offers spectacular open views in all directions from the Petroglyph National Monument Volcanos to distant mountains (e.g., Sangre de Cristos, Sandias, Manzanos, Mt Taylor). Because of this, it is notable that the proposed DEII Design Standards prioritizes maintaining a natural landscape wherever possible. The fact that the proposed Design Standards center sustainable practices, including Green Stormwater Infrastructure (GSI) and Low Impact Development (LID) practices for site design, buildings, and landscape elements to respond to the high desert environment are also important. For the most part, sustainable, context-sensitive Site Design has been proposed. However, Staff contends that the proposed building heights and tree-lined ROWs will negatively impact views to existing landscape elements. Buildings reaching maximum heights of 60 or 85 feet, although in effect represent denser development, will not preserve the open vistas. Within recent history, trees do not naturally contribute to the area's native vegetation and will block views to the natural high desert environment.

18. The request is not consistent with the following Policies from Comprehensive Plan Chapter 11 Heritage Conservation:

- A. GOAL 11.3 CULTURAL LANDSCAPES: Protect, reuse, and/or enhance significant cultural landscapes as important contributors to our heritage and rich and complex identities.

The proposed DEII Airport Design Standards aim to protect the significant cultural landscapes that surround the airport. These critical landscapes include Major Public Open Space and the striking volcano cones and cultural remains protected by the Petroglyph National Monument, all of which contribute to the area's heritage as well as our rich and complex identities. Accordingly, all future development shall be pursuant to Archaeological Sites regulations of the IDO Site Design and Sensitive Lands (see §14-16-5-2(D)(1)).

And although the proposed DEII Design Standards take into consideration these remarkable westside viewsheds and exceed IDO standards, where feasible, additional measure can be taken to minimize the impact of development:

1) Reduce building heights, 2), Increase building density, 3) Constrain development to only the west side of Atrisco Vista, 4) Eliminate the ROW tree requirement along Atrisco Vista and most ROW area; plant trees only in areas with dense development and regular outdoor use or to minimize heat as part of hardscapes, 5) Encourage shade structures, 6) Require shading over parking, 6) Require solar hookups, and 6) When developing land that has not already been bladed, remove plants prior to ground breaking to reuse in landscaping.

Proposed protective DEII Design Standards include minimizing the visibility of structures through color and building material restrictions, minimum building and parking setbacks, adding screening, wall, and fencing requirements. Additionally, building setbacks have been added for MPOS and Atrisco Vista Blvd that are greater than those required by the IDO to minimize impacts to MPOS and the Petroglyph National Monument.

- B. POLICY 11.3.4 PETROGLYPH NATIONAL MONUMENT: Regulate adjacent development to protect and preserve the Petroglyph National Monument – its volcanoes, petroglyphs, and Northwest Mesa Escarpment – as a priceless cultural landscape and community resource that provides physical, cultural, and economic benefits.

The West Side is expanding and the DEII Airport is within a designated Employment Center. The proposed DEII Design Standards attempt to protect the Monument from growth and development pressures by concentrating leased land development primarily to the southwest corner of the property. Additional protective DEII Design Standards include minimizing the visibility of structures through color and building material restrictions, minimum building and parking setbacks, adding screening, wall, and fencing requirements. Additionally, building setbacks have been added for MPOS and Atrisco Vista Blvd that are greater than those required by the IDO to minimize impacts to MPOS and the Petroglyph National Monument.

However, the proposed structure height maximums of 60 feet between 150 and 250 feet from the Atrisco Vista ROW and up to 85 feet in all other areas (provided that they comply with the FFA FAR Part 7 regulations) are incongruent with the area's open grasslands and the Petroglyph National Monument. With these building heights, views to and from those spaces would not be preserved. Trees added to the Atrisco Vista Blvd ROW would also change the character of the landscape and not preserve the natural open spaces that characterize the area.

Instead of the volcanos, any future proposed development would act as a visual reference point, rendering edge treatments and transitions as inappropriate.

19. The request meets the Site Plan-EPC Review & Decision Criteria in IDO Section 14-16-6-6(J)(3) as follows:

A. 6-6(I)(3)(a) The site plan is consistent with the ABC Comp Plan, as amended.

As demonstrated by the policy-based analysis of the proposed Site Plan, the request is generally consistent with applicable Comprehensive Plan Goals and policies.

B. 6-6(I)(3)(b) The Site Plan is consistent with any applicable terms and conditions in any previously approved NR-SU or PD zoning covering the subject property and any related development agreements and/or regulations.

The subject site is zoned NR-SU. There is not an NR-SU Site Plan on file for the DEII Airport Property in its entirety. There is a Site Development Plan for Subdivision with Design standards that is valid for a 300-acre portion of the airport property which includes the Site Plan for Eclipse Aviation. This SDP for Subdivision governs these specific tracts within the Airport, which shall be noted on the NR-SU site plan: Tract F Bulk Land Plat of Double Eagle II Airport and Adjacent Lands; Tract D-1-A-1, Tract S-1-A, and the SW'LY Portion of Tract D-1-A-1, Plat of Tracts D-1-A-1, D-1-A-2 & S-1-A of amended bulk land plat for Aerospace Technology Park; Tract D-1-A-2 and the Southeasterly portion of Tract D-1-A-1, Plat of Tracts D-1-A-1, D-1-A-2 & S-1-A of amended bulk land plat for Aerospace Technology Park; and LTS 1-15 Bulk Land Plat of lots 1 - 15 Aerospace Technology Park, Unit 1. No other related development agreements and/or regulations are in known to be place for the project site.

C. 6-6(I)(3)(c) The Site Plan complies with all applicable provisions of this IDO, the DPM, other adopted City regulations, and any terms and conditions specifically applied to development of the property in a prior permit or approval affecting the property.

Staff has reviewed the proposed as-built Site Plan for documentation purposes. The existing development for the airport use at the subject site was established prior to the adoption of the 2018 IDO, therefore, current, as-built development will be held to Pre-IDO Approvals pursuant to IDO §14-16-1-10 and IDO §14-16-4-1(E). Current land uses, buildings, lots, signs, and site features may become nonconforming due to the adoption of the IDO and will be pursuant to IDO §14-16-6-8 Nonconformities.

The subject site lies adjacent to Major Public Open Space (MPOS) and is within 330 feet of the Petroglyph National Monument. Under current IDO regulations, this location requires compliance to IDO §14-16-5-2 Site Design and Sensitive Lands,

which prompts a Cumulative Impact Analysis review for any future development. MPOS adjacency also necessitates an analysis of MPOS Edges requirements (see IDO §14-16-5-2(J)). This has been completed as part of the staff report.

Although there are not any proposed new development at this time, any future proposed development will be required to comply with all applicable provisions and Design Standards for the NR-SU zone district pursuant to IDO 14-16-2-5(E) as well with all applicable provisions of the IDO, the DPM, other adopted City regulations, and any other terms and conditions specific to the subject site.

- D. 6-6(I)(3)(d) The City's existing infrastructure and public improvements, including but not limited to its street, trail, drainage, and sidewalk systems, have adequate capacity to serve the proposed development, and any burdens on those systems have been mitigated to the maximum extent practicable.

The City's existing infrastructure and public improvements should have adequate capacity to continue to serve the existing development. At this time, there is no proposed changes are being proposed to the existing use, access points, circulation, parking, or traffic patterns, noise, or hazardous material use; this application is to document the as-built physical environment at the DEll Airport.

- E. 6-6(I)(3)(e) The application mitigates any significant adverse impacts on the project site and the surrounding area to the maximum extent practicable.

The current manufacturing plant is a development pre-existing to the 2018 IDO and does not have a controlling Site Plan. Current uses shall be allowed pursuant to IDO §14-16-4-1(E) Previously Allowed Uses. The requested as-built Site Plan for the subject site establishes a baseline for allowable uses, design standards, and other development regulations for current and any future development.

- F. 6-6(I)(3)(f) If the subject property is within an approved Master Development Plan, the Site Plan meets any relevant standards in the Master Development Plan in addition to any standards applicable in the zone district the subject property is in.

The subject site is not within an approved Master Development Plan area.

- G. 6-6(I)(3)(g) If a cumulative impact analysis is required in the Railroad and Spur Small Area pursuant to Subsections 14-16-5-2(E) (Cumulative Impacts) and 14-16-6-4(H) (Cumulative Impacts Analysis Requirements), the Site Plan incorporates mitigation for all identified cumulative impacts. The proposed development will not create material adverse impacts on water quality or other land in the surrounding area through increases in traffic congestion, parking congestion, noise, vibration, light spillover, or other nuisances without sufficient mitigation or civic or environmental benefits that outweigh the expected impacts.

The request is for an as-built Site Plan under the NR-SU zone district for the DEII Airport subject site, which establishes a baseline for allowable uses, design standards, and other development regulations for current and any future development

20. A member of the Santa Fe Village Neighborhood Association (SFVNA) wrote a letter of opposition dated August 5, 2024. The concerned citizen is opposed to the proposed 85-foot building height standard outlined in the Design Standards for the DEII Site Plan.
21. The National Park Service (NPS), Petroglyph National Monument, submitted a letter to the Planning Department on August 5, 2024 expressing concerns about the proposed building heights included in the Design Standards. They state that they “cannot support building heights as currently included in the design guidelines and request the City of Albuquerque Aviation Department reconsider and institute a maximum building height of 36 feet in order to protect the values of the West Mesa and Petroglyph National Monument.”

Recommendation

APPROVAL of Project #: 2018-001577 / SI #: 2024-00994, a request for a Site Plan-EPC for an approximately 4,200 acre site legally described as, all or a portion of Tract A-1, Plat of Tract A-1 & Tract L-1, Parcels 1-5, Double Eagle II Airport; Tract C and Parcel-1 Tract M Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; the Southeasterly Portion of Tract D-1-A-1, Tract D-1-A-2, and Tract S-1-A Plat of Tracts D-1-A-1, D-1-A-2, & S-1-A of Amended Bulk Land Plat for Aerospace Technology Park; Tract E, F, and K Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; Tract N-1 and Tract N-2 AKA Parcel 4 Bulk Land Plat of Tracts N-1, O-1, & N-2 Parcels 1, 2, 3, & 4 Double Eagle II Airport & Adjacent Lands; and Tract S-2 Bulk Land Plat for Aerospace Technology Park, Tracts D & S of Double Eagle II Airport & Adjacent Lands, located at 7401 Paseo Del Volcan NW, NW of the Atrisco Vista Blvd NW and Shooting Range Access Rd intersection, based on the preceding Findings and subject to the following Conditions of Approval.

Conditions of Approval

Project #: 2018-001577 / Case #: 2024-00994 - Site Plan-EPC

1. The applicant shall coordinate with the staff planner to ensure that all Conditions of Approval are met and then submit a vetted, final version to the staff planner for filing at the Planning Department.
2. After approval by the Environmental Planning Commission (EPC), the applicant shall submit the site plan which meets conditions of approval to the Staff Planner or relevant Urban Design and Development Staff for final sign-off. The reviewer will be responsible

for ensuring that all EPC Conditions have been satisfied and that the IDO, DPM, and all other applicable City requirements have been met

3. Pursuant to IDO §14-16-6-6(I)(2)(m) Site Plans shall be reviewed administratively for compliance with conditions of approval, DPM, and zoning standards prior to the issuance of a building permit.
4. Pursuant to IDO section 14-16-6-4(P)(4) Any conditions shall be met within 1 year of the approval, unless stated otherwise in the approval. If any conditions are not met within that time, the approval is void. The Planning Director may extend the time limit up to an additional 1 year.
5. Site Plan:
 - A. G5 shall be added to the Zone Atlas description for all pages (e.g., SDP1.2).
 - C. Zoning descriptions shall be indicated for each parcel outside of the DEII boundary on Sheet SDP1.1.
 - D. A note shall be added to explain the Obstacle Free Zone on Sheets SDP1.2 and 2.1.
 - E. A keyed note shall be added for the diagonal (NW-SE) solid line running from the area with buildings Q1-4 and road on Sheet SDP1.2.
 - F. "Building" shall be added to state "Building Setbacks" to describe the first section of the DEII Dimensional Standards Summary Table in Section C (Site Design) on Sheet SD 4.1.
 - G. Add sidewalks to Site Plan where appropriate with dimensions.
 - H. The existing fencing east and south of Building Z shall be added to Sheet SDP 2.1.
 - I. The depicted parking spaces shall match the number of parking spaces provided in the Facility Index Table on Sheet SDP1.2.
 - J. Building S (Shade Cover) and Area/ Building Y (Vehicle Fueling Facility) shall be added to elevation drawings with complete dimensions.
 - K. Existing motorcycle spaces and bicycle parking shall be documented as provided parking, if applicable.
 - L. The DEII Airport Site Plan cover page, Sheet Index page titles for SDP 1.1 and SDP 1.2 shall be updated to reflect that the request is for an As-Built Site Plan and not an existing Site Plan.
 - M. The DEII Airport Site Plan cover page, Sheet Index shall include Building identifiers.
 - N. The DEII Airport Site Plan cover page, Sheet Index page titles shall be updated to match sheet titles for AE 3.22 (Site Amenities) and AE 3.26 (Walls, Fencing and Screening).
 - O. The spelling inconsistency of "DE-II" in the Table, Wall and Fences row of Table of Applicable Standards, Sheet SDP1.1 shall be updated to DEII to reduce confusion.
6. Design Standards:

- A. Building and structure heights shall be limited 36 feet at the DEII Airport except for critical aeronautical support infrastructure such as Air Traffic Control Towers.
- B. The ROW tree requirement along Atrisco Vista shall be removed; trees shall only be planted in areas with dense development and regular outdoor use or to minimize heat as part of hardscapes.
- C. Design Standards regarding clustered building (i.e., Conservation Design), shade structures, and shading over parking shall be added to Design Standards with agreed upon regulations to promote integrated site design that is considerate to the natural features and topography.
- D. Solar hookups shall be required for all new development.
- E. The text 14-6-5-11 under Design Standards, section H. Architecture – Building Design, 1.: shall be updated to the correct IDO section 14-16-5-11.
- F. The text under Design Standards, section H. Architecture – Building Design, 7.: shall be updated to the correct IDO section 14-6-5-4-3.
- G. Building Height Section text “Within 250’ of Atrisco Vista Blvd ROW shall be updated to “From 150’ to 250’ of Atrisco Vista Blvd ROW” on Sheet SD 4.1.
- H. Text shall be added that describes the proposed “50’ Required screen zone” in Section I.2. (Screening) on Sheet SD 4.1.
- I. The Design Standard text of Sheet SD4.1, “The DEII Design Standards will also help provide a framework for review of proposed site developments. Subsequent site development plan requirements for developments on lease areas of 5 acres or less or that contain buildings of 50,000 sq. ft or less shall be delegated to the City of Albuquerque’s Development Facilitation Team (DFT) as a Minor Amendment. Development greater than these thresholds shall be delegated to the Environmental Planning Commission (EPC).” shall be moved to Section B (Process).

7. Elevations

- A. The storage building west of Building R shall be added to the proposed Site Plan, Sheet SDP1.2 with a Keyed Note.
- B. Dimensions for western and northern structure features of Building X shall be added to AE3.20 as seen on SDP1.2.
- C. The building height for the single-story portion of Building Z shall be added to Sheet AE3.21.
- D. Both (2) Utility Structures shall be depicted with dimensions on Sheet AE3.23 (Site Amenities).
- E. Wall mounted signage dimension shall be provided.

- F. A fifth (5th) Freestanding Sign at the main gate (as indicated on SDP1.2) shall be added with dimensions and descriptions to Sheet AE3.25 (Freestanding Signs).
- G. Height dimensions to Freestanding signs shall be added to Sheet AE3.25.
- H. The symbols next to the Utility Structure to the southwest of Building B shall be added to the legend on Sheet SDP2.1.
- I. Building length shall be added to the elevation depiction of Building O2 on Sheet AE3.11.
- 8. Development shall not occur east of the right-of way (ROW) of Atrisco Vista Blvd. and adjacent to MPOS or within 330-feet of the Petroglyph National Monument.
- 9. The Applicant shall coordinate with NMDOT District 3 Assistant Engineer Margaret Haynes to discuss the Facility Plan's potential impacts to Atrisco Vista Blvd and verify the DEII Airport address.
- 10. The Site Plan shall comply with the General Regulations of the IDO and all other applicable design regulations, except as specifically approved by the EPC.

Megan Jones

Megan Jones, Principal Planner

Catherine Heyne

Catherine Heyne, Planner

Notice of Decision cc list:

City of Albuquerque Aviation Department, Manny Manriquez, mmanriquez@cabq.gov
Dekker, Will Gleason, WillG@dpsdesign.org
Dekker, Jessica Lawlis, JessicaL@dpsdesign.org
Dekker, Rebecca Shank, RebeccaS@dpsdesign.org
DEII, Christina Sandoval, cmsandoval@cabq.gov
City of Albuquerque Legal, Andrew S. Coon, Acoon@cabq.gov
EPC File

Agency Comments

PLANNING DEPARTMENT

Transportation Development Services

Site plan shall comply and be in accordance with all applicable City of Albuquerque requirements, including the Development Process Manual and current ADA criteria.

- Developer is responsible for permanent improvements to the transportation facilities adjacent to the proposed development site plan, as required by the Development Facilitation Team (DFT).
- An approved Traffic Circulation Layout will be required prior to site plan or building permit.

OTHER CITY OF ALBUQUERQUE DEPARTMENTS / OFFICES

Solid Waste Department

No comment at this time.

OTHER AGENCIES

Albuquerque Metropolitan Arroyo Flood Control (AMAFCA)

No adverse comments to the Site Plan – EPC. The Double Eagle site drainage is governed by the Double Eagle II Municipal Airport Drainage Masterplan (Molzen-Corbin, 2003), the Double Eagle II Municipal Airport Storm Drainage Masterplan Update 2009, Basin Summary (Molzen-Corbin, 2010) and the Boca Negra DMP above Unser Blvd (Bohannon Huston, 2024) and must continue to conform to these drainage master plans until an update/new DMP is proposed. Programmed drainage facilities identified in the DMP are needed to limit developed flow to not overwhelm downstream drainage infrastructure.

Bernalillo County

Bernalillo County Planning and Development Services supports the amendment to the Double Eagle II Master Plan with a Site Plan and design standards. The south end of the subject site is adjacent to Bernalillo County unincorporated areas, and solely divides the two jurisdictions by right-of-way. The amendment appears to be necessary to provide detailed guidance for future growth. The amendment endeavors to “encourage connections between different uses on the property and access to shared amenities, reducing travel demand to other areas,” and the addition of a comprehensive framework has the potential to create appropriate synergy between uses.

Mid-Region Metropolitan Planning Organization (MRMPO)

MRMPO has no adverse comments. For informational purposes:

- MRMPO supports the development and integration of a Site Plan and design standards for the Double Eagle II Master Plan to help guide future development.

Appendix G of the MTP recommends the following as it relates to the subject request:

- Adopt parking management strategies to decrease parking requirements in activity centers and redevelopment areas and increase parking costs in high demand locations.
- Adopt/update land use policies and building and site development design standards that promote walkability
- Encourage a mix of land uses (retail, housing, entertainment, etc.) and multimodal facilities in appropriate locations to encourage shorter and more active trips.
- Encourage networks that improve connectivity (i.e., those with shorter block lengths and fewer dead ends/cul-de-sacs).
- Encourage place-making and the proliferation of community identity and innovation.

National Park Service (NPS) / Petroglyph National Monument

Many of our comments related to setbacks, lighting and the protection of dark skies, building materials and colors, and screening have been addressed and are included in the design guidelines.

We are concerned, however, about the building height and lack of dimensional standards proposed to be authorized on the site. Buildings as tall as 85 feet would be allowed 250 feet from Atrisco Vista Blvd, and buildings as tall as 60 feet would be allowed 150 feet from Atrisco Vista. In our prior meeting, the City of Albuquerque Aviation Department provided us with information that the maximum building height would be around three stories (30-36 feet) with the exception of the control tower. While the Master Plan must allow for the control tower, as currently written, the plan would allow for future structures of any use, any square footage, and heights up to 85 feet tall.

As we've seen from recent development south of the airport, tall buildings detract from the viewshed and cultural landscape of the area and mar the landscape. For example, the Amazon buildings can be seen not only from Petroglyph National Monument, but from as far away as I-25. Double Eagle Airport II is even closer to the Monument and structures of this scale would intrude on the landscape and the experience of visitors to Petroglyph National Monument, including the Tribal people who hold this landscape sacred.

We cannot support building heights as currently included in the design guidelines and request the City of Albuquerque Aviation Department reconsider and institute a maximum building height of 36 feet in order to protect the values of the West Mesa and Petroglyph National Monument.

New Mexico Department of Transportation (NMDOT)

Please coordinate with NMDOT District 3 Assistant Engineer Margaret Haynes to discuss the Facility Plan's potential impacts to Atrisco Vista Blvd. Please verify the development's address. Paseo Del Volcan (NM 347) is an east - west arterial in Rio Rancho. Margaret.Haynes@dot.nm.gov, Cell: 505-288-2086"

Public Service Company of New Mexico (PNM)

There are numerous PNM facilities and/or easements within and around DEll Airport.

It is the applicant's obligation to determine if existing utility easements or rights-of-way are located within the property and to abide by any conditions or terms of those easements.

Any existing easements may have to be revisited and/or new easements may need to be created for any electric facilities as determined by PNM.

Any existing and/or new PNM easements and facilities need to be reflected on any future Site Plan and any future Plat.

Structures, especially those made of metal like restrooms, storage buildings, and canopies, should not be within or near PNM easements without close coordination with and agreement from PNM.

Relocation and/or undergrounding of existing electric lines is at the expense of the property owner.

Perimeter and interior landscape design should abide by any easement restrictions and not impact PNM facilities.

The applicant should contact PNM's New Service Delivery Department as soon as possible to coordinate electric service regarding any proposed project. Submit a service application at <https://pnmnsd.powerclerk.com/MvcAccount/Login> for PNM to review.

If existing electric lines or facilities need to be moved, then that is at the applicant's expense. Please contact PNM as soon as possible at <https://pnmnsd.powerclerk.com/MvcAccount/Login> for PNM to review.

A) PHOTOGRAPHS

Figure 1: Overview of the subject site, looking WSW from airport east entrance.



Figure 2: Overview of subject site, looking NW.



Figure 3: Airport tower and buildings, looking W.



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Figure 4: Subject site Welcome Sign, looking SE to airport runway.



Figure 5: Overview of subject site buildings, looking W.



Figure 6: Overview of subject site hangers, looking E.



Figure 7: Panoramic overview of the subject site looking S.



Figure 8: Panoramic overview of the subject site looking E.



B) HISTORY

City of Albuquerque
Planning Department
Development Services Division
P.O. Box 1293
Albuquerque, New Mexico 87103

Date: June 20, 1997

OFFICIAL NOTIFICATION OF DECISION

City of Albuquerque
Open Space Division
P.O. Box 1293
Albuq. NM 87103

FILE: Z-96-57/AX-96-8

LEGAL DESCRIPTION: For Section 331, Township 11 North, Range 2 East, a portion of the north ½ of Section 6, Township 10 North, Range 2 east, and the northeast 1/4 of Section 1, Township 10 North, and Range 1 East, N.M.P.M., located west of the Volcanoes and south of the Double Eagle Airport, containing approximately 943 acres. (F-7, G-6, G-7)

The City of Albuquerque expands this request to include the Double Eagle Airport and Volcano Ranch Park, to be zoned SU-1 for Airport and SU-1 for Major Public Open Space, containing approximately 4,484 acres. (A-3 to A-9, B-3 to B-8, C-2 to C-8, D-2 to D-6, E-2 to E-6, F-2 to F-6, and G-2 to G-6) Russell Brito, Staff Planner

On June 19, 1997, the Environmental Planning Commission voted to recommend approval to City Council of AX-96-8, a request for annexation of Section 31, Township 11 North, Range 2 East, a portion of the north ½ of Section 6, Township 10 North, Range 2 East, the northeast 1/4 of Section 1, Township 10 North, Range 1 East, N.M.P.M., the Double Eagle Airport, and Volcano Ranch Park, based on the following Findings:

FINDINGS:

1. The Environmental Planning Commission recommended approval of the original request for annexation and establishment of RO-20 zoning of a smaller piece of land, approximately 957 acres, on June 20, 1996; this recommendation was not forwarded to the City Council because the City has established a new zoning category, SU-1 for Major Public Open Space, which is more appropriate for this site.
2. The City of Albuquerque has expanded the original request to include annexation of the Double Eagle Airport and Volcano Ranch Park, to be zoned SU-1 for Airport and Related Facilities and SU-1 for Major Public Open Space, respectively.
3. The land of the soil amendment facility and the strip of land connecting it to the Municipal boundary are already part of the City and are not part of this request.
4. This request meets the requirements of Resolution 54-1990: the land to be annexed is contiguous to existing City boundaries; the land has provision for street access to the City; the land has reasonable boundaries for delivery of public services; and the lands meet the guidelines established in the Reserve Area section (II-B-2) of the Comprehensive Plan.

OFFICIAL NOTIFICATION OF DECISION

Z-96-57/AX-96-8

PAGE 2

5. There is no need for a pre-annexation agreement for a majority of the subject open space and airport properties since these lands are already owned and managed by the City of Albuquerque and either no development will occur within them or development agreements can be handled within the City government structure; a pre-annexation agreement between the City and the State which outlines the concerns and needs of the State Land Office is appropriate and should be completed prior to final action by the City Council.
 6. The West Side Strategic Plan designates the subject properties as part of the City Service Area and within the jurisdiction of the City of Albuquerque.
-

On June 19, 1997, the Environmental Planning Commission voted to recommend approval to City Council of Z-96-57, a request for establishment of SU-1 for Major Public Open Space zoning for Section 31, Township 11 North, Range 2 East, a portion of the north ½ of Section 6, Township 10 North, Range 2 East, the northeast 1/4 of Section 1, Township 10 North, Range 1 East, N.M.P.M., and Volcano Ranch Park; and for establishment of SU-1 for Airport for Double Eagle Airport, be based on the following Findings.

FINDINGS:

1. The annexation of the subject lands is a changed community condition and is justification for a zone change as required by Resolution 270-1980; also, the SU-1 for Major Public Open Space and SU-1 for Airport zoning categories are more advantageous to the community by clearly defining the Open Space lands for preservation and the Double Eagle Airport land for future planning and development.
2. The establishment of SU-1 for Major Public Open Space zoning and SU-1 for Airport zoning will provide for uniform public safety and management jurisdiction for City owned land and facilities currently in the County.
3. This request to establish SU-1 for Major Public Open Space zoning and SU-1 for Airport zoning for the subject properties does not contradict the goals and policies of the Comprehensive Plan.
4. This request for establishment of SU-1 for Airport and SU-1 for Major Public Open Space zoning is consistent with the Northwest Area Plan by reinforcing the airport's potential to act as a focal point of economic/employment activity and by further defining geographical boundaries for major public open space.
5. The request for SU-1 for Airport zoning is consistent with the West Side Strategic Plan by requesting zoning that will facilitate additional, future planning with regard to the airport where critical land use interfaces will occur.

IF YOU WISH TO PROTEST THIS DECISION, YOU MUST DO SO BY JULY 7, 1997 IN THE MANNER DESCRIBED BELOW.

OFFICIAL NOTIFICATION OF DECISION

Z-96-57/AX-96-8

PAGE 3

Protest to the City Council: Persons aggrieved with any determination of the Environmental Planning Commission acting under this ordinance and who have legal standing as defined in Section 14-16-4-4.B.2 of the City of Albuquerque Comprehensive Zoning Code may file a protest to the City Council by submitting written application on the Planning Division form to the Planning Division within 15 days of the Planning Commission's decision. The date the determination in question is issued is not included in the 15-day period for filing an protest, and if the fifteenth day falls on a Saturday, Sunday or holiday as listed in the Merit System Ordinance, the next working day is considered as the deadline for filing the protest.

YOU WILL RECEIVE NOTIFICATION IF ANY OTHER PERSON FILES A PROTEST.

Sincerely,



Ronald N. Short, AICP
Planning Director

RNS/RB/ac

cc: Tim Callahan, NM State Land Office, 4308 Carlisle NE, Suite 209, Albuquerque, NM 87109
Patrick Chapman, P.O. Box 27666, Albuquerque, NM 87125

CITY of ALBUQUERQUE TWELFTH COUNCIL

COUNCIL BILL NO. F/S 0-10-1ENACTMENT NO. 43-1997SPONSORED BY: Alan B. Armijo

ORDINANCE

ANNEXING 10,859 ACRES MORE OR LESS, LOCATED GENERALLY ON EITHER SIDE OF PASEO DEL VOLCAN NW, ON THE NORTH, SOUTH AND WEST SIDES OF THE SOIL AMENDMENT FACILITY, SOUTH OF THE TOWN OF ALAMEDA GRANT, NORTH OF THE TOWN OF ATRISCO GRANT, WEST OF THE EXISTING CITY LIMITS AND EAST OF THE RIO PUERCO ESCARPMENT AND THE TOWN OF ATRISCO GRANT TO THE CITY OF ALBUQUERQUE; AMENDING THE ZONE MAP TO ESTABLISH SU-1 FOR MAJOR PUBLIC OPEN SPACE ZONING AND SU-1 FOR AIRPORT AND RELATED FACILITIES ZONING.

BE IT ORDAINED BY THE COUNCIL, THE GOVERNING BODY OF THE CITY OF ALBUQUERQUE:

Section 1. AREA PROPOSED FOR ANNEXATION. The owners of a majority of the area annexed hereby presented a properly signed petition to annex the following territory: 11,756 acres more or less, located generally on either side of Paseo Del Volcan NW, on the north, south and west sides of the Soil Amendment Facility, south of the Town of Alameda Grant, North of the Town of Atrisco Grant, west of the existing City limits and east of the Rio Puerco Escarpment and the Town of Atrisco Grant and more particularly described as follows:

A. Section 31 T11N, R2E; a portion of the N ½ of Section 6 T10N, R2E; the NE 1/4 of Section 1 T10N, R1E; Lots 1 through 8 of Section 6 T10N, R2E; Section 6 T10N, R2E; Lots 1 through 8 of Section 1 T10N, R1E; Lots 1 through 8 of Section 3 T10N, R1E; Lots 1 through 8 of Section 4 T10N, R1E; Lots 1 through 8 of Section

Underscored Material - New
[Bracketed Material] - Deletion

1 T11N, R1E; the NE 1/4 of the NW 1/4 of Section 29 T11N; the SE 1/4 of Section 29
 2 T11N, R1E; Lot 1 of Section 31 T11N, R1E; Lots 1 through 3 of Section 32 T11N, R1E;
 3 the NE 1/4 of Section 32 T11N, R1E; the SE 1/4 of Section 32 T11N, R1E; the E 1/2 of the
 4 SW 1/4 of Section 32 T11N, R1E; the SW 1/4 of the SW 1/4 of Section 32 T11N, R1E;
 5 Sections 20 through 28 T11N, R1E; Sections 33 through 35 T11N, R1E, N.M.P.M.

6 Excepting from the above described parcel of land that portion within
 7 Sections 22, 25, 26, 27 and 34 T11N, R1E previously annexed to said City of
 8 Albuquerque by City Ordinance No. O-23, Enactment No. 16-1990 as described and
 9 shown in the Ordinance filed in the office of the County Clerk of Bernalillo County,
 10 New Mexico, May 2, 1990, Book 907, pages 8069 - 8081.

11 B. All of the right-of-way adjoining the land described in this section.
 12 The above described territory is contiguous to the City of Albuquerque.

13 Section 2. ANNEXATION ACCEPTED. The petition and the area specified in
 14 Section 1 above are accepted and the above territory is hereby annexed.

15 Section 3. ZONE MAP AMENDED.

16 A. The zone map adopted by Section 14-16 R.O. 1994 is hereby
 17 amended, establishing SU-1 for Major Public Open Space zoning for the area
 18 described in Section 1 above excepting the land described in "Exhibit A".

19 B. The zone map adopted by Section 14-16 R.O. 1994 is hereby
 20 amended, establishing SU-1 for Airport and Related Facilities zoning for the area
 21 described in "Exhibit A".

22 Section 4. SEVERABILITY CLAUSE. If any section, paragraph, sentence,
 23 clause, word or phrase of this ordinance is for any reason held to be invalid or
 24 unenforceable by any court of competent jurisdiction, such decision shall not affect
 25 the validity of the remaining provisions of this ordinance. The Council hereby
 26 declares that it would have passed this ordinance and each section, paragraph,
 27 sentence, clause, word or phrase thereof irrespective of any provisions being
 28 declared unconstitutional or otherwise invalid.

29 Section 5. EFFECTIVE DATE AND PUBLICATION. This ordinance shall
 30 become effective five or more days after publication in full when a copy of the

Underscored Material - New
 [Bracketed Material] - Deletion

EXHIBIT A

Section 36 T11N, R1E, N.M.P.M. and two parcels of land situate in T11N, R1E and T10N, R1E, N.M.P.M. Bernalillo County, New Mexico. The two parcels being more particularly described by grid bearings and ground distances as follows:

PARCEL 1

Beginning at the northeast corner of the parcel herein described, (a 1 1/2" from pipe) being the closing corner Section 13, T11N, R1E, and Section 18, T11N, R2E, N.M.P.M. and the southerly boundary of the Town of Alameda Grant; thence,

N89°39'43"W, along said southerly boundary of the Town of Alameda Grant, a distance 1021.15' to the 9 1/2 Mile Corner of the Town of Alameda Grant; thence,

N89°32'18"W, along said southerly boundary of the Town of Alameda Grant, a distance of 2642.10' to the 10 Mile Corner of said Town of Alameda Grant; thence,

N89°33'55"W, along said southerly boundary of the Town of Alameda Grant, a distance 2639.70' to the 10 1/2 Mile Corner of the Town of Alameda Grant; thence,

N89°41'06"W, along said southerly boundary of the Town of Alameda Grant, a distance 1417.25' to a point being on the northerly boundary of the partial remaining portion of Section 14, T11N, R1E; thence,

S01° 00'08"W, through said remaining portion of Section 14, and Section 23, T11N, R1E, a distance 6957.65' to a point in said Section 23; thence,

S46°30'00"W, through said Section 23 and Sections 26, 27 and 34, T11N, R1E, a distance 11102.38' to a point on the projected common to said Section 34 and Section 33, T11N, R1E; thence,

S00°59'36"W, along said projected common line, a distance 3157.52' to the projected corner common to Sections 33 and 34, T11N, R1E, and 4 and 3, T10N, R1E; thence,

S00°02'39"W, along the common line of Sections 4 and 3, T10N, R1E, a distance 2156.43 to the Closing Corner of Sections 4 and 3, T10N, R1E, and the northerly boundary of the Town of Atrisco Grant; thence,

S89°57'32"E, along said northerly boundary of the Town of Atrisco Grant, a distance 498.10' to the 8 Mile Corner of the Town of Atrisco Grant; thence,

S89°35'43"E, along said northerly boundary of the Town of Atrisco Grant, a distance 2658.52' to the 7 1/2 Mile Corner of the Town of Atrisco Grant; thence,

S89°46'45"E, along said northerly boundary of the Town of Atrisco Grant, a distance 2133.83' in the Closing Corner of Sections 3 and 2, T10N, R1E; thence,

N00°00'37"E, along the project line common to said Sections 3 and 2, a distance 2144.48' to the projected corner common to Sections 3 and 2, T10N, R1E, and Sections 34 and 35, T11N, R1E; thence,

S89°34'27"E, along the projected line common to Section 35, T11N, R1E, and Section 2, T10N, R1E, a distance 545.37'; thence

N46°30'00"E, through said Section 35, a distance 6645.72' to a point on the projected line common to said Section 35 and Section 36, T11N, R1E; thence,

N00°57'20"E, along said projected line, a distance 700.48' to the corner common to Sections 25, 25, 35 and 26, T11N, R1E; thence,

S89°22'18"E, along the line common to said Sections 25 and 36, a distance 2613.63' to the 1/4 corner common to said Sections 25 and 36; thence,

S89°14'14"E, along the projects line common to said Sections 25 and 36, a distance 2620.01' to the projected corner common to Sections 25 and 36, T11N, R1E and Sections 30 and 31, T11N, R2E; thence,

N00° 27'24"E, along the projected line common to Section 25, T11N, R1E, and Section 30, T11N, R2E, a distance 2643.72' to the 1/4 Corner common to said Sections 25, T11N, R1E, and Section 30, T11N, R2E; thence,

N00°15'09"E, along the line common to said Section 25, T11N, R1E, and Section 30, T11N, R2E, a distance 2642.43' to the Corner common to Sections 24 and 25, T11N, R1E, and Sections 19 and 30, T11N, R2E; thence,

N00°08'07"E, along the line common to Section 24, T11N, R1E, and Section 19, T11N, R2E and Section 13, T11N, R1E and Section 18, T11N, R2E, a distance 7246.77 to the point of beginning and containing 3852.5290 acres more or less.

PARCEL 2

Beginning at the Closing Corner of Sections 1 and 2, T10N, R1E, N.M.P.M. and the norther boundary of the Town of Atrisco Grant (a brass cap); thence,

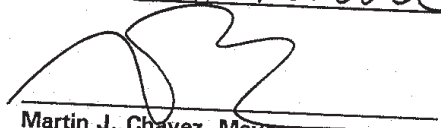
N00°09'00"E, along the project line common to said Sections 1 and 2, a distance 2123.20' to the projected corner common to said Section 1 and 2, T10N, R1E, and Sections 35 and 36, T11N, R1E; thence,

S89°34'27"E, along the line common to said Section 36, T11N, R1E, and Section 1, T10N, R1E, a distance 3962.91'; thence,

1 PASSED AND ADOPTED THIS 17th DAY OF November, 1997
2 BY A VOTE OF: 9 FOR 0 AGAINST.
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Vickie S. Perea, President
City Council

APPROVED THIS 25th DAY OF November, 1997


Martin J. Chavez, Mayor
City of Albuquerque

18
19 ATTEST:

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21 City Clerk
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STATE OF NEW MEXICO
COUNTY OF BERNALILLO
FILED FOR RECORD

CITY OF ALBUQUERQUE
PLANNING DEPARTMENT
DEVELOPMENT SERVICES DIVISION
P. O. BOX 1293
ALBUQUERQUE, NEW MEXICO 87103

Date: January 15, 1998
CERTIFICATE OF ZONING

City of Albuquerque
Open Space Division
P.O. Box 1293
Albuquerque, NM 87103

FILE: Z-96-57/AX-96-8 (Council Bill 0-101)
LEGAL: Section 31, Township 11 North, Range 2 East, a portion of the north ½ of Section 6, Township 10 North, Range 2 east, and the northeast 1/4 of Section 1, Township 10 North, and Range 1 East, N.M.P.M., located west of the Volcanoes and south of the Double Eagle Airport, containing approximately 943 acres. (F-7, G-6, G-7)

The City of Albuquerque expands this request to include Section 36, State of New Mexico Trust Land, and portions of Sections 23 - 27, 34 and 35, the Double Eagle Airport, all in Township 11 North, Range 1 East and to be zoned SU-1 for Airport and Related Facilities, plus portions of Sections 19- 23 and 27 - 35 of Township 11 North and 3 - 6 of Township 10 North, Volcano Ranch Park, all in Range 1 East, N.M.P.M. and to be zoned SU-1 for Major Public Open Space, containing approximately 11,443 acres. (A-3 to A-9, B-3 to B-8, C-2 to C-8, D-2 to D-6, E-2 to E-6, F-2 to F-6, and G-2 to G-6)
Russell Brito, Staff Planner

On November 17, 1997, the City Council approved your request to annex and amend the zone map as it applies to the above-cited property. The possible appeal period having expired, the property status is now changed as follows:

**ANNEXATION AND ESTABLISHMENT OF SU-1
FOR MAJOR PUBLIC OPEN SPACE ZONING AND
SU-1 FOR AIRPORT AND RELATED FACILITIES ZONING**

Successful applicants are reminded that other regulations of the City must be complied with, even after approval of the zoning is secured. Approval of this case does not constitute approval of plans for a building permit. You should take two copies of your plans to the Building & Inspection Division of the City to initiate a building permit.

Sincerely,

Bob Paulsen
FOR Fabrizio Bertoletti
Acting Planning Director

cc: Patrick Chapmam, P.O Box 27666, 87125
Neal Weinberg, AGIS
David Ning, Policy Planning
City Zoning
County Zoning
George King, Maps & Records, Public Works

Z-CERTIFICATION

TWELFTH COUNCIL

COUNCIL BILL NO. F/S 0-101
SPONSORED BY: Alan B. Armijo

ENACTMENT NO. 43-1997

ORDINANCE

ANNEXING 10,859 ACRES MORE OR LESS, LOCATED GENERALLY ON EITHER SIDE OF PASEO DEL VOLCAN NW, ON THE NORTH, SOUTH AND WEST SIDES OF THE SOIL AMENDMENT FACILITY, SOUTH OF THE TOWN OF ALAMEDA GRANT, NORTH OF THE TOWN OF ATRISCO GRANT, WEST OF THE EXISTING CITY LIMITS AND EAST OF THE RIO PUERCO ESCARPMENT AND THE TOWN OF ATRISCO GRANT TO THE CITY OF ALBUQUERQUE; AMENDING THE ZONE MAP TO ESTABLISH SU-1 FOR MAJOR PUBLIC OPEN SPACE ZONING AND SU-1 FOR AIRPORT AND RELATED FACILITIES ZONING.

BE IT ORDAINED BY THE COUNCIL, THE GOVERNING BODY OF THE CITY OF ALBUQUERQUE:

Section 1. AREA PROPOSED FOR ANNEXATION. The owners of a majority of the area annexed hereby presented a properly signed petition to annex the following territory: 11,756 acres more or less, located generally on either side of Paseo Del Volcan NW, on the north, south and west sides of the Soil Amendment Facility, South of the Town of Alameda Grant, North of the Town of Atrisco Grant, west of the existing City limits and east of the Rio Puerco Escarpment and the Town of Atrisco Grant and more particularly described as follows:

A. Section 31 T11N, R2E; a portion of the NE 1/4 of Section 6 T10N, R2E; the NE 1/4 of Section 1 T10N, R1E; Lots 1 through 8 of Section 6 T10N, R2E; Section 6 T10N, R2E; Lots 1 through 8 of Section 1 T10N, R1E; Lots 1 through 8 of Section 3 T10N, R1E; Lots 1 through 8 of Section 4 T10N, R1E; Lots 1 through 8 of Section 5 T10N, R1E; Lots 1 and 2 of Section 6 T10N, R1E; Lots 1 through 4 of Section 13 T11N, R1E; Lots 1 through 4 of Section 14 T11N, R1E; Lots 1 through 4 of Section 15 T11N, R1E; Lots 1 through 4 of Section 16 T11N, R1E; Lots 2 through 5 of Section 17 T11N, R1E; Lots 1 through 3 of Section 29 T11N, R1E; the NE 1/4 of Section 29 T11N, R1E; the NE 1/4 of the NW 1/4 of Section 29 T11N, R1E; the SE 1/4 of Section 29 T11N, R1E; Lot 1 of Section 31 T11N, R1E; Lots 1 through 3 of Section 32 T11N, R1E; the NE 1/4 of Section 32 T11N, R1E; the SE 1/4 of Section 32 T11N, R1E; the E 1/2 of the SW 1/4 of Section 32 T11N, R1E; the SW 1/4 of the SW 1/4 of Section 32 T11N, R1E; Sections 20 through 28 T11N, R1E; Sections 33 through 35 T11N, R1E, N.M.P.M.

Excepting from the above described parcel of land that portion within Section 22, 25, 26, 27 and 34 T11N, R1E previously annexed to said City of Albuquerque by City Ordinance No. O-23, Enactment No. 15-1990 as described and shown in the Ordinance filed in the office of the County Clerk of Bernalillo County, New Mexico, May 2, 1990, Book 907, pages 8069-8081.

B. All of the right-of-way adjoining the land described in this section. The above described territory is contiguous to the City of Albuquerque.

Section 2. ANNEXATION ACCEPTED. The petition and the area specified in Section 1 above are accepted and the above territory is hereby annexed.

Section 3. ZONE MAP AMENDED.

A. The zone map adopted by Section 14-16 R.O. 1994 is hereby amended, establishing SU-1 for Major Public Open Space zoning for the area described in Section 1 above excepting the land described in "Exhibit A".

B. The zone map adopted by Section 14-16 R.O. 1994 is hereby amended, establishing SU-1 for Airport and Related Facilities zoning for the area described in "Exhibit A".

Section 4. SEVERABILITY CLAUSE. If any section, paragraph, sentence, clause, word or phrase of this ordinance is for any reason held to be invalid or unenforceable by any court of competent jurisdiction, such decision shall not affect the validity of the remaining provisions of this ordinance. The Council hereby declares that it would have passed this ordinance and each section, paragraph, sentence, clause, word or phrase thereof irrespective of any provisions being declared unconstitutional or otherwise invalid.

Section 5. EFFECTIVE DATE AND PUBLICATION. This ordinance shall become effective five or more days after publication in full when a copy of the ordinance and a plat of the territory hereby annexed is filed in the office of the County Clerk.

EXHIBIT A

Section 36 T11N, R1E, N.M.P.M. and two parcels of land situate in T11N, R1E and T10N, R1E, N.M.P.M. Bernalillo County, New Mexico. The two parcels being more particularly described by grid bearings and ground distances as follows:

PARCEL 1

Beginning at the northeast corner of the parcel herein described, (a 1 1/2" from pipe) being the closing corner Section 13, T11N, R1E; and Section 18, T11N, R2E, N.M.P.M. and the southerly boundary of the Town of Alameda Grant; thence,

N89°39'43"W, along said southerly boundary of the Town of Alameda Grant, a distance 1021.15' to the 9 1/2 Mile Corner of the Town of Alameda Grant; thence,

N89°32'18"W, along said southerly boundary of the Town of Alameda Grant, a distance of 2642.10' to the 10 Mile Corner of said Town of Alameda Grant; thence,

N89°33'55"W, along said southerly boundary of the Town of Alameda Grant, a distance 2639.70' to the 10 1/2 Mile Corner of the Town of Alameda Grant; thence,

N89°41'06"W, along said southerly boundary of the Town of Alameda Grant, a distance 1417.25' to a point being on the northerly boundary of the partial remaining portion of Section 14, T11N, R1E; thence,

S01 degrees 00'08"W, through said remaining portion of Section 14, and Section 23, T11N, R1E, a distance 6957.65' to a point in said Section 23; thence,

S48°30'00"W, through said Section 23 and Section 26, 27 and 34, T11N, R1E, a distance 11102.38' to a point on the projected common to said Section 34 and Section 33, T11N, R1E; thence,

S00°59'36"W, along said projected common line, a distance 3157.52' to the projected corner common to Sections 33 and 34, T11N, R1E, and 4 and 3, T10N, R1E; thence,

S00°02'39"W, along the common line of Sections 4 and 3, T10N, R1E, a distance 2156.43' to the Closing Corner of Sections 4 and 3, T10N, R1E, and the northerly boundary of the Town of Atrisco Grant; thence,

S89°57'32"E, along said northerly boundary of the Town of Atrisco Grant, a distance 498.10' to the 8 Mile Corner of the Town of Atrisco Grant; thence,

S89°35'43"E, along said northerly boundary of the Town of Atrisco Grant, a distance 2658.52' to the 7 1/2 Mile Corner of the Town of Atrisco Grant; thence,

N00°00'37"E, along the project line common to said Sections 3 and 2, a distance 2144.48' to the projected corner common to Sections 3 and 2, T10N, R1E, and Sections 34 and 35, T11N, R1E; thence,

S89°34'27"E, along the projected line common to Section 35, T11N, R1E, and Section 2, T10N, R1E, a distance 545.37'; thence,

N46°30'00"E, through said Section 35, a distance 6645.72' to a point on the projected line common to said Section 35 and Section 36, T11N, R1E; thence,

N00°57'20"E, along said projected line, a distance 700.48' to the corner common to Sections 26, 25, 35 and 26, T11N, R1E; thence,

PLANNING DEPARTMENT

Date 1-17-98 FAX 76-0

THIS ANNEXATION HAS BEEN COMPLETED

The attached sketch shows the area which has been annexed by City Commission Resolution on 11-17-97 and zoning established by Ordinance # 0-101, published on 11-25-97 (date) and which was considered by the Planning and City Commissions as Ax-96-8 Z-96-87

If the area was platted simultaneously, the subdivision number is S-_____. Other related cases, if any, are: _____ Please change your records accordingly.

S89°22'18"E, along the line common to said Sections 25 and 36, a distance 2613.63' to the 1/4 corner common to said Sections 25 and 36; thence,

S89°14'14"E, along the projects line common to said Sections 25 and 36, a distance 2620.01' to the projected corner common to Sections 25 and 36, T11N, R1E and Sections 30 and 31, T11N, R2E; thence,

N00 degrees 27'24"E, along the projected line common to Section 25, T11N, R1E, and Section 30, T11N, R2E, a distance 2543.72' to the 1/4 Corner common to said Sections 25, T11N, R1E, and Section 30, T11N, R2E; thence,

N00°15'09"E, along the line common to said Section 25, T11N, R1E, and Section 30, T11N, R2E, a distance 2642.48' to the Corner common to Sections 24 and 25, T11N, R1E, and Sections 19 and 30, T11N, R2E; thence,

N00°08'07"E, along the line common to Section 24, T11N, R1E, and Section 19, T11N, R2E and Section 13, T11N, R1E and Section 18, T11N, R2E, a distance 7246.77' to the point of beginning and containing 3852.5290' acres more or less.

PARCEL 2

Beginning at the Closing Corner of Sections 1 and 2, T10N, R1E, N.M.P.M. and the northern boundary of the Town of Atrisco Grant (a brass cap); thence,

N00°09'00"E, along the projected corner common to said Section 1 and 2, T10N, R1E, and Sections 35 and 36, T10N, R1E; thence,

S89°34'27"E, along the line common to said Section 36, T11N, R1E, and Section 1, T10N, R1E, a distance 3962.8'; thence,

S00°16'41"W, through said Section 1, T10N, R1E, and distance 2108.22' to a point on the northerly boundary of the Town of Atrisco; thence,

N89°47'27"W, along said northerly boundary of the Town of Atrisco Grant, a distance 821.38' to the 5 1/2 Mile Corner of the Town of Atrisco Grant; thence,

N89°27'26"W, along said northerly boundary at the Town of Atrisco Grant, a distance 3136.98' to the point of beginning and containing 192.3624 acres more or less.

PASSED AND ADOPTED THIS 17th DAY OF November, 1997
BY A VOTE OF 9 FOR 0 AGAINST.

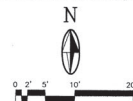
s/Vickie S. Perea, President
City Council

APPROVED THIS 25TH DAY OF NOVEMBER, 1997
s/Martin J. Chavez, Mayor
City of Albuquerque

ATTEST:
s/Millie Santillanes
City Clerk

10-10-98
12-12-98
10-10-98
12-12-98

2	ROADWAY CURVE LISTING
	Included angle = 66-06-25
	Radius = 30.00'
	Tangent length = 19.52'
	Arc length = 34.61'
	Chord length = 32.73'
	External secant = 5.79'
	Mid ordinate = 4.86'
	Degree of curve = 190-59-09



AS BUILT INFORMATION	
CONTRACTOR	
WORK	
STARTED BY	DATE
ACCEPTANCE BY	DATE
FIELD	DATE
VERIFICATION BY	DATE
CORRECTED BY	DATE
MICRO-FILM INFORMATION	
RECORDED BY	DATE
NO.	

[illegible][illegible][illegible]

DESIGNED BY	JMP
DRAWN BY	DKS
CHECKED BY	NO

11/26/03

----- NEW CHAINLINK FENCE

④ ALL DISTURBED AREAS, SHALL BE REVEGETATED AND RESTABILIZED IN ACCORDANCE WITH THE SPECIFICATIONS SEE LANDSCAPING PLAN

CITY OF ALBUQUERQUE
PUBLIC WORKS DEPARTMENT
ENGINEERING DEVELOPMENT GROUP

TITLE: DOUBLE EAGLE II AIRPORT INFRASTRUCTURE DEVELOPMENT
SOUTH LIFT STATION SITE DEVELOPMENT PLAN
FOR BUILDING PERMIT

Design Review Committee	City Engineer Approval	Last Design Update	Mo./Day/Yr.	Mo./Day/Yr.
City Project No.	Zone Map No.	Sheet	Of	
		2	7	



City of Albuquerque
Planning Department
Development Services Division
P.O. Box 1293
Albuquerque, New Mexico 87103

Date: January 16, 2004

OFFICIAL NOTIFICATION OF DECISION

FILE: **Project # 1003125***
03EPC-02054 EPC Site Development Plan-
Subdivision

Ranch Joint Venture
3613 Rio Rancho Rd. Ste H
Albuq. NM 87114

LEGAL DESCRIPTION: for all or a portion of Tracts D and a portion of S, **Lands of Double Eagle II Airport**, zoned SU-1 for Airport Related Uses, located on DOUBLE EAGLE II AIRPORT, west of PASEO DEL VOLCAN and NORTH OF SHOOTING RANGE, ACCESS Road, containing approximately 300 acre(s). (F-5) Chris Hyer, Staff Planner

On January 15, 2004 the Environmental Planning Commission voted to approve Project 1003125/ 03EPC 02054, a request for site development plan for subdivision, for a 300 site legally described as Tracts D and a portion of S (Section 1), Bulk Land Plat of Double Eagle II Airport and Adjacent Lands, Book 2002C, Page 228, zoned SU-1 for airport and related facilities, located at the Double Eagle II Airport, the north side of the Shooting Range Access Road, to the west of old Paseo del Volcan between the existing runways, based on the following Findings and subject to the following Conditions:

FINDINGS:

1. This is a request for Site Development Plan for Subdivision for 300-acres of lands at the *Double Eagle II Airport*, Tract D and a portion of Section I of tract S as shown on the Bulk Land Plat of Double Eagle II Airport and Adjacent Lands, Book 2002C, Page 228. Tract D is owned by the applicant, Ranch Joint Ventures, and is 200 acres. Tract S, Section I is owned by the City's Aviation Department and is 192.36 acres. Only 100-acres of Tract S is a part of this request. The Lands are located west of Runway 17-35, adjacent to that runway's taxiway network, south of the inner angle formed by the two current runways at *Double Eagle II Airport* and north of Shooting Range Access Road.
2. The request is in compliance with the *Comprehensive Plan's* Goal of promoting Economic Development. (Goal B.2, D.6, Policies a, b & g)

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3. This request does not contradict the *Comprehensive Plan* Reserve Area Goal by not incorporating a residential component. Consequently, the reference to the planned communities is not applicable to the airport -- the goal is specific to future planned communities, not the existing airport. The request proposes improvements to an existing facility that predated the "Reserve Area" designation.
4. This request supports the *West Side Strategic Plan's* concept of establishing an employment center at the *Double Eagle II Airport*. (Policies 1.6, 3.76, 3.85, 6.27)
5. Design Guidelines are a part of this submittal and any future Site Development Plans for Building Permit are requested to be delegated to the DRB and not require a public hearing provided they are consistent with this Site Plan for Subdivision and Design Guidelines.
6. The Aerospace Technology Park will be the home of Eclipse Aviation, a start-up airplane manufacturer, on the northern half of the site. The southern half of the site will be created into 30 separate lots, 1 to 10 acres in size, for businesses who desire to locate at the airport..
7. All but 5 of the lots within the Aerospace Technology Park will be accessible by car or truck, or airplane via private taxiways that connect the airport to the various businesses in the Tech-Park.

CONDITIONS:

1. The EPC delegates final sign-off authority of this site development plan to the Development Review Board (DRB). The DRB is responsible for ensuring that all EPC Conditions have been satisfied and that other applicable City requirements have been met. A letter shall accompany the submittal, specifying all modifications that have been made to the site plan since the EPC hearing, including how the site plan has been modified to meet each of the EPC conditions. Unauthorized changes to this site plan, including before or after DRB final sign-off, may result in forfeiture of approvals.
2. The following recommended conditions by the City Engineer:
 - a. requirements of previous actions taken by the EPC and the DRB must be completed and /or provided for.
 - b. The Developer is responsible for permanent improvements to the transportation facilities adjacent to the proposed site development plan for building permit. Those improvements will include any additional right-of-way requirements, paving, curb and gutter, sidewalk and ADA accessible ramps that have not already been provided for.
 - c. Traffic Impact Study is required and must be submitted prior to any DRB action or prior to EPC Site Development Plan for Building Permit action, whichever is made first.
 - d. At a minimum, access to site shall meet DPM requirements but may be modified by the Traffic Engineer depending on the results of the TIS.
 - e. Requirements for street right-of-way, geometry, connections and signalization shall be determined by the Traffic Engineer based on the TIS and DPM.

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- f. All access points to be designed per DPM standards.
 - g. Provide cross access agreement between tracts.
 - h. Site shall comply and be designed per DPM Standards.
 - i. The note on sheet 1 of the Site Development Plan regarding the delegation of future site development plans for building permits, shall read Development Review Board (DRB) rather than "Administrative Review".
 - j. Platting should be a concurrent DRB action.
3. Under roadway access the text will be changed as follows:
It is anticipated that Aerospace Parkway will be an 86-foot right of way. It is further anticipated that access into specific development areas will be from a series of cul-de-sac streets with 60-foot rights-of-way. Specific right-of-way width for Aerospace Parkway and the cul-de-sacs shall be determined before final approval at the Development Review Board. The cul-de-sac streets also function as the primary utility corridors and will accommodate water, sewer, and storm drains.
4. The design guidelines and street sections on sheet, letter M shall be changed accordingly to match the Site Plan prior to final approval at the Development Review Board.
5. The Eclipse Aviation property (northeastern most 150 acres) is not governed by the following design standards. Eclipse Aviation shall prepare a separate Site Development Plan for Subdivision and Design Standards to be reviewed and approved by the Environmental Planning Commission as part of their internal master planning." Eclipse Aviation design standards shall be accompanied by the Aerospace Technology Park design standards to ensure compatibility.

IF YOU WISH TO APPEAL/PROTEST THIS DECISION, YOU MUST DO SO BY JANUARY 30, 2004 IN THE MANNER DESCRIBED BELOW. A NON-REFUNDABLE FILING FEE WILL BE CALCULATED AT THE LAND DEVELOPMENT COORDINATION COUNTER AND IS REQUIRED AT THE TIME THE APPEAL IS FILED.

Appeal to the City Council: Persons aggrieved with any determination of the Environmental Planning Commission acting under this ordinance and who have legal standing as defined in Section 14-16-4-4.B.2 of the City of Albuquerque Comprehensive Zoning Code may file an appeal to the City Council by submitting written application on the Planning Department form to the Planning Department within 15 days of the Planning Commission's decision. The date the determination in question is issued is not included in the 15-day period for filing an appeal, and if the fifteenth day falls on a Saturday, Sunday or holiday as listed in the Merit System Ordinance, the next working day is considered as the deadline for filing the appeal. The City Council may decline to hear the appeal if it finds that all City plans, policies and ordinances have been properly followed. If it decides that all City plans, policies and ordinances have not been properly followed, it shall hear the appeal. Such appeal, if heard, shall be heard within 45 days of its filing.

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YOU WILL RECEIVE NOTIFICATION IF ANY OTHER PERSON FILES AN APPEAL. IF THERE IS NO APPEAL, YOU CAN RECEIVE BUILDING PERMITS AT ANY TIME AFTER THE APPEAL DEADLINE QUOTED ABOVE, PROVIDED ALL CONDITIONS IMPOSED AT THE TIME OF APPROVAL HAVE BEEN MET. SUCCESSFUL APPLICANTS ARE REMINDED THAT OTHER REGULATIONS OF THE CITY MUST BE COMPLIED WITH, EVEN AFTER APPROVAL OF THE REFERENCED APPLICATION(S).

Successful applicants should be aware of the termination provisions for Site Development Plans specified in Section 14-16-3-11 of the Comprehensive Zoning Code. Generally plan approval is terminated 7 years after approval by the EPC.

Sincerely,


for Victor J. Chavez
Planning Director

VJC/CH/ac

cc: Consensus Planning, 924 Park Ave. SW, Albuquerque, NM 87102



City of Albuquerque
Planning Department
Development Review Division
P.O. Box 1293
Albuquerque, New Mexico 87103

Date: May 20, 2005

OFFICIAL NOTIFICATION OF DECISION

FILE: Project # 1003125*
05EPC-00566 EPC Site Development Plan-
Subdivision

Eclipse Aviation
2503 Clark Carr Loop SE
Albuq. NM 87106

LEGAL DESCRIPTION: for all or a portion of Tract D-1, **Double Eagle II Airport**, zoned SU-1 for Airport Related Facilities, located on **DOUBLE EAGLE II AIRPORT**, between **PASEO DEL VOLCAN** and **SHOOTING RANGE ACCESS RD.**, containing approximately 150 acre(s). (F-6) Stephanie Shumsky, Staff Planner

On May 19, 2005 the Environmental Planning Commission voted to approve Project 1003125/ of 05EPC-00566, a site development plan for subdivision and design standards, for Tract D-1 of the Bulk Land Plat for Aerospace Technology Park, Double Eagle II Airport, based on the preceding Findings and subject to the following Conditions:

FINDINGS:

1. This is a request for site development plan for subdivision with design standards for Eclipse Aviation on a 150-acre tract located within the Double Eagle II Master Plan area and adjacent to the future Aerospace Technology Park. The subject site is legally known as Tract D-1 of the Bulk Land Plat for Aerospace Technology Park, Double Eagle II Airport.
2. Eclipse Aviation is an airplane manufacturer with design and prototype development capabilities. Many Eclipse Aviation suppliers will be located in the Aerospace Technology Park, just south of the subject site.
3. The proposed design standards comply with associated sections of the Zoning Code and in general are more restrictive than Zoning Code requirements.
4. An Air Quality Impact Analysis is required and will be provided as part of the overall Environmental Impact Assessment.

5. A Traffic Impact Study is required and has been reviewed for this proposal.
6. Several *Comprehensive Plan* policies are furthered by this request:
 - a. The Activity Center goal and policy a are furthered because the development of Eclipse Aviation will occur in an area already designated as a Special Activity Center. The concentration of employment opportunities in this area is appropriate for a designated Activity Center. Policy a is furthered because of the unique nature of the Eclipse Aviation manufacturing facility.
 - b. The Environmental Protection and Heritage Conservation, Air Quality goal and policies a and i are furthered by this request because an Air Quality Impact Assessment will be completed as part of a larger Environmental Impact Assessment of the entire Eclipse Aviation site and surrounding sites. Policy a is furthered by the City's enforcement of air quality standards and the use of this information in decision making will further Policy i.
 - c. The Community Resource Management, Energy Management goal and policies a, b, and c will be furthered because Eclipse Aviation is proposing the future use of wind, solar and other alternative energy resources.
 - d. The Economic Development goal and policies a, b and g are furthered because Eclipse Aviation will rely on local and out-of-town suppliers and will provide diversified economic development with a variety of service, manufacturing, and high-tech employment opportunities. Eclipse Aviation is located in a Special Activity Center and concentrations of employment are encouraged in activity centers (policy g).
7. Several *West Side Strategic Plan* goals, objectives, and policies are furthered by this request:
 - a. Goal 12 of the *West Side Strategic Plan* will be furthered by this request because Eclipse Aviation is committed to and will provide long-term sustainable development on the West Side.
 - b. Objective 1 and 8 are furthered because Eclipse Aviation will promote and provide large-scale employment and training opportunities on the west side, thereby minimizing the need for cross-metro trips (for west side residents).
 - c. Policy 1.6 is furthered because Eclipse Aviation will facilitate the realization of large-scale employment opportunities on the west side.
 - d. Policy 1.8 is furthered because the proposed design standards provide specific design criteria for all facilities within the Eclipse Aviation campus.
 - e. Policy 3.85 is furthered because the creation of base or export jobs and regionally based jobs on the West Side will be facilitated by Eclipse Aviation aircraft development and manufacturing operations.
8. The Double Eagle II Master Plan is furthered because the proposed site plan for subdivision and design standards for Eclipse Aviation comply with the Double Eagle II Master Plan (Project 1001656/02EPC-00989). In addition, the design standards for Eclipse Aviation compliment the standards approved for the Aerospace Technology Park to the south (Project 1003125/03EPC-02054).

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MAY 19, 2005
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9. Design standards are part of this submittal and future Site Development Plans for Building Permit are delegated to Building Permit and do not require a public hearing or review by the DRB, provided they are consistent with this Site Plan for Subdivision and Design Standards.
10. There is no known opposition to this request.

CONDITIONS:

1. The EPC delegates final sign-off authority of this site development plan to the Development Review Board (DRB). The DRB is responsible for ensuring that all EPC Conditions have been satisfied and that other applicable City requirements have been met. A letter shall accompany the submittal, specifying all modifications that have been made to the site plan since the EPC hearing, including how the site plan has been modified to meet each of the EPC conditions. Unauthorized changes to this site plan, including before or after DRB final sign-off, may result in forfeiture of approvals.
2. An Air Quality Impact Assessment is required and shall be provided prior to final sign off by the DRB. AQIA requirements will become the responsibility of the Aviation Department as part of the upcoming EA at Double Eagle II Airport and the AQIA must be complete no later than December 2006.
3. The Transportation Development (City Engineer/Planning Department) has included the following conditions of approval:
 - All the requirements of previous actions taken by the EPC and/or the DRB must be completed and /or provided for.
 - The Developer is responsible for permanent improvements to the transportation facilities adjacent to the proposed site development plan for subdivision. Those improvements will include any additional right-of-way requirements, paving, curb and gutter, sidewalk and ADA accessible ramps that have not already been provided for. All public infrastructure constructed within public right-of-way or public easements shall be to City Standards. Those Standards will include but are not limited to sidewalks (std. dwg. 2430), driveways (std. dwg. 2425), private entrances (std. dwg. 2426) and wheel chair ramps (std. dwg. 2441).
 - Completion of the required TIS mitigation measures, per Transportation Development Staff, must be completed if assumed to be in place for the current TIS for this site (i.e. street improvements recommended in the Traffic Impact Study (TIS)).
 - Provide more detail of public to private connection between Aerospace Technology Park and Eclipse Aviation Campus via Aerospace Parkway.
 - Site plan shall comply and be designed per DPM Standards.

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4. All temporary buildings shall comply with the design standards set forth for permanent buildings in relation to Material and color, site planning and building Design Standards, with the exception that tensile and/or fabric structures will be permitted. All temporary parking areas shall comply with the design standards set forth for parking with the exception that landscaping will not be required.

IF YOU WISH TO APPEAL/PROTEST THIS DECISION, YOU MUST DO SO BY **JUNE 3, 2005** IN THE MANNER DESCRIBED BELOW. A NON-REFUNDABLE FILING FEE WILL BE CALCULATED AT THE LAND DEVELOPMENT COORDINATION COUNTER AND IS REQUIRED AT THE TIME THE APPEAL IS FILED. IT IS NOT POSSIBLE TO APPEAL EPC RECOMMENDATIONS TO CITY COUNCIL; RATHER, A FORMAL PROTEST OF THE EPC's RECOMMENDATION CAN BE FILED WITHIN THE 15 DAY PERIOD FOLLOWING THE EPC's DECISION.

Appeal to the City Council: Persons aggrieved with any determination of the Environmental Planning Commission acting under this ordinance and who have legal standing as defined in Section 14-16-4-4.B.2 of the City of Albuquerque Comprehensive Zoning Code may file an appeal to the City Council by submitting written application on the Planning Department form to the Planning Department within 15 days of the Planning Commission's decision. The date the determination in question is issued is not included in the 15-day period for filing an appeal, and if the fifteenth day falls on a Saturday, Sunday or holiday as listed in the Merit System Ordinance, the next working day is considered as the deadline for filing the appeal. The City Council may decline to hear the appeal if it finds that all City plans, policies and ordinances have been properly followed. If they decide that all City plans, policies and ordinances have not been properly followed, they shall hear the appeal. Such appeal, if heard, shall be heard within 45 days of its filing.

YOU WILL RECEIVE NOTIFICATION IF ANY PERSON FILES AN APPEAL. IF THERE IS NO APPEAL, YOU CAN RECEIVE BUILDING PERMITS AT ANY TIME AFTER THE APPEAL DEADLINE QUOTED ABOVE, PROVIDED ALL CONDITIONS IMPOSED AT THE TIME OF APPROVAL HAVE BEEN MET. SUCCESSFUL APPLICANTS ARE REMINDED THAT OTHER REGULATIONS OF THE CITY MUST BE COMPLIED WITH, EVEN AFTER APPROVAL OF THE REFERENCED APPLICATION(S).

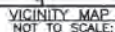
Successful applicants should be aware of the termination provisions for Site Development Plans specified in Section 14-16-3-11 of the Comprehensive Zoning Code. Generally plan approval is terminated 7 years after approval by the EPC

Sincerely,

for Richard Dineen
Planning Director

RD/SS/ac

cc: Dekker/Perich/Sabatini, 6801 Jefferson NE, Suite 100, Albuquerque, NM 87109



ZONE ATLAS MAPS
C-4-Z & C-5-Z
D-5-Z & D-6-Z
E-4-Z, E-5-Z & E-6-Z
G-4-Z, G-5-Z & G-6-Z

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Page: 1 of 5
11/13/2006 03:4
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PROJECT # 1001656
APP# 06DRB-01510

DISCLOSURE STATEMENT

PURPOSE OF PLAT

1. TO CREATE 2 TRACTS INCLUDING 5 PARCELS FROM EXISTING 2 TRACTS, ELIMINATE LOT LINES AS SHOWN HEREON AND GRANT BLANKET EASEMENTS

DATA

1. NEW CASE NO. : BULK LAND VARIANCE NO.
2. GROSS SUBDIVISION ACREAGE 3489.0219 ACRES
3. DATE OF SURVEY: 6/2006
4. TALOS LOG NO: 2006271931

NOTES

1. BASIS OF BEARINGS IS GRID NORTH, NEW MEXICO STATE PLANE COORDINATE SYSTEM, PROJECT COORDINATES BASED ON CITY OF ALBUQUERQUE CONTROL SYSTEM (NO 27 CENTRAL ZONE, NGVD 29).
2. DISTANCES ARE GROUND DISTANCES.
3. BEARINGS AND DISTANCES ARE THE SAME AS RECORD.
4. TRACT "L" AND "M" ARE ZONED SU-1 AIRPORT & RELATED FACILITIES PER EPO APPROVAL 1001656/03EPC00915.
5. BASIS OF BOUNDARY IS FROM THE FOLLOWING REFERENCE DOCUMENTS:
A. BULK LAND PLAT OF DOUBLE EAGLE II AND ADJACENT LANDS, FILED IN THE OFFICE OF THE BERNALILLO COUNTY CLERK IN BOOK 2002, PAGE 228.
6. MAP OF F 10 N, E 1 M, N.M.P.M. PREPARED BY THE SURVEYOR GENERAL'S OFFICE, SANTA FE, N.M., NOV. 20, 1875 AND ON FILE WITH THE U.S. BUREAU OF LAND MANAGEMENT.
7. PLAT SHOWS ALL EASEMENTS OF RECORD PROVIDED BY A DOCUMENT SEARCH BY ALBUQUERQUE TITLE CO, INC., FILE NO. 1881791 (ON NOVEMBER 1, 2000).
8. BLANKET DRAINAGE EASEMENTS ARE HEREBY GRANTED TO THE CITY OF ALBUQUERQUE ON TRACTS N-1, N-2 & O-1. FUTURE SUBDIVISION ACTIONS MAY FURTHER DEFINE THESE BLANKET DRAINAGE EASEMENTS. THESE BLANKET DRAINAGE EASEMENTS ARE FOR THE BENEFIT OF ALL TRACTS CREATED BY THIS PLAT AND ARE TO BE MAINTAINED BY EACH RESPECTIVE OWNER.
9. JOINT USE OF PUBLIC UTILITY EASEMENTS SHOWN ON THIS PLAT ARE GRANTED FOR THE COMMON AND JOINT USE OF:
A. GROUND FOR INSTALLATION, MAINTENANCE AND SERVICE OF ALL BURIED AND AERIAL COMMUNICATION LINES AND OTHER RELATED EQUIPMENT AND FACILITIES REASONABLY NECESSARY TO PROVIDE COMMUNICATION SERVICES, INCLUDING BUT NOT LIMITED TO ABOVE GROUND PEDESTALS AND CLOSURES.
10. CABLE TV FOR INSTALLATION, MAINTENANCE AND SERVICE OF SUCH LINES, CABLE AND OTHER RELATED EQUIPMENT AND FACILITIES REASONABLY NECESSARY TO PROVIDE CABLE TV SERVICE.
INCLUDED, IS THE RIGHT TO BUILD, REBUILD, CONSTRUCT, RECONSTRUCT, LOCATE, RELOCATE, CHANGE, MAINTAIN, IMPROVE, NOW OPERATE AND MAINTAIN FACILITIES FOR THE PURPOSES DESCRIBED ABOVE, TOGETHER WITH FREE ACCESS TO, FROM, AND OVER SAID RIGHT OF WAY AND EASEMENT, WITH THE RIGHT AND PRIVILEGE OF GOING UPON, OVER AND ACROSS ADJOINING LANDS OF GRANTOR FOR THE PURPOSES SET FORTH HEREIN, WITH THE RIGHT TO UTILIZE THE RIGHT OF WAY AND EASEMENT TO EXTEND SERVICES TO CUSTOMERS OF GRANTEE, AND TO TRIM AND REMOVE TREES, SHRUBS OR BUSHES WHICH INTERFERE WITH THE PURPOSES SET FORTH HEREIN, NO BUILDING, SIGN, POOL (ABOVE GROUND OR SUBSURFACE), HOT TUB, CONCRETE OR WOOD POOL DECKING, OR OTHER STRUCTURE SHALL BE ERRECTED OR CONSTRUCTED ON SUD EASEMENTS, NOR SHALL ANY WELL BE DRILLED OR OPERATED THEREON.
11. IN APPROVING THIS PLAT, THE UTILITY COMPANIES DID NOT CONDUCT A TITLE SEARCH OF THE PROPERTIES SHOWN HEREON. CONSEQUENTLY, THE UTILITY COMPANIES DO NOT WAIVE OR RELEASE ANY EASEMENT OR EASEMENT RIGHTS WHICH MAY HAVE BEEN GRANTED BY PRIOR PLAT, REPLAT OR OR OTHER DOCUMENT AND WHICH ARE NOT SHOWN ON THIS PLAT.
12. PNM GAS SERVICES FOR INSTALLATION, MAINTENANCE AND SERVICE OF NATURAL GAS LINES, VALVES AND OTHER EQUIPMENT AND FACILITIES REASONABLY NECESSARY TO PROVIDE NATURAL GAS.
13. PNM ELECTRICAL SERVICES FOR THE INSTALLATION, MAINTENANCE AND SERVICE OF OVERHEAD AND UNDERGROUND ELECTRICAL LINES, TRANSFORMERS AND OTHER EQUIPMENT, FIXTURES STRUCTURES AND RELATED FACILITIES REASONABLY NECESSARY TO PROVIDE ELECTRIC SERVICE.

LEGAL DESCRIPTION

TRACTS "A" AND "L" OF BULK LAND PLAT OF DOUBLE EAGLE II AIRPORT AND ADJACENT LANDS
BK-2002C, PG-228, FILED 07/10/2003.

OWNERS' FREE CONSENT AND DEDICATION

THE SUBDIVISION HEREON DESCRIBED IS WITH THE FREE CONSENT AND IN ACCORDANCE WITH THE DESIRES OF THE UNDERSIGNED OWNERS AND/OR PROPRIETORS THEREOF AND SAID OWNERS AND/OR PROPRIETORS DO HEREBY GRANT TO THE CITY OF ALBUQUERQUE, EASEMENTS FOR PUBLIC STREET RIGHT-OF-WAY, ACCESS, DRAINAGE, AND UTILITIES AS INDICATED HEREIN, INCLUDING THE RIGHT TO CONSTRUCT, OPERATE, INSPECT, AND MAINTAIN FACILITIES THEREIN. FURTHER, SAID OWNERS AND/OR PROPRIETORS DO HEREBY GRANT OTHER PRIVATE EASEMENTS AS SHOWN AND INDICATED.

OWNER FOR TRACT "A", NOW BEING CREATED AND CONFIGURED AS TRACT "A-1",
CITY OF ALBUQUERQUE A MUNICIPAL CORPORATION.

BRUCE J. PERLMAN, PhD
CHIEF ADMINISTRATIVE OFFICER
CITY OF ALBUQUERQUE

STATE OF NEW MEXICO) SS)
COUNTY OF BERNALILLO)

THIS INSTRUMENT WAS ACKNOWLEDGED BEFORE ME ON October 9 BY BRUCE J. PERLMAN, PhD
AS CHIEF ADMINISTRATIVE OFFICER OF THE CITY OF ALBUQUERQUE, NEW MEXICO, A MUNICIPAL
CORPORATION, ON BEHALF OF SAID CORPORATION.

Felicia Lison
NOTARY PUBLIC
[MY COMMISSION EXPIRES: 1-22-80]

OFFICIAL SEAL
Felicia Giron
 NOTARY PUBLIC
 STATE OF NEW MEXICO
 My Commission Expires: **1-29-2011**

OWNER FOR TRACT "L", NOW BEING CREATED AND CONFIGURED AS TRACT "L-1"
NEW MEXICO STATE LAND OFFICE.

Patrick H. Lyons
PATRICK H. LYONS
COMMISSIONER OF PUBLIC LANDS

NOTICE OF SUBDIVISION PLAT CONDITION

A. FOR DESIGN VARIANCE: THE PROCEDURE FOR A SUBMISSION FOR WHICH A DESIGN VARIANCE IS REQUESTED IS THE SAME AS FOR STANDARD SUBMISSION (SEE PAGE 2-6). HOWEVER, A DESIGN VARIANCE REQUEST SHOULD BE SHOWN ON THE SUBMISSION APPLICATION FORM.

B. FOR A VARIANCE TO SUBDIVISION IMPROVEMENTS GUARANTEE. THE ONLY BASIS UPON WHICH THE DRI MAY WAIVE THE REQUIRED SUBDIVISION IMPROVEMENTS GUARANTEE IS FOR SUBDIVISIONS INTENDED TO FACILITATE BULK LAND TRANSFERS. THE REVIEW AND APPROVAL PROCEDURE FOR SUCH SUBDIVISIONS INVOLVES THE SAME STEPS AS FOR STANDARD SUBDIVISIONS. HOWEVER, THERE ARE SEVERAL SIGNIFICANT MODIFICATIONS IN THE MATERIALS TO BE SUBMITTED FOR REVIEW. THESE MODIFICATIONS ARE SHOWN BELOW UNDER "PROCEDURES FOR BULK LAND TRANSFERS ONLY." IF A VARIANCE IN SUBDIVISION IMPROVEMENTS GUARANTEES IS DESIRED, THE REQUEST SHOULD BE SHOWN ON THE SUBDIVISION APPLICATION FORM.

C. FOR ANY VARIANCE REQUEST: THE VARIANCE REQUEST WILL BE ACTED UPON BY THE DRB SIMULTANEOUSLY AS IT REVIEWS THE SUBDIVISION REQUEST. A PRE-APPLICATION DISCUSSION SHOULD TAKE PLACE PRIOR TO ENTERING PHASE I.

THIS IS TO CERTIFY THAT TAXES ARE CURRENT AND
PAID ON UDC # 060622613310164 L1, SEE ATTACH
PROPERTY OWNER OF RECORD
State of New Mexico
PINAL COUNTY TREASURER'S OFFICE

F. J. Jones 11/13/02

Smatson 11/13/06
PLANNING DIRECTOR, CITY OF ALBUQUERQUE DATE

TRAFFIC ENGINEER, TRANSPORTATION DEPARTMENT 11-13-66
DATE

Christina Sandoval 10/25/06
PARKS AND RECREATION DEPARTMENT DATE

Lead 95. Mark 11-09-06
PUBLIC SERVICE COMPANY DATE

Kenneth A. Lile. 4/10/06
 DWEST COMMUNICATIONS, INC. DATE




L. J. MARK
 DATE

KB Jal 10-9-06
CITY SURVEYOR ENGINEERING DIVISION DATE

N/A

Robert Chen 10-25-20

Bradley L. Bingham 10/25/66

Bradley L. Bingham 10/25/06

CERTIFICATE OF SURVEY

I, SANTIAGO ROMERO JR., NEW MEXICO REGISTERED PROFESSIONAL SURVEYOR NO. 7924, DO HEREBY CERTIFY THAT THIS PLAN WAS PREPARED UNDER MY SUPERVISION FROM ACTUAL FIELD SURVEYS PERFORMED FROM JULY 2006 TO AUGUST 2006 AND IS, TO THE BEST OF MY KNOWLEDGE AND BELIEF, CORRECT AND CONFORMS TO THE MINIMUM REQUIREMENTS OF THE STANDARDS FOR LAND SURVEYS IN NEW MEXICO AS ADOPTED BY THE NEW MEXICO STATE BOARD OF REGISTRATION FOR PROFESSIONAL ENGINEERS AND LAND SURVEYORS AND IS IN COMPLIANCE WITH THE ALBUQUERQUE SUBDIVISION ORDINANCE.

SANTIAGO ROMERO JR P.S. NO. 7924 DATE 10/2/2000

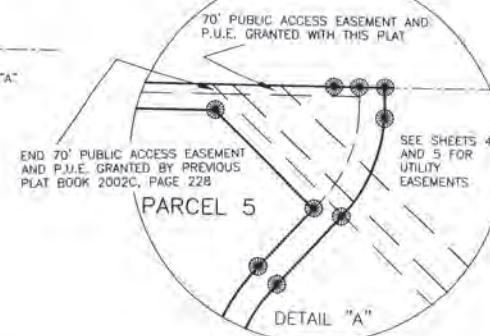
050116A,OWG 10/06

S&ASSOC *Luis Romero Jr.*
and Associates, Inc.
ENGINEERS • SURVEYORS
808 BRANDYME RD., N.E.
ALBUQUERQUE, NEW MEXICO 87111



&
TRACT L-1 PARCELS 1-5
DOUBLE EAGLE II AIRPORT

DOUBLE EAGLE II AIRPORT
WITHIN SECTIONS 13, 14, 23, 24, 25, 26, 27, 34, 35 T11N, R1E,
SECTION 3 T10N, R1E, NMPM
CITY OF ALBUQUERQUE, BERNALILLO COUNTY, NEW MEXICO
EASTERN PORTION OF BLACK RANCH
OCTOBER, 2006



PROPERTY LINE TABLE			
	BEARING		DISTANCE
L1	S	89°41'52" E	800.74
L2	S	46°20'54" E	846.20
L3	S	43°11'00" W	490.67
L4	S	90°00'00" W	745.55
L5	S	89°39'04" E	154.78
L6	S	89°39'04" E	156.01
L7	S	00°15'03" E	127.95
L8	S	43°11'00" W	571.34
L9	N	30°31'05" W	119.19
L10	N	89°22'18" W	580.00
L11	S	00°56'49" W	700.48
L12	N	89°35'06" W	545.37
L13	N	89°57'32" W	498.10
L14	N	89°57'32" E	170.19

PROPERTY CURVE TABLE						
	ARC	DELTA	RADIUS	CHORD	CHORD BEARING	TANGENT
C1	733.05	4°26'03"	967.00	715.63	S 21°27'58" W	385.15
C2	1070.84	56°39'09"	1083.00	1027.75	S 14°51'26" W	583.78
C3	623.91	22°06'27"	1617.00	620.05	S 02°24'55" E	315.89
C4	94.78	05°41'07"	878.00	94.74	S 11°43'52" W	47.44
C5	1182.58	56°41'12"	1239.00	1138.20	N 15°50'25" E	640.68

CL P.A.E. CURVE TABLE					CL P.A.E. LINE TABLE				
CURVE	DELTA	LENGTH	RADIUS	TANGENT	CHORD BEARING	CHORD	LINE	LENGTH	BEARING
C0	07:52:52.8	104.37	81.00	52.36	N30:33:38"E	104.37	LINE	48.19	N67:57:30"E
C1	08:02:29.8	81.00	52.36	52.36	N59:33:38"E	81.00	LINE	17.55	N44:48:50"E
C9	13:22:50.07	891.00	2950.91	347.09	S80:13:38"E	891.00	LINE	345.07	N45:07:50"E
D1	50:22:59.8	101.50	106.90	545.25	N17:54:54"E	798.93	LINE	9.3353	N46:40:50"E
E1	4:13:27.28	72.00	106.90	545.25	N17:54:54"E	101.50	LINE	266.30	N46:40:50"E
F1	4:53:41.4	230.55	281.63	122.17	S69:24:55"E	224.16	LINE	1302.44	S69:44:50"E
G1	74:09:26.8	278.25	215.11	162.41	N83:10:30"E	259.24	LINE		
H1	74:09:26.8	532.41	800.00	690.93	N40:50:40"E	519.67	LINE		
I1	38:10:36.44	113.52	52.36	52.36	N30:33:38"E	113.52	LINE		

TRACT 1*
BULK LAND PLAT OF DOUBLE EAGLE II AIRPORT
AND ADJACENT LANDS, BK-2002C, PG-228,
FILED, 07/10/2002.
ZONING SU-1 OPEN SPACE
(OWNER CITY OF ALBUQUERQUE)

- FOUND PROPERTY CORNER NO. 5
REBAR W/CAP STAMPED URS
PS 7924 OR AS INDICATED ON THE FLAT
- SET NO. 5 REBAR W/CAP STAMPED S. ROMERO PS 7924

IRON PIPE

SEE SHEET 3/5

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Page: 3 of 5
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SOIL AMENDMENT
FACILITY TRACT I
VOL. 90C, FOLIO 267
FILED 10/25/90

TRACT "Q"
BULK LAND PLAT OF DOUBLE EAGLE II AIRPORT
AND ADJACENT LANDS, BK-2002C, PG-228.
FILED, 07/10/2002.
ZONING SU-1 OPEN SPACE
(OWNER CITY OF ALBUQUERQUE)

SEE SHEETS 4 AND 5 FOR UTILITY EASEMENTS

TRACT A-1
3196.8609 AC.
TRACTS "L-1" & "A-1"
ZONING: SU-1 AIRPORT & RELATED FACILITIES

CENTERLINE 156' PUBLIC
ACCESS EASEMENT (P.A.E.)
GRANTED BY PREVIOUS PLAT,
BK. 2002C, PG. 228.
CENTERLINE OF EXISTING ROAD
IS 35' EAST OF CENTERLINE
EASEMENT

TRACT "1"
BULK LAND PLAT OF DOUBLE EAGLE II AIRPORT
AND ADJACENT LANDS, BK-2002C, PG-228,
FILED, 07/10/2002.
ZONING SU-1 OPEN SPACE
(OWNER CITY OF ALBUQUERQUE)
(OWNER SPACE)

TRACT "J"
BULK LAND PLAT OF DOUBLE EAGLE II AIRPORT
LAND ADJACENT LANDS, BK-2002C, PG-22B,
FILED, 07/10/2002.
ZONING SU-1 OPEN SPACE

PLAT
S. ROMERO PS 7
LINE DELETED WITH

CITY OF ALBUQUERQUE
OPEN SPACE

SECTION 4

3 1/4" B.C. STAMPED
DE2-K GREINER INC.
L.S. 3517 40' O/S ON LINE

3 1/4" B.C. STAMPED

60' WIDE PUBLIC ACCESS
EASEMENT GRANTED BY PREVIOUS
PLAT BOOK 2002C, PAGE 228

3 1/4" B.C. STAM
DEZ-L GREINER IN
L.S. 3517

L 2 3/4" B.C. STAMPED
TAG B MI. WHITEMAN ENGR. CO.
(DISTURBED)

2 3/4" B.C. (STAMPED TAG
7 1/2 ML. WHITEMAN ENGR.
CO. AND ADJACENT STONE
MARKED 1/2 M.

TRACT "R"
BULK LAND PLAT OF DOUBLE EAGLE II AIRPORT
AND ADJACENT LANDS, BK-2002C, PG-22B,
FILED, 07/10/2002.
ZONING SU-1 AIRPORT & RELATED FACILITIES
(OWNER CITY OF ALBUQUERQUE)

EXISTING 60'
WIDE RIGHT
WAY PLAT C
PG. 181

STAMPED
CO. (TAG
ENGR. CO.)

TOWN OF ATRISCO GRANT

P.A.E. CURVE TABLE					C. 15% P.A.E. LINE TABLE		
CURVE	DELTA	LENGTH	RADIUS	TANGENT	CHORD BEARING	CHORD	LINE LENGTH BEARING
C0	0°27'29"	104.37	811.35	54.26	N30°37'58"E	104.30	L16 485.19 N07°57'51"E
C1	08°20'23"	104.37	811.35	54.26	N59°12'01"E	104.30	L17 485.19 S07°57'51"E
C2	15°24'30"	691.00	2950.91	34.09	S04°51'39"E	689.42	L18 1003.09 S01°07'51"E
C3	52°31'59"	1013.50	1108.90	54.25	N71°17'54"E	978.59	LL3 932.59 N46°40'20"E
C4	42°52'30"	1013.50	1108.90	54.25	N68°25'24"E	978.59	LL4 932.59 S46°40'20"E
C5	46°36'41"	530.55	281.63	123.17	S69°29'59"E	224.16	LL1 1282.44 S89°44'20"E
C6	74°06'26"	278.23	215.11	162.41	N83°53'55"E	259.24	LL2 1282.44 S89°44'20"E
C15	43°34'42"	532.41	705.00	276.83	S88°57'47"E	519.67	
C16	43°34'42"	1134.88	1005.00	276.83	N88°57'47"E	1100.00	

PROPERTY CURVE TABLE						
	ARC	DELTA	RADIUS	CHORD	CHORD BEARING	TANGENT
C1	733.05	43°26'03"	967.00	715.63	S 21°27'58" W	385.19
C2	1070.84	56°39'09"	1083.00	1027.75	S 14°51'26" W	583.78
C3	823.91	22°06'27"	1617.00	620.05	S 02°24'55" E	315.89
C4	94.78	06°11'07"	878.00	94.74	S 11°43'52" W	47.44
C5	1189.58	54°41'19"	1236.00	1138.20	N 15°50'26" E	840.66

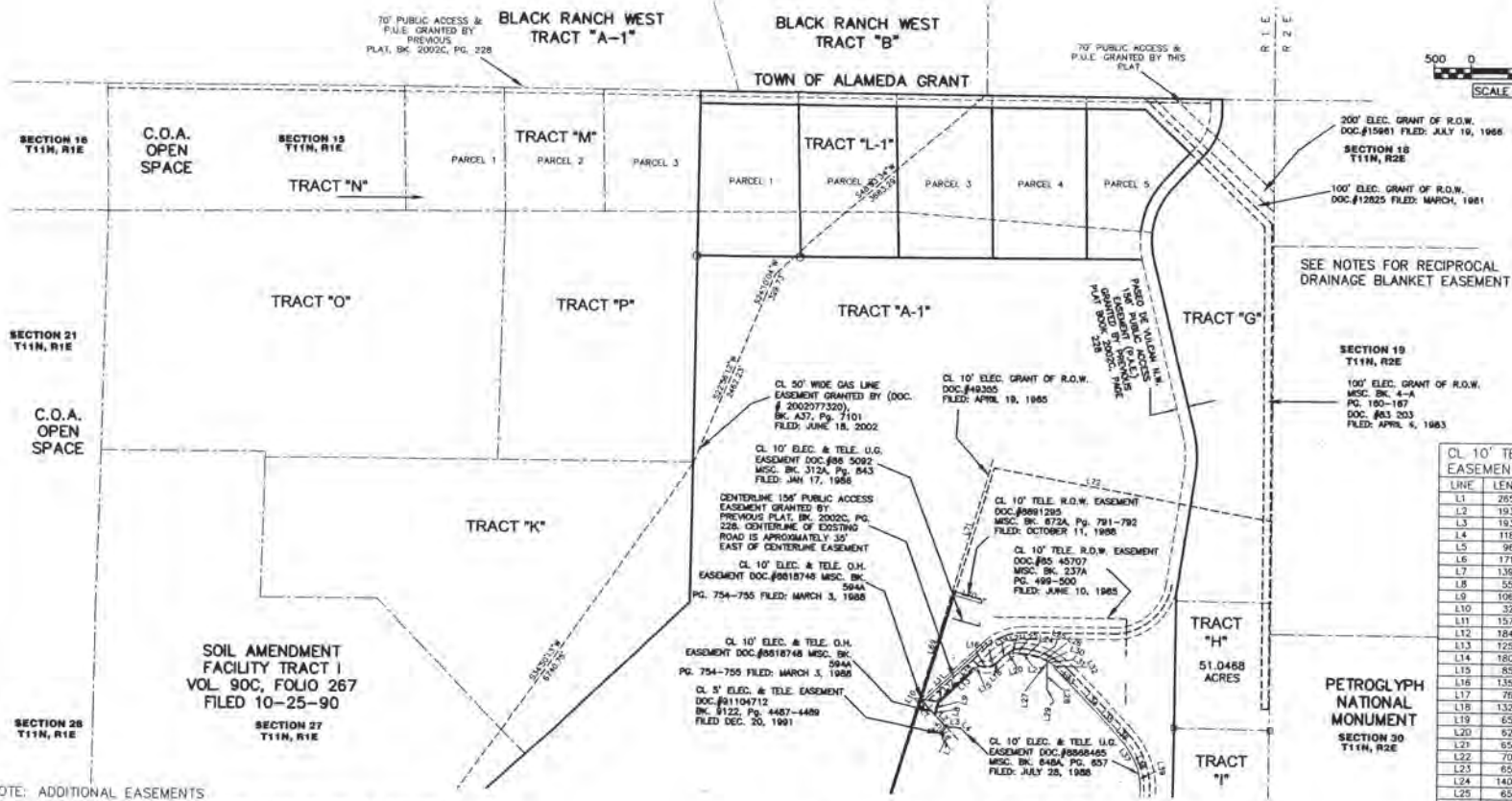
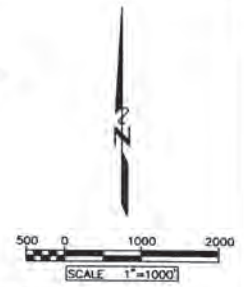
PROPERTY LINE TABLE		
	BEARING	DISTANCE
L1	S 89°41'52" E	800.74
L2	S 45°20'54" E	846.29
L3	S 43°11'00" W	490.6
L4	S 90°00'00" W	745.55
L5	S 89°30'04" E	154.7
L6	S 89°30'04" E	156.6
L7	S 00°15'03" E	127.9
L8	S 43°11'00" W	571.3
L9	S 00°31'05" W	119.1
L10	N 89°22'18" W	580.0
L11	S 00°56'49" W	700.4
L12	N 89°35'06" W	545.5
L13	N 89°57'32" W	498.1
L14	N 00°59'15" E	170.1

LEGEND

- FOUND PROPERTY CORNER NO. 5
REBAR W/CAP STAMPED URS
PS 7924 OR AS INDICATED ON THE PLAT
- Q SET NO. 5 REBAR W/CAP STAMPED S. ROMERO PS 7924
- PUBLIC LAND SURVEY SYSTEM LOT LINE DELETED WITH THIS PLAT.
- ADJOINING PROPERTY LINES
1. IRON PIPE

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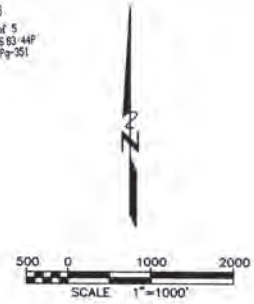
CL 10' TELE. AND ELEC. EASEMENT LINE TABLE		
LINE	LENGTH	BEARING
L1	265.34	S43°32'44"E
L2	193.65	S43°28'45"E
L3	193.88	S87°24'24"E
L4	118.57	S45°07'05"E
L5	98.80	S41°18'36"W
L6	171.09	S19°02'33"E
L7	136.98	S25°15'28"W
L8	55.13	N01°13'27"W
L9	106.45	N16°25'44"E
L10	32.47	N43°43'37"E
L11	157.78	N45°19'49"E
L12	184.32	N46°01'50"E
L13	125.81	N45°26'28"E
L14	160.15	N47°52'28"E
L15	85.67	N63°01'51"E
L16	135.20	S89°03'18"E
L17	76.70	N84°49'15"E
L18	132.05	N48°13'19"E
L19	65.83	N55°34'42"E
L20	92.29	N73°33'58"E
L21	65.19	N83°51'43"E
L22	70.24	N89°46'13"E
L23	65.71	S85°16'15"E
L24	140.46	S79°34'28"E
L25	65.36	S70°26'00"E
L26	104.58	S65°27'09"E
L27	89.22	S01°36'30"W
L28	270.43	S00°45'20"E
L29	28.48	S08°55'53"W
L30	70.03	S48°20'50"E
L31	79.45	S51°15'31"E
L32	55.04	S26°37'09"E
L33	517.70	S43°37'51"E
L34	274.74	S44°24'18"E
L35	324.53	S42°39'08"E
L36	284.88	S41°10'33"E
L37	235.54	S30°58'30"E
L38	128.47	S21°09'46"E
L39	187.10	S11°32'36"E

- NOTE: ADDITIONAL EASEMENTS
- 1) ROADWAY & PIPELINE EASEMENT, DOC. #81-21573, BK. 846, PG. 676-680, SECTIONS 24, 25, 26 AND 35, T11N, R1E, LOTS 1 TO 8 SECTION 2, T10N, R1E.
 - 2) RIGHT-OF-WAY EASEMENT, DOC. #3869 PG. 39-42, VOL. 294, FOLIO 39, AUG. 27, 1957, SECTIONS 25 AND 27, T11N, R1E.
 - 3) RIGHT-OF-WAY EASEMENT, DOC. #8821203 BK. 327A, PG. 909-REFERENCED TO PLAT C35-191 FILED ON FEBRUARY 11, 1988, LOCATED IN TOWN OF ARTRISCO GRANT, PROJECTED SECTIONS 1, 2 AND 3, T10N, R1E.

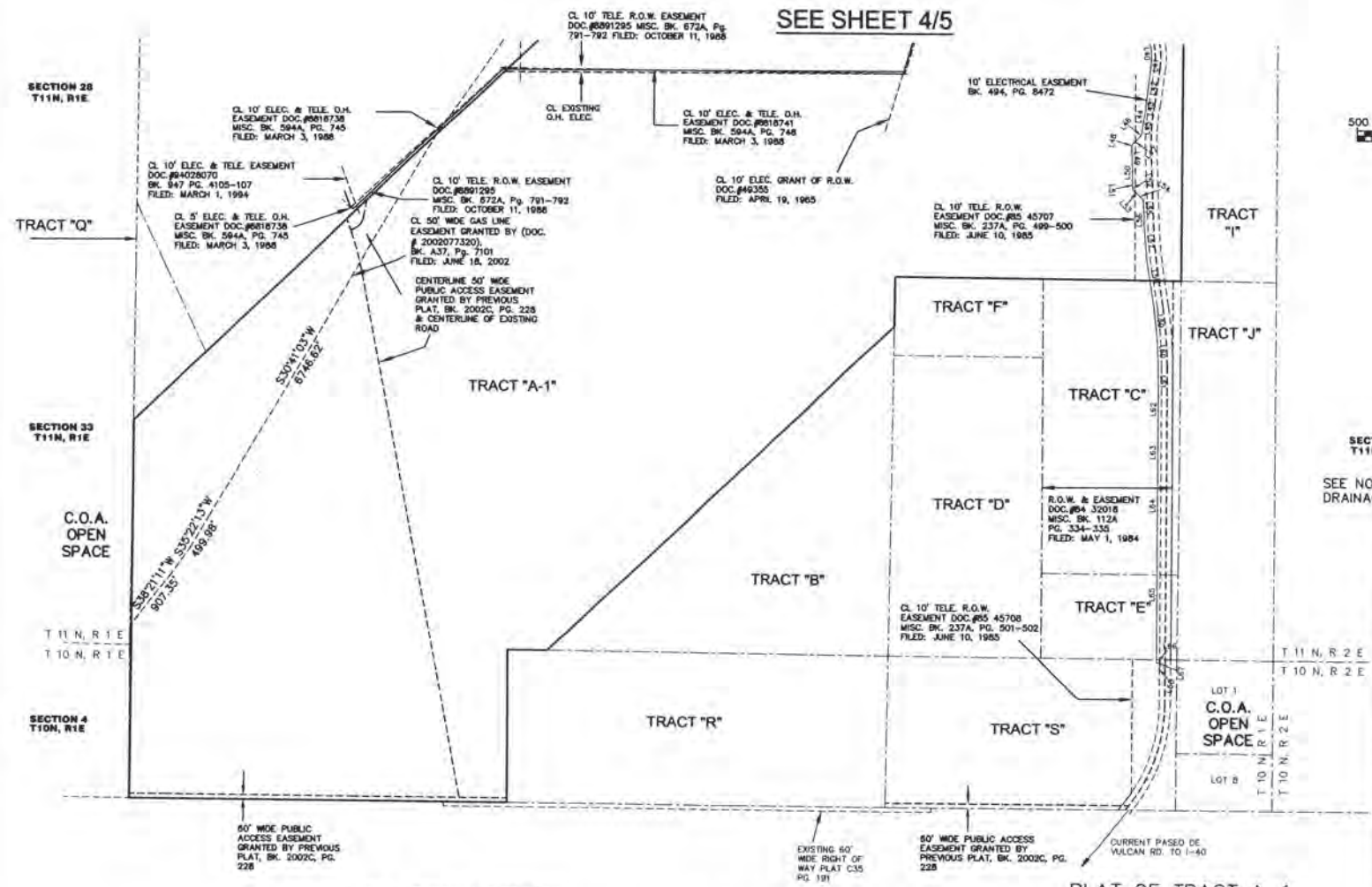
SR Santiago Romero Jr.
& ASSOC.
and Associates, Inc.
ENGINEERS, SURVEYORS
1100 BRANTWINE RD.
ALBUQUERQUE, NEW MEXICO 87111
(505) 864-5119

PLAT OF TRACT A-1
&
TRACT L-1 PARCELS 1-5
DOUBLE EAGLE II AIRPORT
WITHIN SECTIONS 13, 14, 23, 24, 25, 26, 27, 34, 35 T11N, R1E,
SECTION 3 T10N, R1E, NMPM
CITY OF ALBUQUERQUE, BERNALILLO COUNTY, NEW MEXICO
OCTOBER, 2006

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 Page: 5 of 5
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SEE SHEET 4/5



SECTION 31
 T11N, R2E
 SEE NOTES FOR RECIPROCAL
 DRAINAGE BLANKET EASEMENT

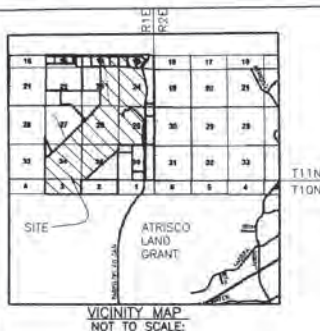
CL. TELE. AND ELEC. EASEMENT LINE TABLE		
LINE	LENGTH	BEARING
L40	124.04	S01°04'47"E
L41	188.22	S05°16'21"W
L42	442.01	S02°02'33"W
L43	128.16	S05°19'21"W
L44	132.01	S10°28'35"W
L45	195.28	S08°40'37"W
L46	64.71	S36°07'03"W
L47	128.01	S44°46'58"W
L48	35.31	S12°02'04"W
L49	326.52	S01°59'57"E
L50	65.43	S02°29'19"W
L51	172.38	S01°21'34"E
L52	29.37	S09°35'12"E
L53	27.47	S41°01'50"E
L54	58.79	S51°59'35"E
L55	252.29	S03°50'38"E
L56	94.76	S01°42'22"W
L57	503.26	S03°44'03"E
L58	650.69	S08°20'34"E
L59	459.80	S05°53'57"E
L60	387.46	S02°21'09"E
L61	457.19	S00°12'17"E
L62	317.32	S00°26'42"W
L63	894.77	S00°23'32"W
L64	586.87	S00°09'25"W
L65	1895.53	S00°08'42"W
L66	51.91	N88°51'25"E
L67	48.20	S03°51'03"W
L68	80.52	S10°10'44"W

- NOTE: ADDITIONAL EASEMENTS
- 1) ROADWAY & PIPELINE EASEMENT, DOC.#81-21573, BK. 846, PG. 676-680, SECTIONS 24,25,26 AND 35, T11N, R1E, LOTS 1 TO 8 SECTION 2, T10N, R1E.
 - 2) RIGHT-OF-WAY EASEMENT, DOC.#3869 PG. 39-42, VOL. 294, FOLIO 39, AUG. 27, 1957, SECTIONS 25 AND 27, T11N, R1E
 - 3) RIGHT-OF-WAY EASEMENT, DOC.#8821203 BK. 327A, PG. 909-REFERENCED TO PLAT C35-191 FILED ON FEBRUARY 11, 1988, LOCATED IN TOWN OF ARTISCO GRANT, PROJECTED SECTIONS 1,2 AND 3 T10N, R1E.

PLAT OF TRACT A-1
 &
 TRACT L-1 PARCELS 1-5
 DOUBLE EAGLE II AIRPORT
 WITHIN SECTIONS 13, 14, 23, 24, 25, 26, 27, 34, 35 T11N, R1E,
 SECTION 3 T10N, R1E, NMPM
 CITY OF ALBUQUERQUE, BERNALILLO COUNTY, NEW MEXICO
 OCTOBER, 2006

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SR
 & ASSOC
 Santiago Romero Jr.
 and Associates, Inc.
 ENGINEERS SURVEYORS
 10000 BRANIFF AVENUE, SUITE 100
 ALBUQUERQUE, NEW MEXICO 87111
 (505) 264-3119



VICINITY MAP
NOT TO SCALE

ZONE ATLAS MAPS
C-4-Z & C-5-Z
D-5-Z & D-6-Z
E-4-Z, E-5-Z & E-6-Z
G-4-Z, G-5-Z & G-6-Z

PLAT OF TRACT A-1
&
TRACT L-1 PARCELS 1-5
DOUBLE EAGLE II AIRPORT
WITHIN SECTIONS 13, 14, 23, 24, 25, 26, 27, 34, 35 T11N, R1E,
SECTION 3 T10N, R1E, NMPM
CITY OF ALBUQUERQUE, BERNALILLO COUNTY, NEW MEXICO
OCTOBER, 2006

DISCLOSURE STATEMENT

PURPOSE OF PLAT

1. TO CREATE 2 TRACTS INCLUDING 5 PARCELS FROM EXISTING 2 TRACTS, ELIMINATE LOT LINES AS SHOWN HEREON AND GRANT BLANKET EASEMENTS.

DATA

1. NEW CASE NO.: BULK LAND VARIANCE NO.
2. GROSS SUBDIVISION ACREAGE 3489.0219 ACRES
3. DATE OF SURVEY: 6/2006
4. TALOS LOG NO: 2006273931

NOTES

1. BASIS OF BEARINGS IS GRID NORTH, NEW MEXICO STATE PLANE COORDINATE SYSTEM, PROJECTIONS COORDINATES BASED ON CITY OF ALBUQUERQUE CONTROL SYSTEM (NAD 27 CENTRAL ZONE, NAD 29).
2. DISTANCES ARE GROUND DISTANCES.
3. BEARINGS AND DISTANCES ARE THE SAME AS RECORD.
4. TRACT "L" AND "A" ARE ZONED SU-1 AIRPORT & RELATED FACILITIES PER EPC APPROVAL 1001856/03EP000915.
5. BASIS OF BOUNDARY IS FROM THE FOLLOWING REFERENCE DOCUMENTS:
A. BULK LAND PLAT OF DOUBLE EAGLE II AND ADJACENT LANDS FILED IN THE OFFICE OF THE BERNALILLO COUNTY CLERK IN BOOK 2002C, PAGE 228.
B. MAP OF T10 N, R1 E N.M.P.M. PREPARED BY THE SURVEYOR GENERAL'S OFFICE, SANTA FE, N.M., NOV. 20, 1875 AND ON FILE WITH THE U.S. BUREAU OF LAND MANAGEMENT.
6. PLAT SHOWS ALL EASEMENTS OF RECORD PROVIDED BY A DOCUMENT SEARCH BY ALBUQUERQUE TITLE CO. INC., FILE NO. 1881791 ON NOVEMBER 1, 2000.
7. BLANKET DRAINAGE EASEMENTS ARE HEREBY GRANTED TO THE CITY OF ALBUQUERQUE ON TRACTS N-1, N-2 & O-1. FUTURE SUBDIVISION ACTIONS MAY FURTHER DEFINE THESE BLANKET DRAINAGE EASEMENTS. THESE BLANKET DRAINAGE EASEMENTS ARE FOR THE BENEFIT OF ALL TRACTS CREATED BY THIS PLAT AND ARE TO BE MAINTAINED BY EACH RESPECTIVE OWNER.
8. JOINT USE OF PUBLIC UTILITY EASEMENTS SHOWN ON THIS PLAT ARE GRANTED FOR THE COMMON AND JOINT USE OF:
9. QWEST FOR INSTALLATION, MAINTENANCE AND SERVICE OF ALL BURIED AND AERIAL COMMUNICATION LINES AND OTHER RELATED EQUIPMENT AND FACILITIES REASONABLY NECESSARY TO PROVIDE COMMUNICATION SERVICES, INCLUDING BUT NOT LIMITED TO ABOVE GROUND PEDESTALS AND CLOSURES.
10. CABLE TV FOR INSTALLATION, MAINTENANCE AND SERVICE OF SUCH LINES, CABLE AND OTHER RELATED EQUIPMENT AND FACILITIES REASONABLY NECESSARY TO PROVIDE CABLE TV SERVICE.
INCLUDED, IS THE RIGHT TO BUILD, REBUILD, CONSTRUCT, RECONSTRUCT, LOCATE, RELOCATE, CHANGE, REMOVE, MODIFY, RENEW, OPERATE AND MAINTAIN FACILITIES FOR THE PURPOSES DESCRIBED ABOVE, TOGETHER WITH FREE ACCESS TO, FROM, AND OVER SAID RIGHT OF WAY AND EASEMENT, WITH THE RIGHT AND PRIVILEGE OF GOING UPON, OVER AND ACROSS ADJOINING LANDS OF GRANTOR FOR THE PURPOSES SET FORTH HEREIN AND WITH THE RIGHT TO UTILIZE THE RIGHT OF WAY AND EASEMENT TO EXTEND SERVICES TO CUSTOMERS OF GRANTEE, AND TO TRIM AND REMOVE TREES, SHRUBS OR BUSHES WHICH INTERFERE WITH THE PURPOSES SET FORTH HEREIN. NO BUILDING, SIGN, POOL (ABOVE GROUND OR SUBSURFACE), HOT TUB, CONCRETE OR WOOD POOL DECKING, OR OTHER STRUCTURE SHALL BE ERECTED OR CONSTRUCTED ON SAID EASEMENTS, NOR SHALL ANY WELL BE DRILLED OR OPERATED THEREON.
11. IN APPROVING THIS PLAT, THE UTILITY COMPANIES DID NOT CONDUCT A TITLE SEARCH OF THE PROPERTIES SHOWN HEREIN. CONSEQUENTLY, THE UTILITY COMPANIES DO NOT MAKE OR RELEASE ANY EASEMENT OR EASEMENT RIGHTS WHICH MAY HAVE BEEN GRANTED BY PRIOR PLAT, REPLAT OR ON OTHER DOCUMENT AND WHICH ARE NOT SHOWN ON THIS PLAT.
12. PNM GAS SERVICES FOR INSTALLATION, MAINTENANCE AND SERVICE OF NATURAL GAS LINES, VALVES AND OTHER EQUIPMENT AND FACILITIES REASONABLY NECESSARY TO PROVIDE NATURAL GAS.
13. PNM ELECTRIC SERVICES FOR THE INSTALLATION, MAINTENANCE AND SERVICE OF OVERHEAD AND UNDERGROUND ELECTRICAL LINES, TRANSFORMERS AND OTHER EQUIPMENT, FIXTURES STRUCTURES AND RELATED FACILITIES REASONABLY NECESSARY TO PROVIDE ELECTRICAL SERVICE.

LEGAL DESCRIPTION

TRACTS "A" AND "L" OF BULK LAND PLAT OF DOUBLE EAGLE II AIRPORT AND ADJACENT LANDS, BK-2002C, PG-228, FILED 07/10/2002.

OWNERS FREE CONSENT AND DEDICATION

THE SUBDIVISION HEREON DESCRIBED IS WITH THE FREE CONSENT AND IN ACCORDANCE WITH THE DESIRES OF THE UNDERSIGNED OWNERS AND/OR PROPRIETORS THEREOF AND SAID OWNERS AND/OR PROPRIETORS DO HEREBY GRANT TO THE CITY OF ALBUQUERQUE, EASEMENTS FOR PUBLIC STREET, RIGHT-OF-WAY, ACCESS, DRAINAGE, AND UTILITIES AS INDICATED HEREIN, INCLUDING THE RIGHT TO CONSTRUCT, OPERATE, INSPECT, AND MAINTAIN FACILITIES THEREIN, FURTHER, SAID OWNERS AND/OR PROPRIETORS DO HEREBY GRANT OTHER PRIVATE EASEMENTS AS SHOWN AND INDICATED.

OWNER FOR TRACT "A", NOW BEING CREATED AND CONFIGURED AS TRACT "A-1",
CITY OF ALBUQUERQUE A MUNICIPAL CORPORATION.

Bruce J. Perlman
BRUCE J. PERLMAN, PhD
CHIEF ADMINISTRATIVE OFFICER
CITY OF ALBUQUERQUE

STATE OF NEW MEXICO (SS)
COUNTY OF BERNALILLO (SS)

THIS INSTRUMENT WAS ACKNOWLEDGED BEFORE ME ON October 9 BY BRUCE J. PERLMAN, PhD, AS CHIEF ADMINISTRATIVE OFFICER OF THE CITY OF ALBUQUERQUE, NEW MEXICO, A MUNICIPAL CORPORATION, ON BEHALF OF SAID CORPORATION.

Felicia Liron
FELICIA LIRON
NOTARY PUBLIC
[MY COMMISSION EXPIRES: 1-22-2010]



OWNER FOR TRACT "L", NOW BEING CREATED AND CONFIGURED AS TRACT "L-1",
NEW MEXICO STATE LAND OFFICE.

Patrick H. Lyons
PATRICK H. LYONS
COMMISSIONER OF PUBLIC LANDS



NOTICE OF SUBDIVISION PLAT CONDITION

- FOR DESIGN VARIANCE: THE PROCEDURE FOR A SUBDIVISION FOR WHICH A DESIGN VARIANCE IS REQUESTED IS THE SAME AS FOR STANDARD SUBDIVISION (SEE PAGE 2-6). HOWEVER, A DESIGN VARIANCE REQUEST SHOULD BE SHOWN ON THE SUBDIVISION APPLICATION FORM.
- FOR A VARIANCE TO SUBDIVISION IMPROVEMENTS GUARANTEE: THE ONLY BASIS UPON WHICH THE DIB MAY WAIVE THE REQUIRED SUBDIVISION IMPROVEMENTS GUARANTEE IS FOR SUBDIVISIONS INTENDED TO FACILITATE BULK LAND TRANSFERS. THE REVIEW AND APPROVAL PROCEDURE FOR SUCH SUBDIVISIONS INVOLVES THE SAME STEPS AS FOR STANDARD SUBDIVISIONS. HOWEVER, THERE ARE SEVERAL SIGNIFICANT MODIFICATIONS IN THE MATERIALS TO BE SUBMITTED FOR REVIEW. THESE MODIFICATIONS ARE SHOWN BELOW UNDER "PROCEDURES FOR BULK LAND TRANSFERS ONLY". IF A VARIANCE IN SUBDIVISION IMPROVEMENTS GUARANTEES IS DESIRED, THE REQUEST SHOULD BE SHOWN ON THE SUBDIVISION APPLICATION FORM.
- FOR ANY VARIANCE REQUEST: THE VARIANCE REQUEST WILL BE ACTED UPON BY THE DIB SIMULTANEOUSLY AS IT REVIEWS THE SUBDIVISION REQUEST. A PRE-APPLICATION DISCUSSION SHOULD TAKE PLACE PRIOR TO ENTERING PHASE I.



CERTIFICATE OF SURVEY

I, SANTIAGO ROMERO JR., NEW MEXICO REGISTERED PROFESSIONAL SURVEYOR NO. 7924, DO HEREBY CERTIFY THAT THIS PLAT WAS PREPARED UNDER MY SUPERVISION FROM ACTUAL FIELD SURVEYS PERFORMED FROM JULY 2006 TO AUGUST 2006, AND IS, TO THE BEST OF MY KNOWLEDGE AND BELIEF, CORRECT AND CONFORMS TO THE MINIMUM REQUIREMENTS OF THE STANDARDS FOR LAND SURVEYS IN NEW MEXICO AS ADOPTED BY THE NEW MEXICO STATE BOARD OF REGISTRATION FOR PROFESSIONAL ENGINEERS AND LAND SURVEYORS AND IS IN COMPLIANCE WITH THE ALBUQUERQUE SUBDIVISION ORDINANCE.

Santiago Romero Jr.
SANTIAGO ROMERO JR., P.E., NO. 7924

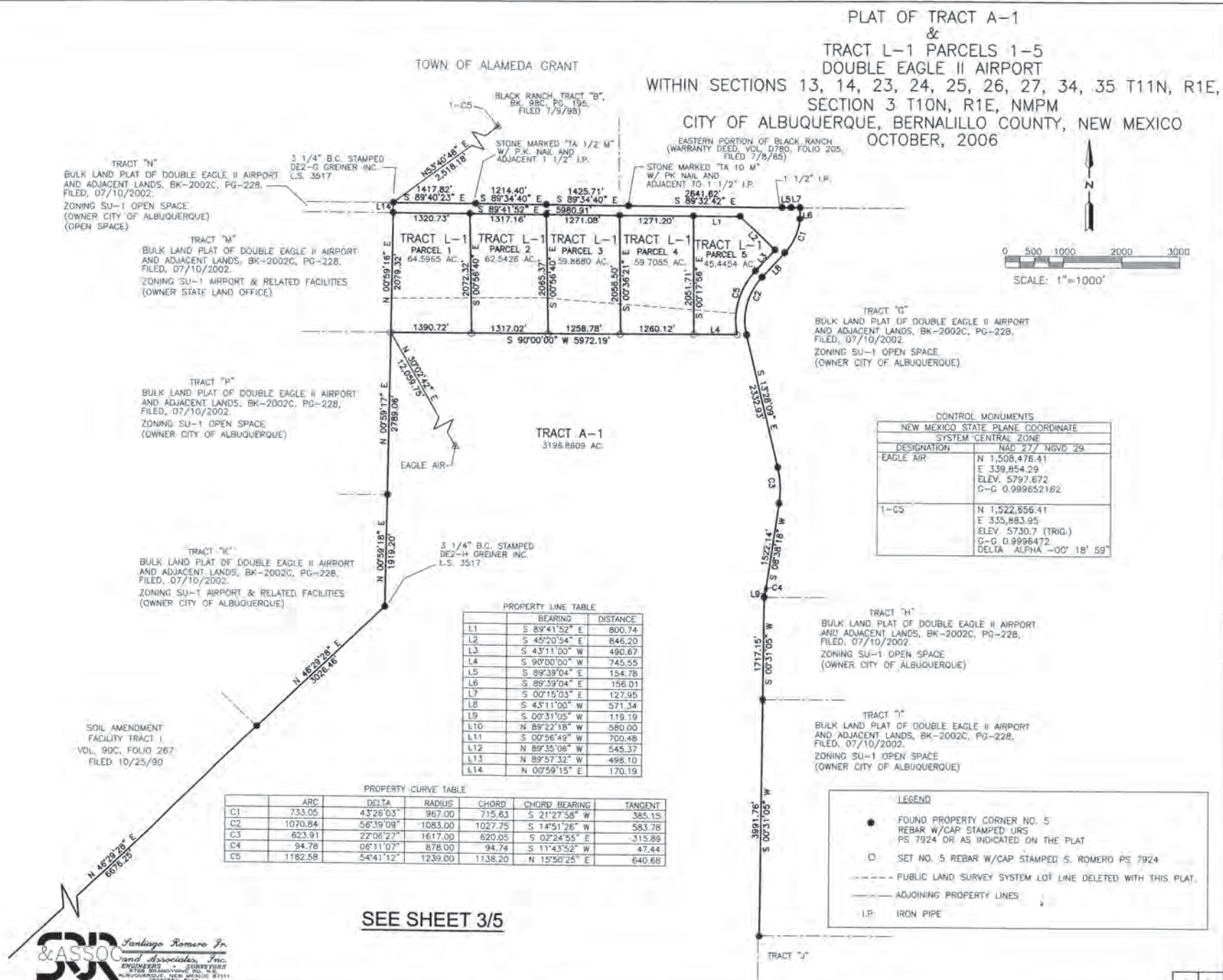
10/9/2006
DATE

APPROVED AND ACCEPTED BY

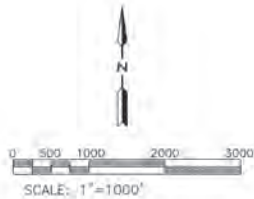
PLANNING DIRECTOR, CITY OF ALBUQUERQUE	DATE
TRAFFIC ENGINEER, TRANSPORTATION DEPARTMENT	DATE
PARKS AND RECREATION DEPARTMENT	DATE
PUBLIC SERVICE COMPANY	DATE
QWEST COMMUNICATIONS, INC.	DATE
GAS COMPANY	DATE
<i>KB</i> CITY SURVEYOR, ENGINEERING DIVISION	10-9-06 DATE
CITY REAL PROPERTY	DATE
UTILITY DEVELOPMENT DEPARTMENT	DATE
ALBUQUERQUE METROPOLITAN ARROYO FLOOD CONTROL AUTHORITY	DATE
CITY ENGINEER, ENGINEERING DIVISION	DATE

050116A.DWG: 10/06

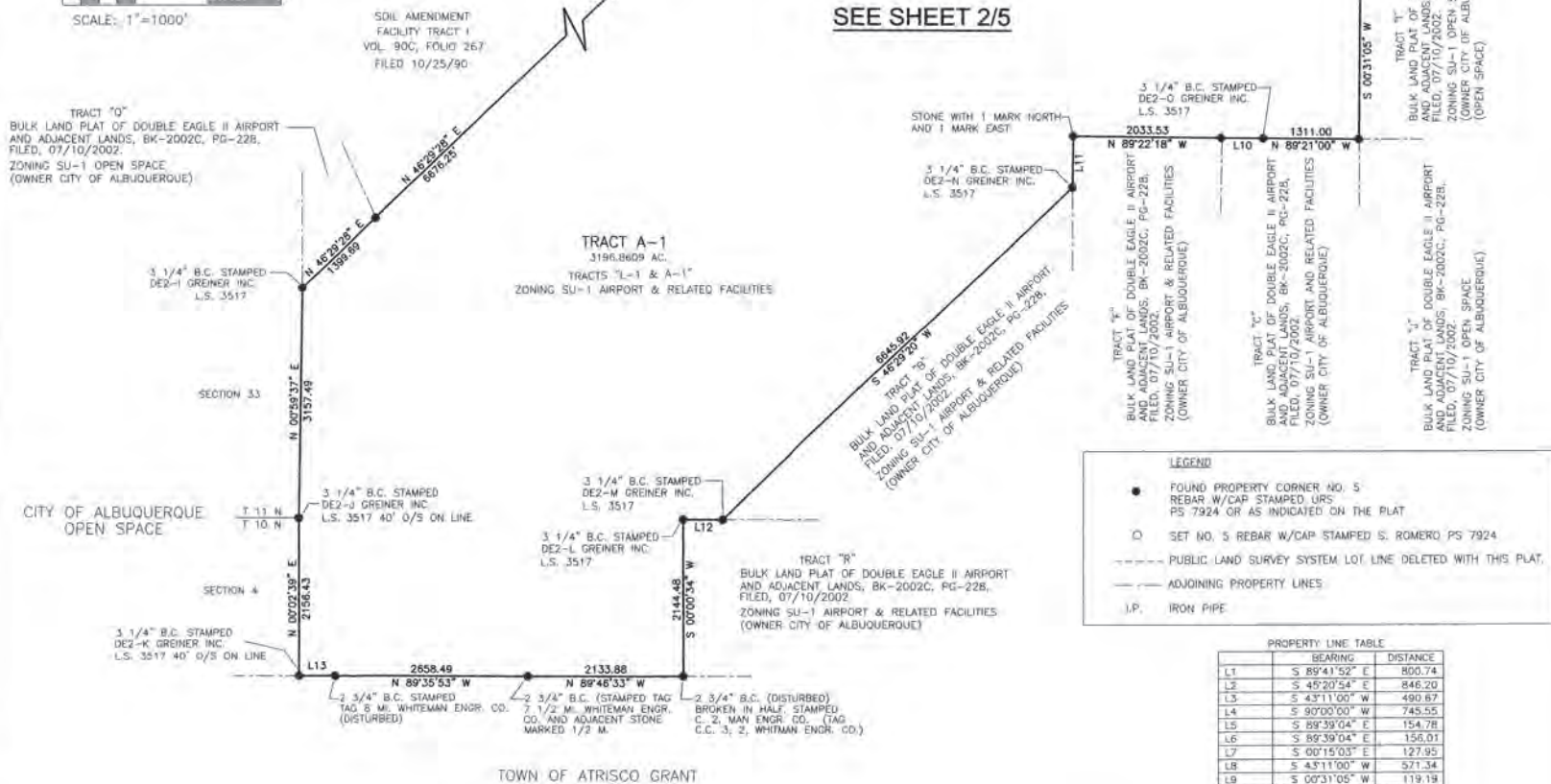
SRR Santiago Romero Jr.
& ASSOC. and Associates, Inc.
ENGINEERS, SURVEYORS
FOR DESIGN
ALBUQUERQUE, NEW MEXICO 87111
(505) 264-5118



PLAT OF TRACT A-1
&
TRACT L-1 PARCELS 1-5
DOUBLE EAGLE II AIRPORT
WITHIN SECTIONS 13, 14, 23, 24, 25, 26, 27, 34, 35 T11N, R1E,
SECTION 3 T10N, R1E, NMPM
CITY OF ALBUQUERQUE, BERNALILLO COUNTY, NEW MEXICO
OCTOBER, 2006



SEE SHEET 2/5



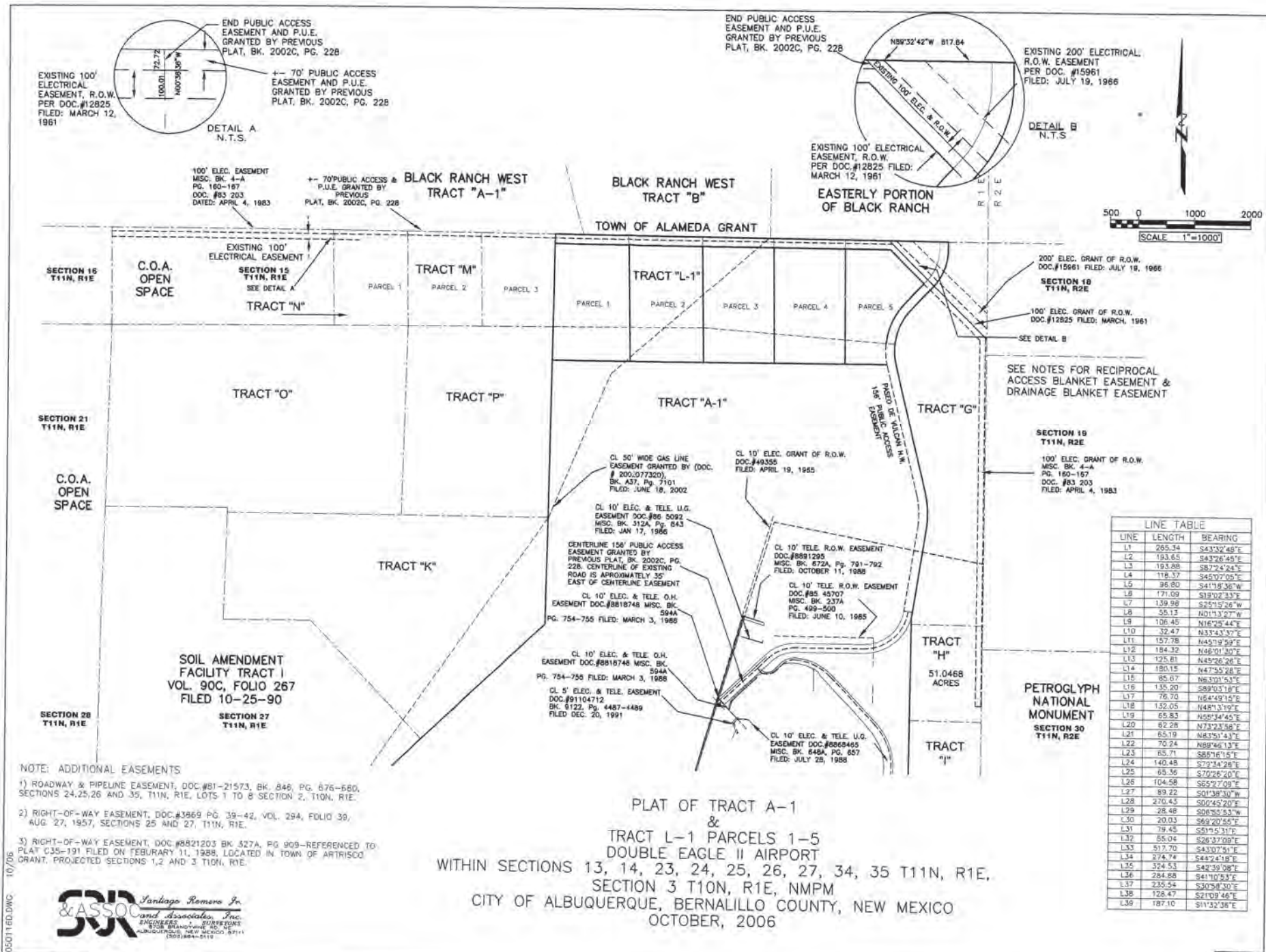
LEGEND

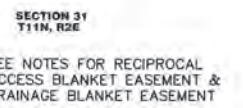
- FOUND PROPERTY CORNER NO. 5 REBAR W/CAP STAMPED URS PS 7924 OR AS INDICATED ON THE PLAT
- SET NO. 5 REBAR W/CAP STAMPED S. ROMERO PS 7924
- PUBLIC LAND SURVEY SYSTEM LOT LINE DELETED WITH THIS PLAT.
- ADJOINING PROPERTY LINES
- I.P. IRON PIPE

PROPERTY LINE TABLE		
	BEARING	DISTANCE
L1	S 89°41'52" E	800.74
L2	S 45°20'54" E	846.20
L3	S 43°11'00" W	490.67
L4	S 90°00'00" W	745.55
L5	S 89°39'04" E	154.78
L6	S 89°39'04" E	156.01
L7	S 00°15'03" E	127.95
L8	S 43°11'00" W	571.34
L9	S 00°31'05" W	119.19
L10	N 89°22'18" W	580.00
L11	S 00°56'49" W	700.48
L12	N 89°35'06" W	545.37
L13	N 89°57'32" W	498.10
L14	N 00°59'15" E	170.19

PROPERTY CURVE TABLE					
	ARC	DELTA	RADIUS	CHORD	CHORD BEARING
C1	733.05	43°26'03"	967.00	715.63	S 21°27'58" W
C2	1070.84	56°39'09"	1083.00	1027.75	S 14°51'26" W
C3	623.91	22°06'27"	1617.00	620.05	S 02°24'56" E
C4	94.78	06°11'07"	878.00	94.74	S 11°43'52" W
C5	1182.58	54°41'12"	1239.00	1138.20	N 15°50'25" E

S&A Santiago Romero Jr. and Associates, Inc.
ENGINEERS & SURVEYORS
8708 BRANCHVIEW RD. N.E.
ALBUQUERQUE, NEW MEXICO 87111
(505) 884-9119





5	5
---	---

NOTE: ADDITIONAL EASEMENTS:

1) ROADWAY & PIPELINE EASEMENT, DOC.#B1-21573, BK. 846, PG. 676-680, SECTIONS 24,25,26 AND 35, T1N, R1E, LOTS 1 TO 8 SECTION 2, T1N, R1E.

2) RIGHT-OF-WAY EASEMENT, DOC.#3869 PG. 39-42, VOL. 294, FOLIO 39, AUG. 27, 1997, SECTIONS 25 AND 27, T1N, R1E.

3) RIGHT-OF-WAY EASEMENT, DOC.#882103 BK 327A, PG 809-REFERENCED TO PLAT C35-191 FILED ON FEBRUARY 11, 1988, LOCATED IN TOWN OF ARRISCO GRANT, PROJECTED SECTIONS 1,2 AND 3 T1N, R1E.

PLAT OF TRACT A-1
&
TRACT L-1 PARCELS 1-5
DOUBLE EAGLE II AIRPORT
WITHIN SECTIONS 13, 14, 23, 24, 25, 26, 27, 34, 35 T11N, R1E,
SECTION 3 T10N, R1E, NMPM
CITY OF ALBUQUERQUE, BERNALILLO COUNTY, NEW MEXICO
OCTOBER, 2006

TRI-MOTOR, LLC
2850 A. RUFINA ST.
SANTA FE, NEW MEXICO 87507

APRIL, 2008

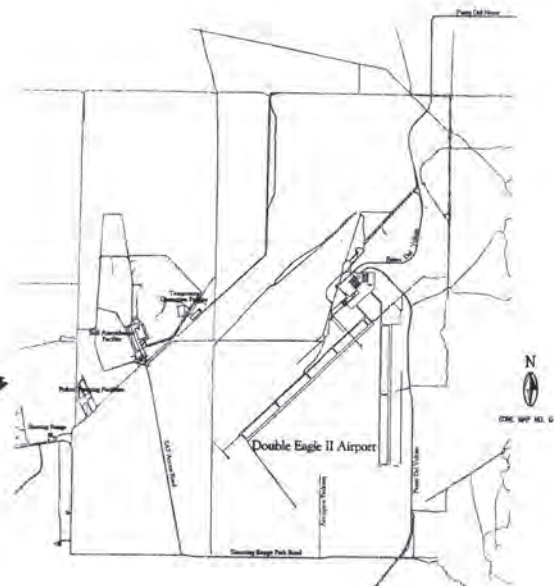
for the
TRI-MOTOR AIRCRAFT HANGAR
7401 Paseo Del Volcan N.W.
Albuquerque, New Mexico 87121

CIVIL ENGINEERS

Molzen-Corbin & Associates
John A. Teller P.E.
John M. Provine P.E.

INDEX OF DRAWINGS

SHEET	DESCRIPTION
C-001	TITLE SHEET AND INDEX TO DRAWINGS
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L1	LANDSCAPE PLAN
C-102	GRADING & DRAINAGE PLAN
C-103	GRADING & DRAINAGE PLAN PROPOSED DRAINAGE BASINS
C-103A	GRADING & DRAINAGE PLAN DOUBLE EAGLE II DWP ADDITIONAL INFORMATION
A5	ELEVATIONS
C-104	UTILITY PLAN
C-501	SITE DETAILS



DESCRIPTION	REV. DATE	REV. NO.	NUMERIC SCALE CONFIRMATION
			DRAWINGS ARE DEPICTED AT INTENDED NUMERIC SCALES. IF THIS DRAWING ONE INCH

PROJECT NUMBER: 1001656

Application Number: 08-70240

Is an Infrastructure List required? (If Yes () No (X) Yes, then a set of approved CDEC plans with a work order is required for any construction within Public Right-of-Way or for construction of public improvements.

CDEC SITE DEVELOPMENT PLAN APPROVAL

Traffic Engineering Department Date: 4/19/08

ASCHMAD Date: 4-4-08

Police and Fire Department Date: 4/14/08

City Engineer Date: 4/14/08

* Environmental Health Department (conditional) Date: 4/14/08

Solid Waste Management Date: 4/14/08

City Planning Department Date: 4/14/08

Environmental Health, if necessary

THESE DRAWINGS DO NOT INCLUDE NECESSARY COMPONENTS FOR THE CONSTRUCTION SAFETY WHICH SHALL REMAIN THE RESPONSIBILITY OF THE CONTRACTOR.

ALL IDEAS, DESIGNS, ARRANGEMENTS, AND PLANS INDICATED OR REPRESENTED BY THIS INSTRUMENT ARE OWNED BY AND THE PROPERTY OF MOLZEN-CORBIN & ASSOCIATES, AND WERE CREATED AND DEVELOPED FOR USE ON AND IN CONNECTION WITH THE SPECIFIED PROJECT. THESE IDEAS, DESIGNS, ARRANGEMENTS OR PLANS SHALL NOT BE USED BY ANY PERSON, FIRM, OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT THE WRITTEN CONSENT OF MOLZEN-CORBIN & ASSOCIATES.

CERTIFICATION:

I, JOHN A. TELLER, REGISTERED PROFESSIONAL ENGINEER NO. 16671 HEREBY CERTIFY THAT THE FOLLOWING PLANS AND DESIGNS WERE MADE UNDER MY SUPERVISION AND DIRECTION AND THAT SAME IS TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF



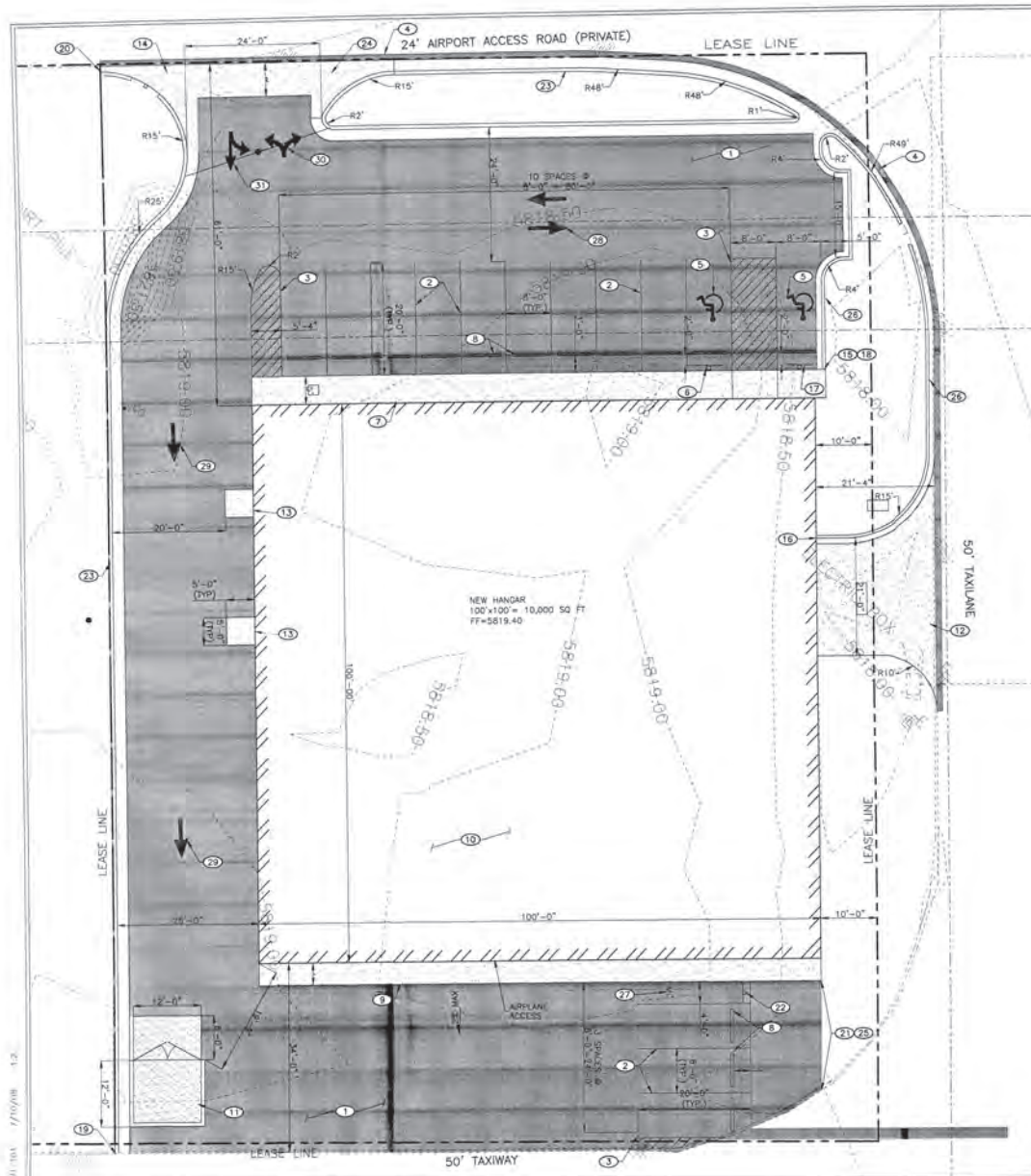
N.M.P.E.

NO. 16671

2701 MILES RD SE
ALBUQUERQUE, NEW MEXICO 87106
TEL: 808 242 5700
FAX: 808 242 0673

MOLZEN-CORBIN & Associates
ENGINEERS/ARCHITECTS/PLANNERS

G-001



PROJECT LOCATION



ZONE ATLAS MAP E-6
SCALE: N.T.S.

GENERAL SHEET NOTES

- ALL RADII ARE TAKEN TO FACE OF CURB UNLESS OTHERWISE NOTED
- VEHICLE TRAFFIC IS PERMITTED ON TAXIWAY AND TAXILANE.

LEGEND

5819.99-	EXISTING MAJOR CONTOUR
5818.50-	EXISTING MINOR CONTOUR
	NEW ASPHALT
	NEW CONCRETE

CAUTION:
NOTE THAT ALL EXISTING UTILITIES MAY NOT BE SHOWN. EXISTING SURFACE CONNECTIONS ARE NOT SHOWN. ANY EXISTING UTILITIES THAT ARE SHOWN ARE SHOWN AS APPROXIMATE LOCATION ONLY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT ALL THE UTILITY OWNERS AND TO CONDUCT ALL NECESSARY FIELD INVESTIGATIONS PRIOR TO ANY DEDICATION TO DETERMINE THE ACTUAL LOCATION OF UTILITIES AND OTHER INTERFERENCES.

KEYED NOTES

- INSTALL 1" ASPHALT SEE DETAIL SHEET C-501
- INSTALL 4" WHITE PARKING STRIPE
- INSTALL 4" YELLOW PARKING STRIPE
- SAWCUT, REMOVE, DISPOSE & REPLACE 243.8 LF 12" ASPHALT STRIP MATCH EXISTING DEPTH SEE DETAIL SHEET C-104
- INSTALL ADA BLUE HANDICAP PARKING SYMBOL
- INSTALL ADA PARKING SIGN
- INSTALL 5'-0" C/P CONCRETE SIDEWALK SEE DETAIL SHEET C-501
- INSTALL 6"x6"x6" CONCRETE WHEEL STOP
- INSTALL 4'-0" C/P CONCRETE SIDEWALK SEE DETAIL SHEET C-501
- NEW AIRCRAFT HANGER - BY OTHERS
- INSTALL NEW TRASH ENCLOSURE SEE DETAIL SHEET C-501
- INSTALL 8" CONCRETE APRON SEE DETAIL SHEET C-501
- INSTALL 5'-0"x5'-0" CONCRETE STOOP REFER TO ARCHITECTURAL SHEETS FOR EXACT LOCATION
- INSTALL PRIVATE ENTRANCE PER COA STD. DWG 2426
- BEGIN MEDIAN CURB & GUTTER INSTALLATION PER COA STD. DWG 2415B
- END MEDIAN CURB & GUTTER INSTALLATION
- INSTALL ADA "VAN ACCESSIBLE" PARKING SIGN
- TAPER CURB & GUTTER AT 2:1 SEE DETAIL SHEET C-501
- BEGIN STANDARD CURB & GUTTER INSTALLATION PER COA STD. DWG 2415A
- END STANDARD CURB & GUTTER INSTALLATION
- EDGE OF NEW PAVEMENT
- INSTALL "MOTORCYCLE PARKING ONLY" SIGN
- INSTALL STANDARD CURB & GUTTER PER COA STD. DWG 2415A
- INSTALL 6'-0" CONCRETE VALLEY GUTTER PER COA STD. DWG 2420
- END NEW ASPHALT PAVING MATCH EXISTING GRADE
- INSTALL MEDIAN CURB & GUTTER PER COA STD. DWG 2415B
- INSTALL PAINTED WHITE MOTORCYCLE PARKING DESIGNATOR "MC" ASPHALT MARKING
- WHITE THERMO-PLASTIC TWO WAY TRAFFIC ARROW
- WHITE THERMO-PLASTIC ONE WAY TRAFFIC ARROW
- WHITE THERMO-PLASTIC LEFT/RIGHT TURN TRAFFIC ARROW
- WHITE THERMO-PLASTIC LEFT TURN & THRU TRAFFIC ARROW

TRAFFIC CIRCULATION LAYOUT (SITE PLAN)

TRI-MOTOR AIRCRAFT HANGAR
TRI-MOTOR, DE II AIRPORT (CITY OF ALBUQUERQUE)
ALBUQUERQUE (NEW MEXICO)

C-101

DRB PROJECT NUMBER: 1001656

SHEET

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Molzen & Corbin, Inc.
2701 Main Street, S.E.
Albuquerque, New Mexico 87102
Tel: (505) 242-0700
Fax: (505) 242-0673



REV.	NO.	DATE	DESCRIPTION
1	1	11/20/08	PROJECT NUMBER: 1001656 - 11/20/08
2	2	11/20/08	DESIGNED BY: J. VELAZQUEZ
3	3	11/20/08	DRAWN BY: J. VELAZQUEZ
4	4	11/20/08	CHECKED BY: J. VELAZQUEZ
5	5	11/20/08	PROJECT DATE: APRIL 2008

1. EXISTING PLANT MATERIAL: THE EXISTING PROPERTY CONSISTS OF NATIVE GRASSES SPATIALLY ESTABLISHED W/ NO TREES. INSTALLATION AND MAINTENANCE OF THE PLANTING MATERIAL SHALL BE THE RESPONSIBILITY OF THE OWNER.
2. THE LANDSCAPE DESIGN SHALL COMPLY WITH THE CITY OF ALBUQUERQUE'S WATER CONSERVATION, LANDSCAPE AND WATER WASTE DRAINAGE.
3. THE USE OF HIGH WATER USE VEGETATION SHALL BE KEPT TO A MINIMUM. THERE IS NO HIGH WATER USE TURF USED IN THIS PROJECT.
4. SEE PLANT LIST FOR MINIMUM PLANT SIZE.
5. PONDING AREAS, SEE GRADING AND DRAINAGE PLAN.
6. SOIL AMENDMENTS SHALL BE BY A RECOGNIZED MANUFACTURER OF THE AMENDMENT PRODUCTS BEING USED.
7. SOIL AMENDMENT AREAS:
7.1. 1 GAL. PLANTS = 15 SF

1. THE LANDSCAPE CONTRACTOR SHALL PROVIDE A DETAILED IRRIGATION PLAN FOR REVIEW AND APPROVAL OF THE ARCHITECT TO PERMANENTLY IRRIGATE THE LANDSCAPING AS THE PLANTING IS DONE. THE IRRIGATION PLAN SHALL SHOW THE LOCATION AND THE PROPOSED PIPING LAYOUT W/ ALL PIPE SIZES CLEARLY INDICATED, ALL EMITTERS PROPERLY LAYED OUT, ALL VALVES AND CONSTRUCTION DETAILS SHOWN. IRRIGATION SHALL BE PROVIDED TO ALL PLANTS AND TREES. THE LANDSCAPE CONTRACTOR SHALL BE THE TENANT SHALL BE RESPONSIBLE FOR HAND WATERING THE PLANTING AS PART OF THE TRADE AGREEMENT.
2. IRRIGATION CONTROLLER: THE ENTIRE IRRIGATION SYSTEM SHALL BE CONTROLLED BY A PLANT AUTOMATIC IRRIGATION CONTROLLER. IRRIGATION TIMES SHALL BE ADJUSTED ACCORDING TO SEASON, PLANT MATURITY, LOCATION AND PLANT PERFORMANCE - DR - HAND WATERING SHALL TAKE INTO ACCOUNT THE TIME OF YEAR.
3. DRIP EMITTERS IF SELECTED, SHALL BE USED FOR ALL PLANTING MATERIAL. SEE PLANTING SPECIFICATIONS FOR EMITTER SIZE AND QUANTITY. EMITTERS SHALL BE PLACED UP-HILL FROM PLANT CENTERS.
4. POINT OF CONNECTION IRRIGATION WATER SHALL BE A 1/4" TAP OFF THE MAIN LINE TO THE BUILDING.
5. PIPING SHALL BE IRRIGATION GRADE PIPE APPROVED FOR THE APPLICATION.

-----PLANTING LEGEND-----

SYMBOL	QUANTITY	SHRUB	INSTALLED SIZE	COMMON NAME	BTOTAL HEIGHT	BTOTAL WIDTH	WATER REQUIREMENTS	EDITHS REQUIREMENTS	
1	3	SHRUB	1 GAL.	SAGE SAGE	3 FT.	3 FT.	RECEIVE WATER	3-5WPM	
2	2	SHRUB	1 GAL.	THORNLESS ROSE	3 LEAF SHAG	6 FT.	6 FT.	RECEIVE WATER	3-5WPM
3	2	SHRUB	1 GAL.	ORIENTAL ROSE	6 FT.	6 FT.	RECEIVE WATER	3-5WPM	
4	2	SHRUB	1 GAL.	ARABIS PLUM	PARADISE	4 FT.	4 FT.	RECEIVE WATER	3-5WPM
5	2	SHRUB	1 GAL.	MOONBERRY	PARADISE	3 FT.	3 FT.	RECEIVE WATER	3-5WPM
6	2	SHRUB	1 GAL.	REDBUD	PARADISE	10 FT.	4 FT.	RECEIVE WATER	3-5WPM
7	3	SHRUB	1 GAL.	WINTERFAT	DEUTEREA	3 FT.	1 FT.	RECEIVE WATER	3-5WPM
8	AS SHOWN	SHRUB 32" x 14" x 14"	NA	NA	NA	NA	NA	NA	NA

The landscape plan shows a rectangular area labeled "NEW HANGAR 100'x100'= 10,000 SQ FT". To the left of the hangar is a "50' TAXIWAY". Above the hangar is a curved boundary labeled "AIRPORT ACCESS ROAD (PRIVATE)". To the right of the hangar is a "50' TAXI LANE". A "BIKE RACK" is located at the intersection of the taxi lane and the road, with a note "SEE DETAIL 2/L1". The area around the hangar is labeled "NEW ASPHALT (SEE CIVIL DRAWINGS)".

**TYPICAL BIKE RACK
DETAIL - 2 BIKES**

96W X 80" LONG 4" CONC. SLAB W/ #4 REBAR AT 24" O.C. COMPACT SOIL UNDER CONC. PAD TO 90% MOD. PROCTOR DENSITY

NO SCALE

2
L1

1"=20'

NORTH

SHEET #
L1

JOB # 08-05

CHECKED BY

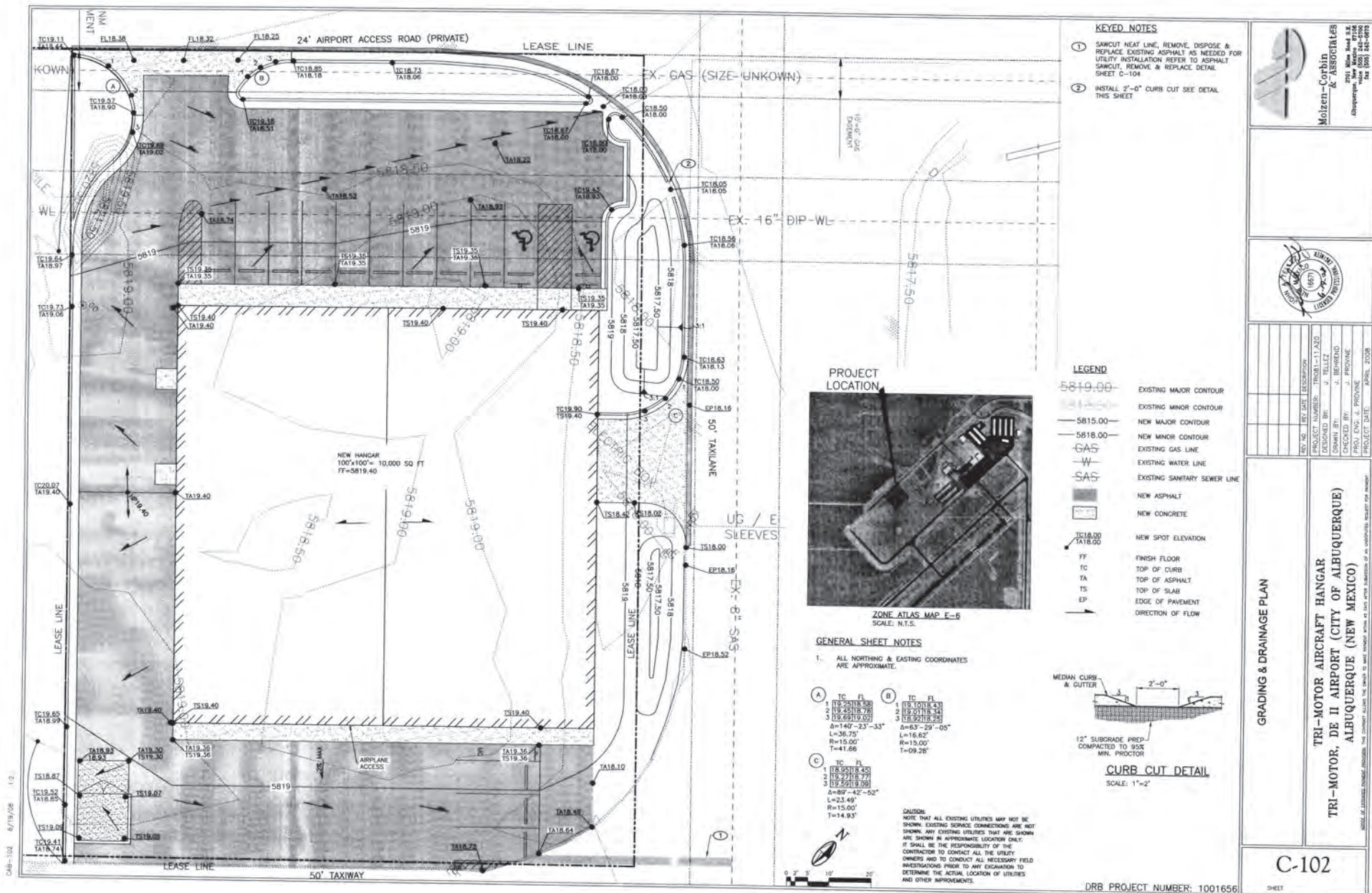
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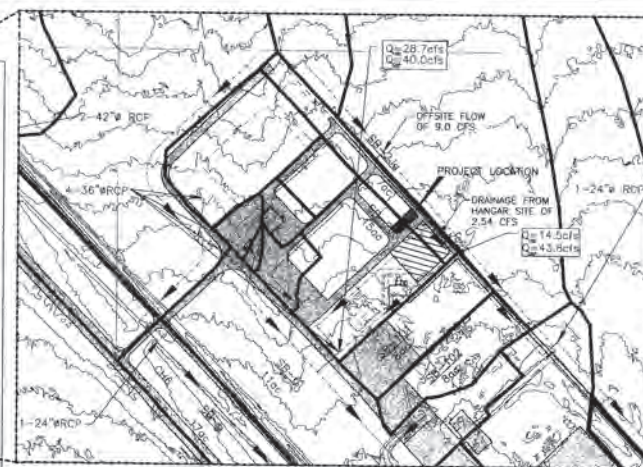
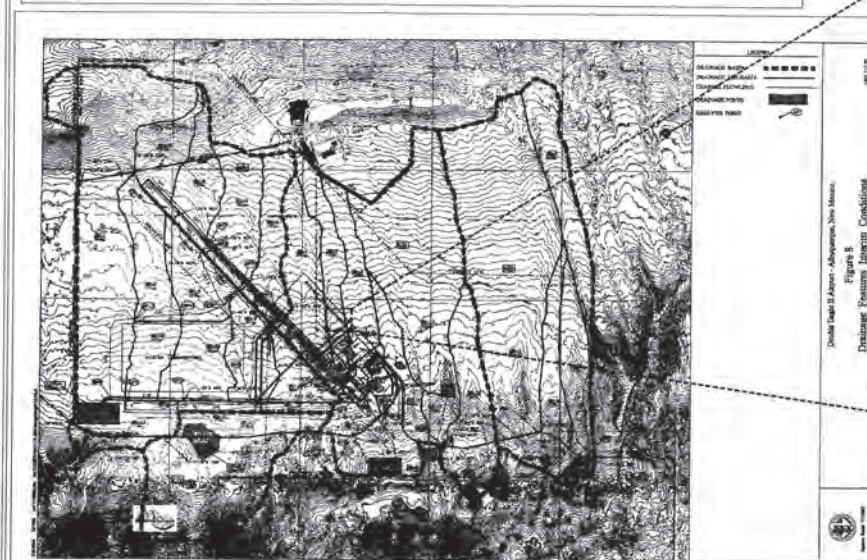
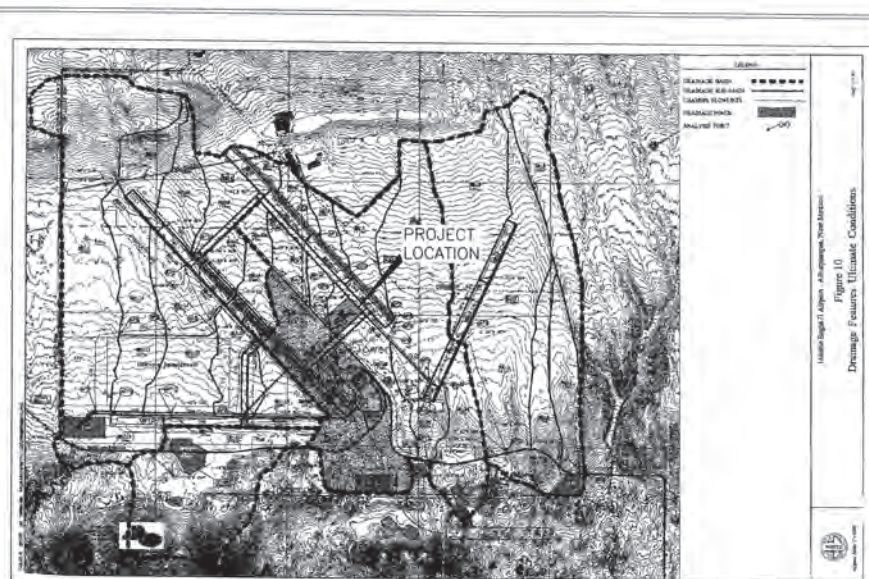
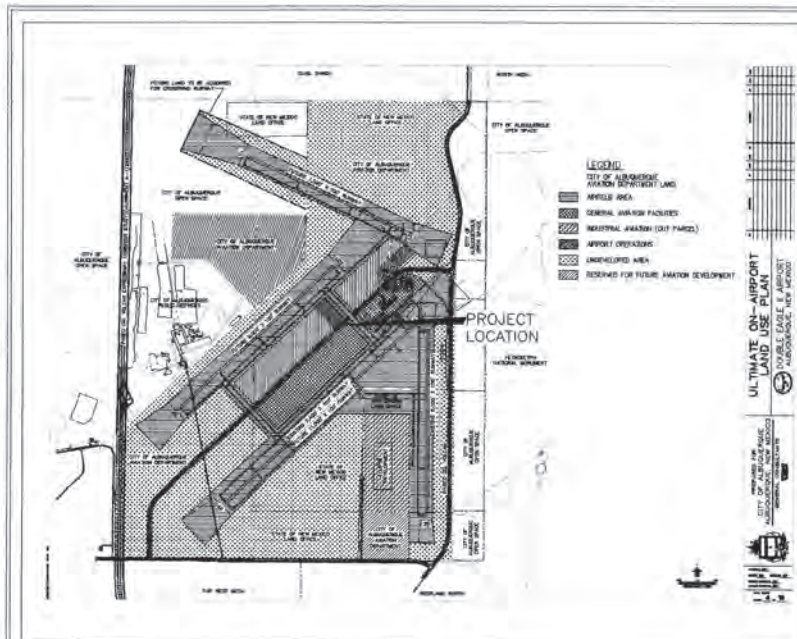
DATE
06.10.08

A NEW HANGAR
TO BE BUILT IN THE INFIELD @ DOUBLE EAGLE II AIRPORT FOR
TRI/MOTOR LLC.
NEW MEXICO 2008 SANTA FE

REVISIONS

LENGYEL & ASSOCIATES 3241 SIRINGO RD. SANTA FE, NEW MEXICO





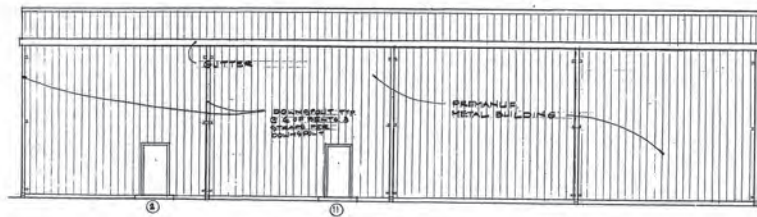
KEYED NOTES

THIS DRAWING PROVIDED FOR "INFORMATION ONLY".

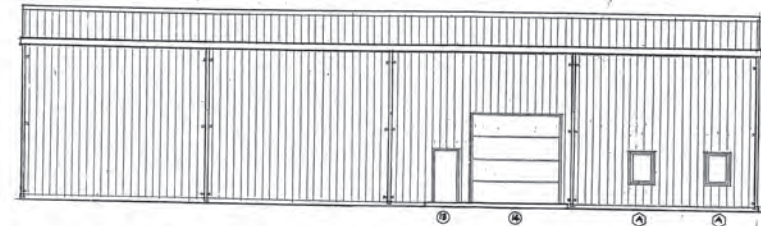
GRADING AND DRAINAGE PLAN
DOUBLE EAGLE II DMP ADDITIONAL INFORMATION

TRI-MOTOR AIRCRAFT HANGAR
TRI-MOTOR, DE II AIRPORT (CITY OF ALBUQUERQUE)
ALBUQUERQUE (NEW MEXICO)

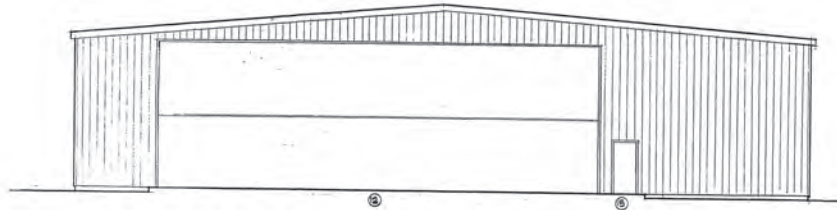
TRI-MOTOR AIRCRAFT HANGAR
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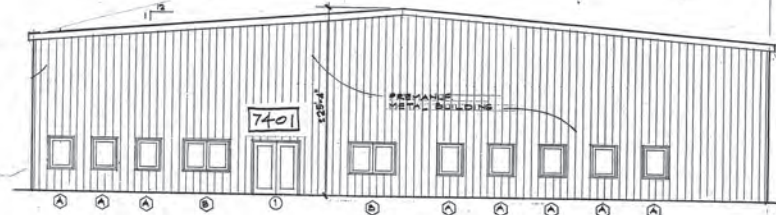
SOUTHWEST ELEVATION
1/8" = 1'-0"



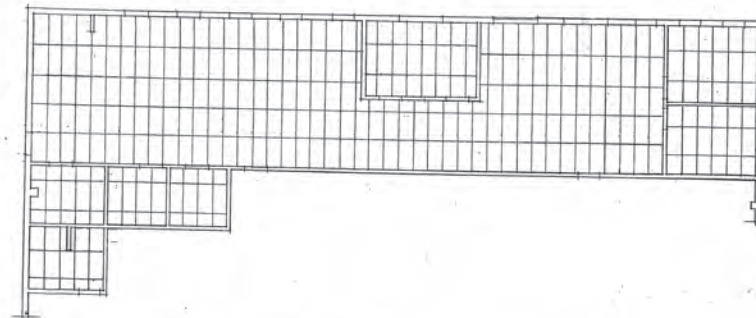
NORTHEAST ELEVATION
1/8" = 1'-0"



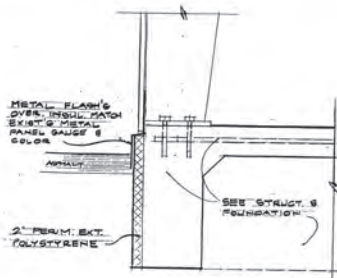
SOUTHEAST ELEVATION
1/8" = 1'-0"



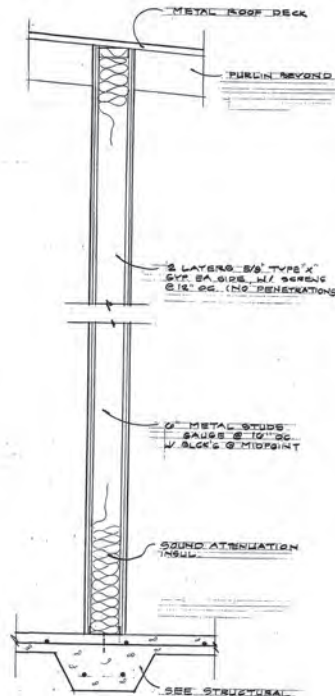
NORTHWEST ELEVATION
1/8" = 1'-0"



REFLECTED CEILING PLAN
1/8" = 1'-0"



SECTION @ PERIM. FLASHG
OVER INSULATION
1/8" = 1'-0"



TYP 2 HR. INTERIOR WALL
1/8" = 1'-0"

REVISIONS

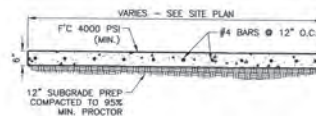
DATE 06.10.08

JOB # 08-05

SHEET # A4

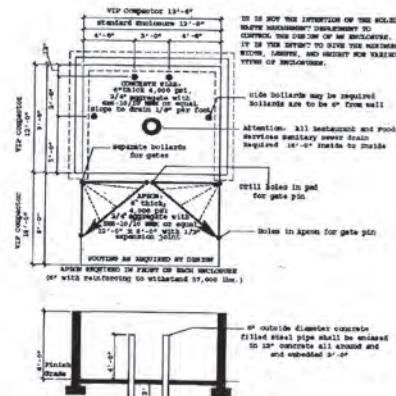
LENGYEL & ASSOCIATES 3241 SIRINGO RD. SANTA FE, NEW MEXICO

A NEW HANGAR
TO BE BUILT IN THE INFIELD @ DOUBLE EAGLE II AIRPORT FOR
TRI/MOTOR LLC.
NEW MEXICO 2008 SANTA FE

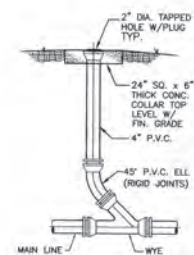


CONCRETE APRON DETAIL
SCALE: 1"=2'

Dimensions given are to the inside of enclosure walls and are the minimum sizes required for the slab itself. Modifying will vary with the design of the enclosure.



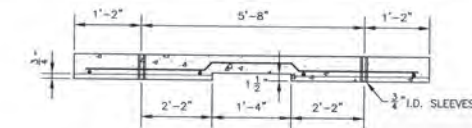
REFUSE ENCLOSURE DETAIL
N.T.S.



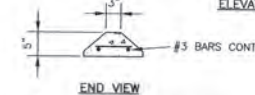
SINGLE TYPE CLEAN-OUT
SCALE: N.T.S.



PLAN



ELEVATION

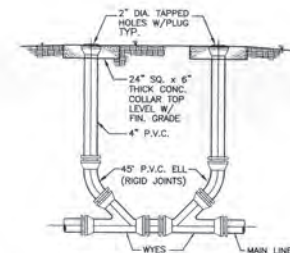


END VIEW

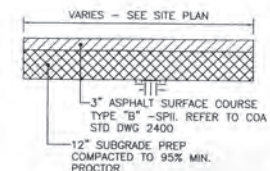
NOTE
APPROVED STANDARD MANUFACTURED
WHEEL STOP MAY BE USED.

ANCHOR ROD

CONCRETE WHEEL STOP DETAIL
N.T.S.

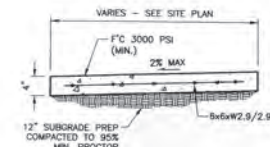


DOUBLE TYPE CLEAN-OUT
SCALE: N.T.S.



NOTE:
NO R VALUE

3" ASPHALT DETAIL
SCALE: N.T.S.



NOTES:

1. INSTALL EXPANSION JOINTS EVERY 18'-0" O.C.
2. INSTALL CONSTRUCTION JOINTS 6'-0" O.C.

CONCRETE SIDEWALK DETAIL
SCALE: 1"=1'

SITE DETAILS

TRI-MOTOR AIRCRAFT HANGAR
TRI-MOTOR, DE II AIRPORT (CITY OF ALBUQUERQUE)
ALBUQUERQUE (NEW MEXICO)

C-501

DRB PROJECT NUMBER: 1001656

SHEET

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Molzen-Corbin
& Associates, Inc.
1001 Main Street, S.E.
Albuquerque, New Mexico 87102
Tel: (505) 241-9971



PROJECT NUMBER: 1001656
DESIGNED BY: J. TELLEZ
DRAWN BY: J. BERNARD
CHECKED BY: J. PROVINE
PROD. ENG.: J. PROVINE
PROJECT DATE: APRIL 2008

TRI-MOTOR, LLC
2850 A. RUFINA ST.
SANTA FE, NEW MEXICO 87507

APRIL, 2008

for the

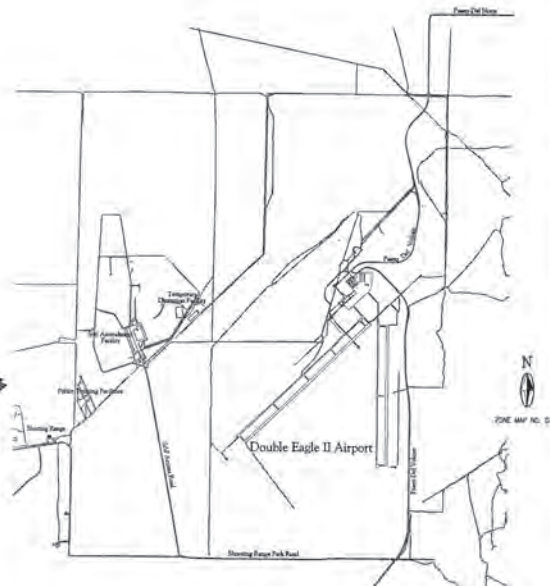
TRI-MOTOR AIRCRAFT HANGAR
7401 Paseo Del Volcan N.W.
Albuquerque, New Mexico 87121

CIVIL ENGINEERS

Molzen-Corbin & Associates
John A. Tellez P.E.
John M. Provone P.E.

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A5	ELEVATIONS
D-104	UTILITY PLAN



PROJECT NUMBER: _____
Application Number: _____
Is an Infrastructure List required? (X) Yes () No If yes, then a set of approved DRC plans with a work order is required for any construction within Public Right-of-Way or for construction of public improvements.

DRC SITE DEVELOPMENT PLAN APPROVAL:

Traffic Engineering, Transportation Division	Date
ABQWMA	Date
Parks and Recreation Department	Date
City Engineer	Date
* Environmental Health Department (conditional)	Date
Solid Waste Management	Date
DRC Chairperson, Planning Department	Date
* Environmental Health, if necessary	

THESE DRAWINGS DO NOT INCLUDE NECESSARY COMPONENTS FOR THE CONSTRUCTION SAFETY WHICH SHALL REMAIN THE RESPONSIBILITY OF THE CONTRACTOR.

ALL IDEAS, DESIGNS, ARRANGEMENTS, AND PLANS INDICATED OR REPRESENTED BY THIS INSTRUMENT ARE OWNED BY AND THE PROPERTY OF MOLZEN-CORBIN & ASSOCIATES, AND WERE CREATED AND DEVELOPED FOR USE ON AND IN CONNECTION WITH THE SPECIFIED PROJECT. THESE IDEAS, DESIGNS, ARRANGEMENTS OR PLANS SHALL NOT BE USED BY ANY PERSON, FIRM, OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT THE WRITTEN CONSENT OF MOLZEN-CORBIN & ASSOCIATES.

CERTIFICATION:

I, JOHN A. TELLEZ, REGISTERED PROFESSIONAL ENGINEER NO. 16671 HEREBY CERTIFY THAT THE FOLLOWING PLANS AND DESIGNS WERE MADE UNDER MY SUPERVISION AND DIRECTION AND THAT SAME IS TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF

John A. Tellez
J.M.P.E. NO. 16671

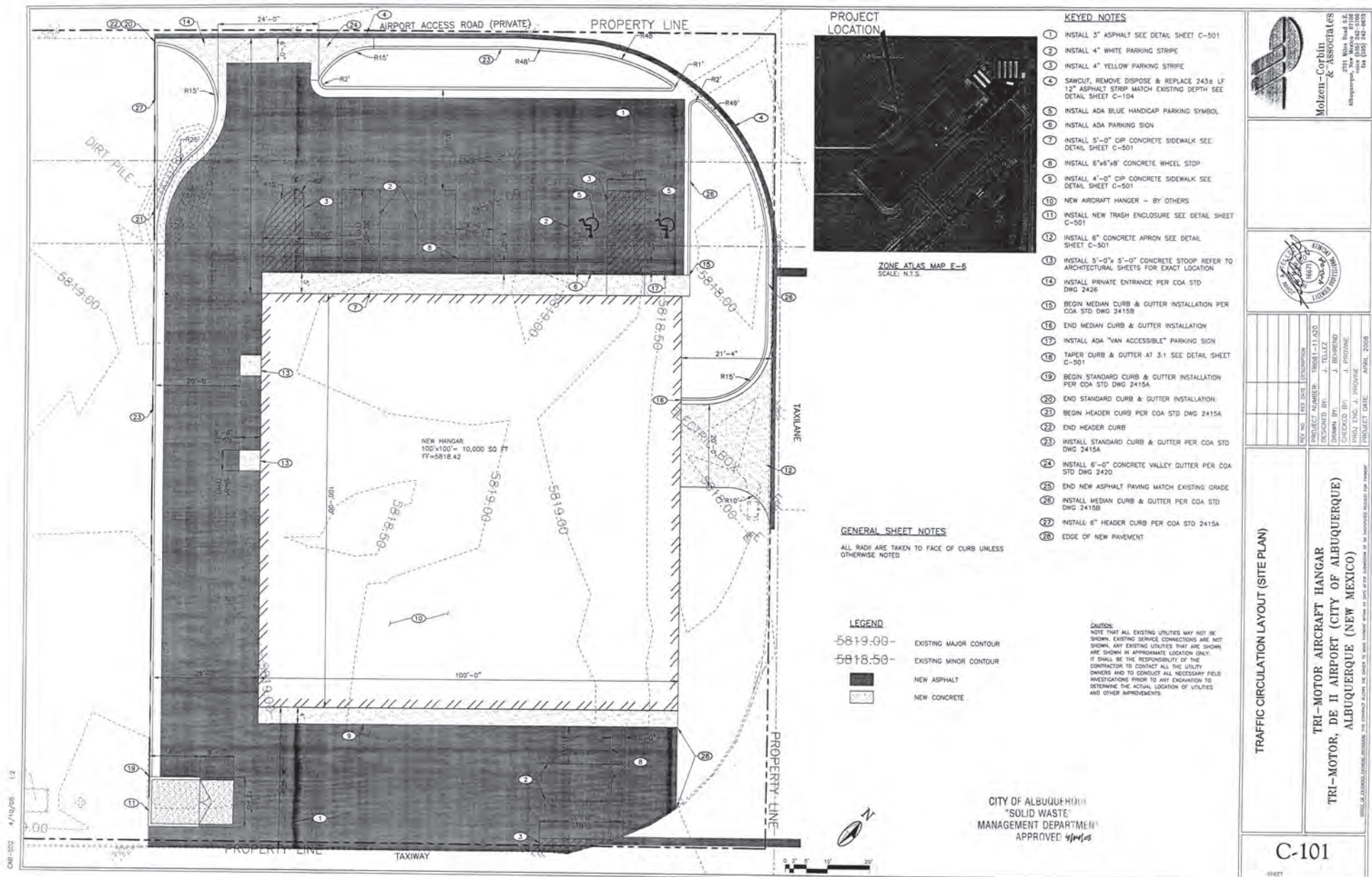
John M. Provone
J.M.P.E. NO. 16671

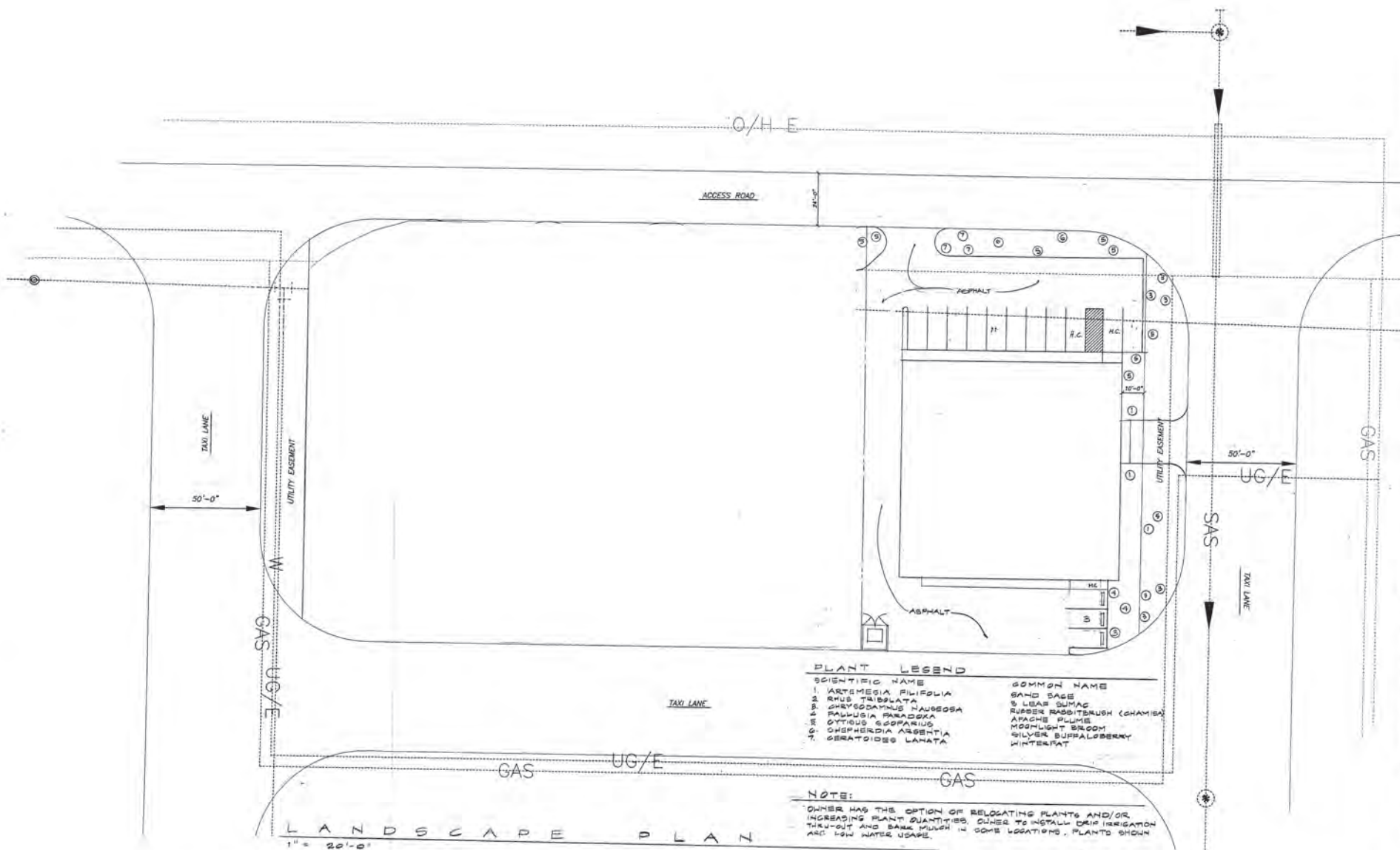


2701 MILES RD SE
ALBUQUERQUE, NEW MEXICO 87106
TEL: 505 242 5700
FAX: 505 242 0973

MOLZEN-CORBIN & Associates
ENGINEERS/ARCHITECTS/PLANNERS

G-001





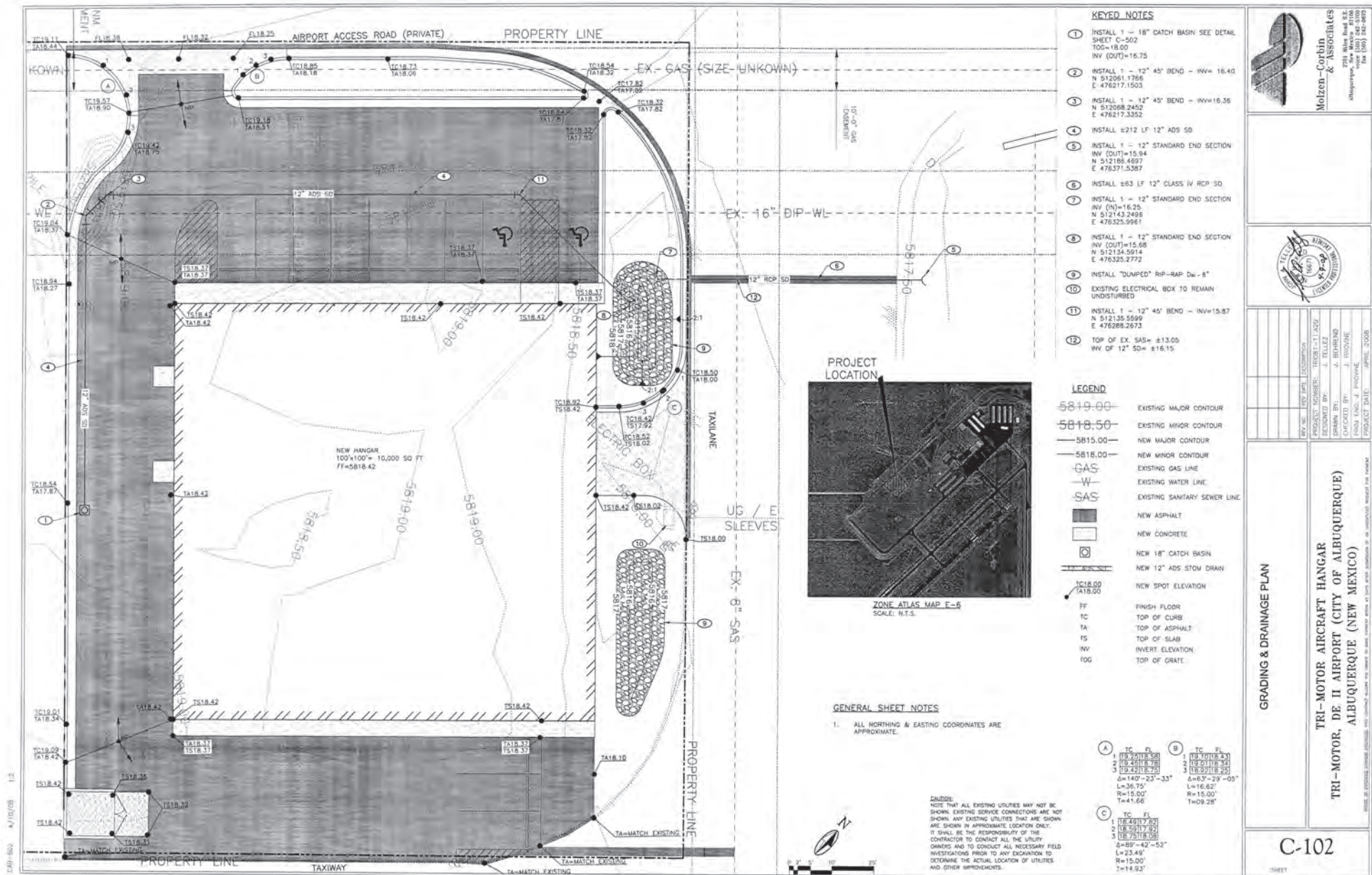
TURNER LECHNER & ROMERO



7401 PASEO DEL VOLCAN NW
ALBUQUERQUE, NEW MEXICO 87101

AIRCRAFT HANGAR AT DOUBLE EAGLE II AIRPORT
7401 PASEO DEL VOLCAN NW
ALBUQUERQUE, NEW MEXICO





2006-003 8/10/08 1:2

OFFSITE DRAINAGE BASIN SB-2/A

AREA = 4.52 AC
Q₁₀₀ = 9.0 CFS

Existing Conditions					
Description	Land Treatment				Flow (cfs)
	Area (ac)	A	B	C	
Ex. Ld	0.63	0.00%	0.00%	100.00%	1.81
TOTAL	0.63				1.81

Proposed Conditions					
Basin ID (Area (ac))	Land Treatment				Flow (cfs)
	A	B	C	D	
A	0.030	0.00%	0.00%	0.00%	0.13
A1	0.034	0.00%	0.00%	100.00%	0.10
B	0.065	0.00%	0.00%	0.00%	0.37
B1	0.018	0.00%	0.00%	0.00%	0.07
B2	0.049	0.00%	0.00%	100.00%	0.14
B3	0.095	0.00%	0.00%	0.00%	0.37
C	0.068	0.00%	0.00%	0.00%	0.42
C1	0.068	0.00%	0.00%	0.00%	0.30
D	0.127	0.00%	0.00%	0.00%	0.55
E	0.010	0.00%	0.00%	100.00%	0.04
F	0.017	0.00%	0.00%	100.00%	0.05
TOTAL	0.62				2.54

NOTE: Section 22 of the Albuquerque DFM used to calculate total Q.

NOTES

PROJECT IS LOCATED ON CITY OF ALBUQUERQUE AVIATION PROPERTY, LANDS OF DEH AIRPORT. AS SHOWN ON THE VICTORY MAP, THE SITE IS LOCATED WEST OF ALBUQUERQUE AND CAN BE ACCESSED BY EXISTING WEST BOUND 1-29 AT THE PASEO DEL VOLCAN OFFRAMP AND CONTINUING NORTH ON PASEO DEL VOLCAN TO THE INTERSECTION OF DOUBLE EAGLE & AIRPORT ACCESS ROAD. FROM THE INTERSECTION THE SITE IS APPROXIMATELY 2000' SOUTHWEST.

THE GRADING PLAN SHOWS: 1) EXISTING GRADES INDICATED BY CONTOURS AT 0.5' INTERVAL, AS SHOWN FROM THE TOPOGRAPHIC SURVEY. 2) PROPOSED GRADES ARE INDICATED BY SPOT ELEVATIONS AND CONTOURS AT 0.5' INTERVALS. FOR CONTOURS SEE SHEET C-102. 3) THE LIMIT AND CHARACTER OF THE EXISTING IMPROVEMENTS. 4) THE LIMIT AND CHARACTER OF PROPOSED IMPROVEMENTS SEE SHEET C-102 and 5) CONTINUITY BETWEEN EXISTING AND PROPOSED GRADES. SEE SHEET C-102 FOR SITE GRADING.

THE PROPOSED DEVELOPMENT CONSISTS OF THE CONSTRUCTION OF APPROXIMATELY 10,000' SF AIRPLANE HANGER INCLUDING A PAVED PARKING AREA. BUILDING AREA = 10,000' SF.

EXISTING CONDITIONS HAVE BEEN IMPROVED DUE TO THE CONSTRUCTION OF THE DEH AIRFIELD DEVELOPMENT PHASE 2 PROJECT IN THE WINTER OF 2004. THE EXISTING CONDITIONS SHOWN ON THIS SHEET REFLECT THIS PROJECT AT ITS CURRENT CONDITIONS. THESE IMPROVEMENTS ARE CONSISTENT WITH THE DOUBLE EAGLE II STORM DRAINAGE MASTERPLAN (DECEMBER 2003).

PROPOSED DEVELOPMENT GRADING WILL CONTINUE TO ALLOW THE SITE TO DRAIN FROM WEST TO EAST AND SOUTH VIA SWALE AND DITCHES SURROUNDING THE SITE. ALL DITCHES THEN DISCHARGED INTO EXISTING DRAINAGE DITCHES AND PONDS AS PER DRAINAGE MASTER PLAN. THE CALCULATIONS SHOWN FOR BOTH EXISTING AND FUTURE CONDITIONS ANALYZE THE 100-YR STORM EVENT. THE PROCEDURE FOR 40-ACRE AND SMALLER BASINS, AS SET FORTH IN THE DESIGN CRITERIA, DATED JANUARY, 1993, IN CONJUNCTION WITH THE ARD-LANDS HYDROLOGIC MODEL "HYMO-97" COMPUTER PROGRAM, HAS BEEN USED TO QUANTIFY THE PEAK RATE OF DISCHARGE AND THE VOLUME OF RUNOFF GENERATED BY THIS DEVELOPMENT. PROPOSED CONDITIONS ARE ALSO CONSISTENT WITH THE DEH STORM DRAINAGE MASTERPLAN (DECEMBER 2003) ULTIMATE CONDITIONS.

KEYED NOTES

THIS DRAWING PROVIDED FOR "INFORMATION ONLY"

REFER TO SHEET C-103A FOR ADDITIONAL INFORMATION

PROJECT LOCATION



ZONE ATLAS MAP E-6
SCALE: N.T.S.

LEGEND

- 5849.00 EXISTING MAJOR CONTOUR
- 5848.50 EXISTING MINOR CONTOUR
- 5848.00 NEW MAJOR CONTOUR
- 5847.50 NEW MINOR CONTOUR
- B BASIN DESIGNATOR
- DIRECTION OF FLOW
- NEW ASPHALT
- NEW CONCRETE
- NEW 18" CATCH BASIN
- NEW 12" ADS STORM DRAIN

GRADING & DRAINAGE PLAN
PROPOSED DRAINAGE BASINS

TRI-MOTOR AIRCRAFT HANGAR
DE II AIRPORT (CITY OF ALBUQUERQUE)
ALBUQUERQUE (NEW MEXICO)

C-103

SHEET

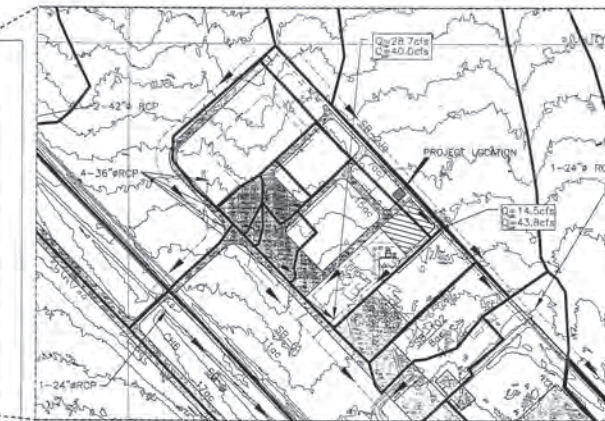
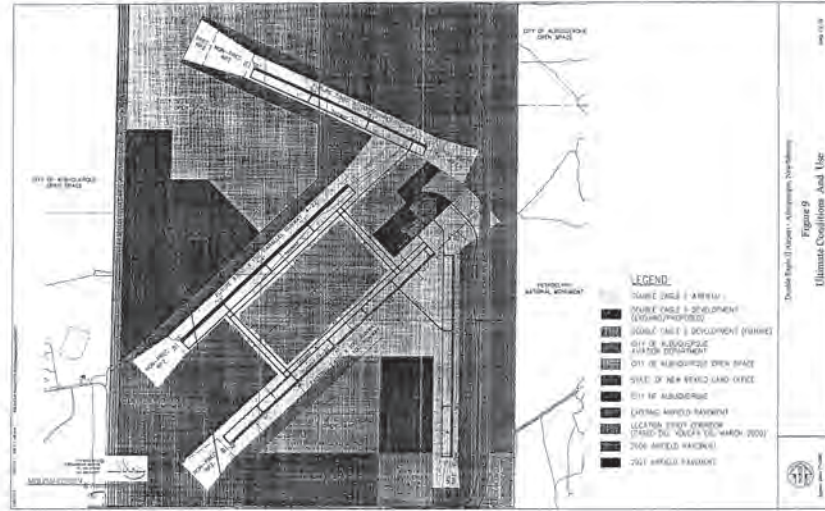


Molzen-Corbin
& Associates, Inc.
2701 Mesa Road, S.E.
Albuquerque, New Mexico 87102-1000
Tel: (505) 882-8970



NEW MEXICO	REGISTERED PROFESSIONAL ENGINEER
PROJECT NUMBER	00000111000
DESIGNED BY	A. VALLI
DRAWN BY	A. BEHREND
CHECKED BY	J. PROBYN
PROJECT NAME	ALBUQUERQUE
PROJECT DATE	JANUARY 2008

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KEYED NOTES
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GRADING AND DRAINAGE PLAN
DOUBLE EAGLE II DMP ADDITIONAL INFORMATION

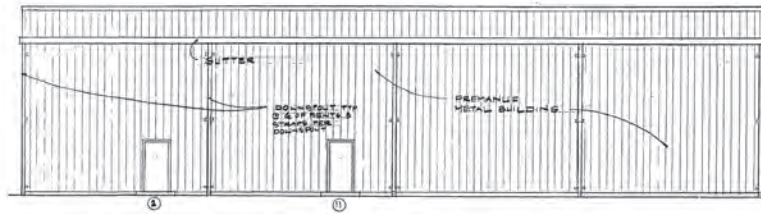
TRI-MOTOR AIRCRAFT HANGAR
TRI-MOTOR, DE II AIRPORT (CITY OF ALBUQUERQUE)
ALBUQUERQUE (NEW MEXICO)

C-103A



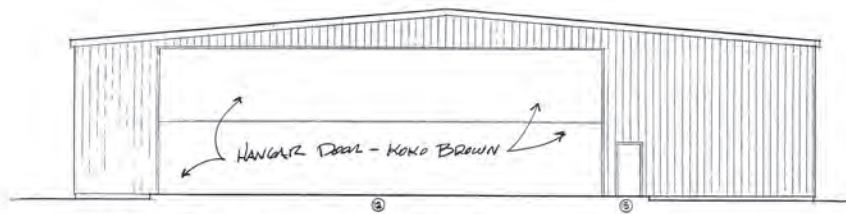
PROJECT NO.	2020-11-12
PROJECT NAME	TRI-MOTOR AIRCRAFT HANGAR
CLIENT	TRI-MOTOR
DATE	11/12/20
PROJECT LOCATION	ALBUQUERQUE, NEW MEXICO
PROJECT DATE	11/12/20

Molzen-Corbin & Associates, Inc.
Civil Engineers
1000 Main Street, S.E.
Albuquerque, NM 87102
Tel: (505) 885-8855



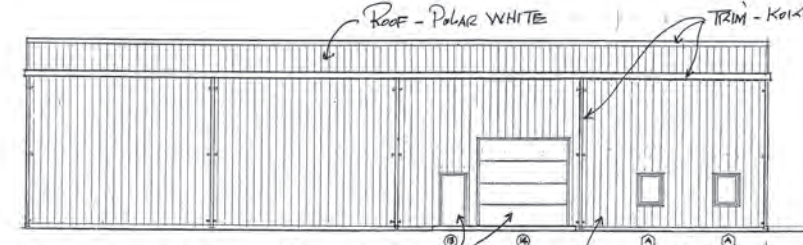
SOUTHWEST ELEVATION

1/8" = 1'-0"



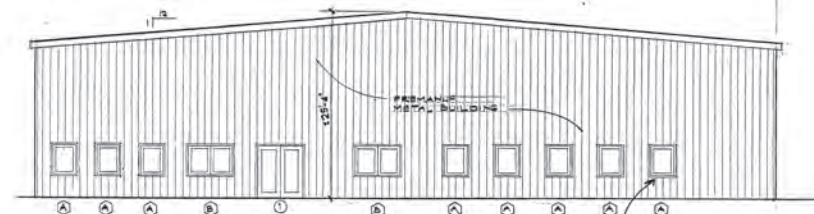
SOUTHEAST ELEVATION

1/8" = 1'-0"



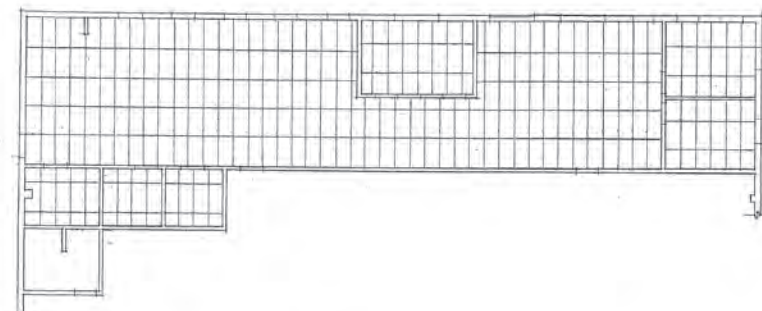
NORTHEAST ELEVATION

1/8" = 1'-0"



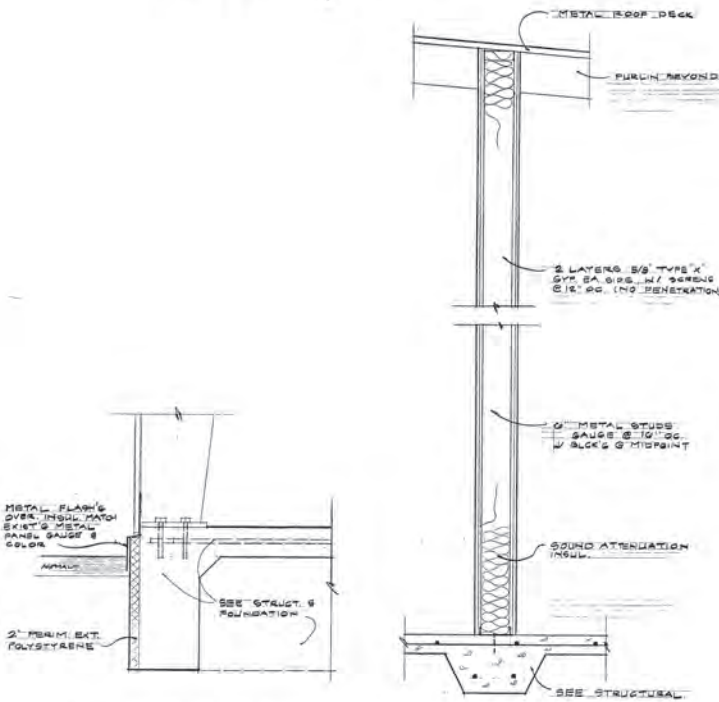
NORTHWEST ELEVATION

1/8" = 1'-0"



REFLECTED CEILING PLAN

1/8" = 1'-0"



SECTION 3 PERIM. FLG'S OVER INSULATION

1/4" = 1'-0"

TYP 2 HR. INTERIOR WALL

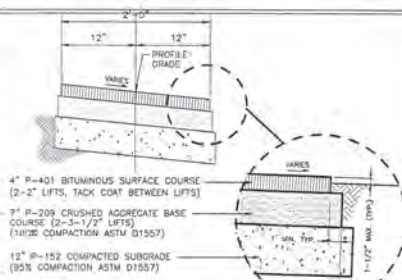
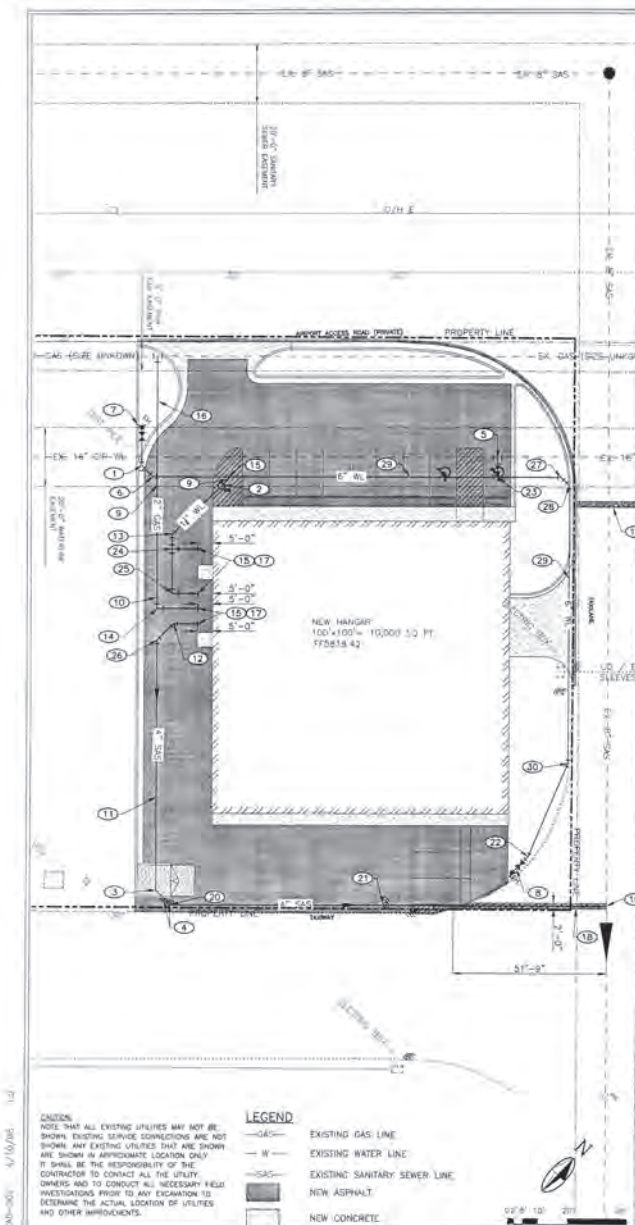
1/4" = 1'-0"



TURNER LECHNER & ROMERO
ARCHITECTS
7401 PASEO DEL VOLCAN NW
ALBUQUERQUE, NEW MEXICO 87109

TURNER LECHNER & ROMERO

A5



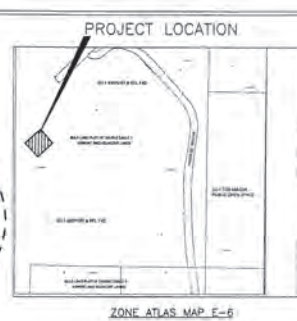
ASPHALT SAWCUT, REMOVE & REPLACE DETAIL

NOTE:
INSTALL MECHANICAL JOINT RESTRAINTS AT ALL BENDS AND JOINTS WHICH FALL WITHIN THE FOLLOWING LENGTHS OF EACH SIDE TO FITTINGS AS FOLLOWS:

PIPE TYPE	8-INCH	12-INCH
45° HORIZONTAL BEND	5'	5'
22.5° HORIZONTAL BEND	5'	5'
TEE (RESTRAIN ALL SIDES OF TEE FITTING)	1'	1'
LENGTH ALONG RUN	21'	21'

COMMENT:
ADD RESTRAINED LENGTHS FOR ALL HORIZONTAL BEND FITTING COMBINATIONS. FOR EXAMPLE, THE RESTRAINED LENGTH FOR A 8-INCH 45° BEND IN COMBO WITH A 11.25° BEND ON PVC PIPE SHOULD BE 14 FEET.

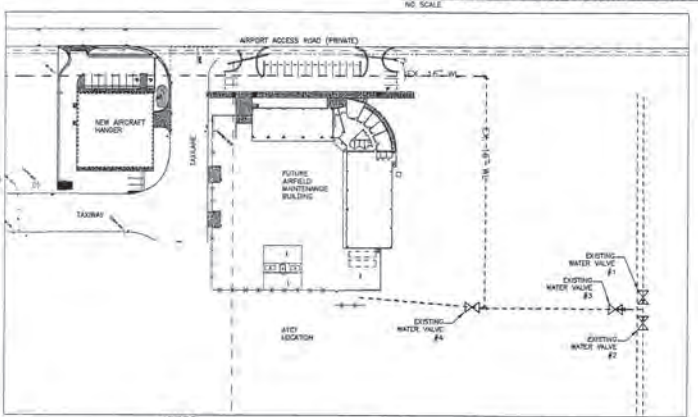
PIPE RESTRAINT REQUIREMENTS



NOTE: ALL FIRE EXTINGUISHER SHALL BE 2A-10BC AND SHALL BE MOUNTED ON THE WALL W/ STANDARD BRACKETS.



FIRE EXTINGUISHER LOCATION DETAIL



- NOTES**
- TURN OFF EXISTING WATER VALVE #3. AFTER VALVE IS OFF PROCEED WITH NEW WATERLINE CONNECTION TO EXISTING 16" WATERLINE.
 - ONCE NEW WATERLINE CONNECTION IS COMPLETE TURN ON EXISTING WATER VALVE #3.

WATER VALVE SHUT-OFF PLAN

- KEYED NOTES**
- INSTALL 1 - 6" 45° BEND
N 512037.8951, E 476116.8497
 - INSTALL 1 - 3" WATER METER PER COA STD DWG 2369 - N 512063.0276, E 476225.6770
 - INSTALL 1 - 4" 45° BEND - INV= 14.84
N 511955.7440, E 476300.6077
 - INSTALL 1 - DOUBLE CLEANOUT SEE DETAIL SHEET C-501
N 511955.6026, E 476326.0150
 - INSTALL 1 - TEE & REDUCER (16"x8") WITH WET CONNECTION. CONTRACTOR TO FIELD VERIFY EXISTING WL SIZE - N 512141.9978 E 476301.6784
 - INSTALL 1 - 4" 45° BEND
N 512057.4177 E 476222.0976
 - INSTALL 1 - 6" FIRE HYDRANT PER COA STD DWG 2340.
FLANGE ELEV.=16.75
N 512065.9184 E 476207.8610
 - INSTALL 1 - 6" FIRE HYDRANT PER COA STD DWG 2340.
FLANGE ELEV.=18.50
N 512042.8456 E 476403.2123
 - INSTALL 85 LF 12" WATERLINE 4'-0" MINIMUM BURY STOP 5' FROM EDGE OF BUILDING, NOT PART OF THIS WORK ORDER, BY OTHERS.
 - INSTALL 2" GAS LINE STOP 5' FROM EDGE OF BUILDING BY OTHERS
 - INSTALL 8260 LF 4" SANITARY SEWER LINE 5' FROM EDGE OF BUILDING
 - INSTALL 1 - 4" 45° BEND - INV= 16.80
N 512025.9230, E 476261.8035
 - INSTALL 1 - 12" 45° BEND
N 512047.5276, E 476240.3953
 - INSTALL 1 - 2" 30° BEND
N 512025.8086, E 476254.1164
 - INSTALL 1 - SERVICE CONNECTION PER COA STD DWG 2301
 - INSTALL 1 - TEE WITH HIGH PRESSURE CONNECTION CONTRACTOR TO FIELD VERIFY EXISTING GAS LINE SIZE - N 512073.7160 E 476201.7591
 - SEE MECHANICAL DRAWINGS FOR UTILITY CONNECTIONS INTO BUILDING SAS INV=16.98
 - SAWCUT NEAT LINE, REMOVE, DISPOSE & REPLACE EXISTING ASPHALT. SEE DETAIL THIS SHEET & COA STD DWG 2465
 - CONNECT NEW GAS LINE TO EXIST. SAS WITH SADDLE CONNECTION N 512056.6631, E 476434.2047 - INV= 311.84
 - INSTALL 1 - 4" 45° BEND - INV= 14.80
N 511955.5591, E 476327.6784
 - INSTALL 1 - SINGLE CLEANOUT SEE DETAIL SHEET C-501 - N 512005.4327, E 476380.2302
 - INSTALL 1 - 6" 22.5° BEND - N 512050.5487, E 476403.4112
 - INSTALL 1 - TEE (6"x8"x8") - N 512137.2320, E 476306.2011
 - INSTALL 1 - 12" TEE WITH STUB AND CAP
N 512043.8432, E 476243.8918
 - INSTALL 1 - 12" 90° BEND N 512032.9194, E 476254.2685
N 512017.9289, E 476261.5843
 - INSTALL 1 - 6" 45° BEND
N 512151.8893, E 476321.7517
 - INSTALL 1 - 6" 45° BEND
N 512151.8893, E 476325.5802
 - INSTALL 8300 LF 6" WATERLINE, 4'-0" MIN. BURY
 - INSTALL 1 - 6" 22.5° BEND
N 512063.0046, E 476390.9518

GENERAL SHEET NOTES

- ALL NORTHING & EASTING COORDINATES ARE APPROXIMATE.



REV	DATE	DESCRIPTION
1	11/15/20	ISSUED FOR PERMIT
2	11/15/20	ISSUED FOR PERMIT
3	11/15/20	ISSUED FOR PERMIT
4	11/15/20	ISSUED FOR PERMIT
5	11/15/20	ISSUED FOR PERMIT
6	11/15/20	ISSUED FOR PERMIT
7	11/15/20	ISSUED FOR PERMIT
8	11/15/20	ISSUED FOR PERMIT
9	11/15/20	ISSUED FOR PERMIT
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28	11/15/20	ISSUED FOR PERMIT
29	11/15/20	ISSUED FOR PERMIT
30	11/15/20	ISSUED FOR PERMIT

UTILITY PLAN

TRI-MOTOR AIRCRAFT HANGAR
TRI-MOTOR, DE II AIRPORT (CITY OF ALBUQUERQUE)
ALBUQUERQUE (NEW MEXICO)

C-104

CITY of ALBUQUERQUE
FIFTEENTH COUNCIL

COUNCIL BILL NO. O-02-28

ENACTMENT NO. 23-2002

SPONSORED BY: Miguel Gomez, by request

ORDINANCE

ANNEXATION, 01114-01874/01110 01875, ANNEXING 899-ACRES MORE OR LESS, LOCATED ON PASEO DEL VOLCAN NW, BETWEEN THE SHOOTING RANGE PARK ACCESS ROAD AND DOUBLE EAGLE AIRPORT AND AMENDING THE ZONE MAP TO ESTABLISH SU-1 FOR AIRPORT AND RELATED FACILITIES AND SU-1 FOR MAJOR PUBLIC OPEN SPACE.

BE IT ORDAINED BY THE COUNCIL, THE GOVERNING BODY OF THE CITY OF ALBUQUERQUE:

Section 1. AREA PROPOSED FOR ANNEXATION. The owners of the area proposed for annexation hereby presented a signed petition to annex the following territory: 899-acres, more or less, located on Paseo del Volcan between the Shooting Range Park access road and Double Eagle Airport; and more particularly described as follows:

A. Section 36, T11N, R1E and a portion of Section 2, T10N, R1E as described in Exhibit A.

B. All of the right-of-way adjoining the land described in A. of this section to the extent it is not already in the City.

The above described territory is contiguous to the City of Albuquerque.

Section 2. ANNEXATION ACCEPTED. The petition and the area specified in Section 1 above are accepted and the above territory is hereby annexed.

Section 3. ZONE MAP AMENDED. The zone map adopted by Section 14-16-1-1 et. seq. R.O.A. 1994 is hereby amended, establishing SU-1 for Airport and Related Uses zoning and SU-1 for Major Public Open Space as shown on Exhibit B.



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Page: 1 of 5
06/05/2002 04:05P
BK-R37 Pg-2214

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1 Section 4. WEST SIDE STRATEGIC PLAN POLICIES AFFIRMED. The
2 annexation of the subject lands is supported by and/or furthers the applicable
3 Policies of the West Side Strategic Plan regarding infrastructure, annexation,
4 employment, emergency services, Double Eagle II Airport, Petroglyph National
5 Monument, Open Space, export industries, and the municipal limits; including
6 Policies A-1, 1.6, 2.3, 3.74, 3.75, 3.76, 3.79, 3.83, 4.19, 6.27, and 7.12.

7 Section 5. The infrastructure phasing plan adopted by the West Side
8 Strategic Plan designates the Double Eagle II Airport area as a "#3 Priority" area
9 that is not scheduled to receive public infrastructure until 2055. By annexing
10 and zoning the subject property, it is the intent of the Council to accommodate
11 the provision of utilities to the annexed area for the proposed zoning to the
12 extent authorized under City policy, so the WSSP phasing plan should be
13 amended to designate the airport and related commercial uses in the area
14 annexed by this ordinance are a "#1 Priority." The phasing and priority for any
15 other areas in the WSSP shall remain unchanged by this ordinance.

16 Section 6. FINDINGS ACCEPTED. The Council adopts the following zone
17 map amendment findings recommended by the Environmental Planning
18 Commission:

- 19 A. This is a request for establishment SU-1 for Airport and Related Uses zoning
20 for approximately 727 acres and the establishment of SU-1 for Major Public
21 Open Space zoning for approximately 172 acres.
- 22 B. Approval of this request will facilitate a exchange of lands between multiple
23 owners which will create a buffer of Open Space land between the airport
24 and the monument and consolidate the lands and zoning for the Double
25 Eagle Airport.
- 26 C. The applicant has adequately justified that proposed zone map
27 amendments as per R-270-1980, Section 1.D.(3): the requested SU-1 for
28 Airport and Related Uses zoning is more beneficial to the community as
29 articulated in the *Comprehensive Plan* by creating an opportunity for high
30 quality, planned development that is bounded by permanent open-space
31 (Reserve Area Goal). The proposed SU-1 for Airport zoning will create
32 economic development opportunities that benefit both the West Side and



Mary Herrera

Bern. Co. ANEX

R 15.00

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Page: 2 of 5
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Bk-R37 Pg-2214

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[-Bracketed/Strikethrough Material-] - Deletion

- 1 the City and the proposed SU-1 Major Public Open Space will allow for
2 greater protection of the volcanoes by providing a buffer of open space
3 between the airport and the Petroglyph National Monument and by
4 consolidating open space lands which will facilitate the creation of trail
5 corridors through the open space network (Policies d and a, Open Space
6 Network).
- 7 D. The requests further the goals and policies of the *West Side Strategic Plan*
8 (*WSSP*) by providing an opportunity for large scale employment on the
9 West Side (Objectives 1, 8, pages 18, 19) at the Double Eagle Airport, which
10 is identified as a major Employment Center (page 39), and by protecting the
11 unique resources of the West Side through the creation of an Open Space
12 buffer along the western boundary of the Petroglyph Monument (Objective
13 3, page 19; Policy 3.75).
- 14 E. The SU-1 zoning requires that site development plans be submitted for
15 review by the EPC. It is the applicant's intent, upon successful annexation
16 and replatting of affected properties to submit a Master Plan for the entire
17 area zoned SU-1 for Airport and Related Uses.

18 Section 7. SEVERABILITY CLAUSE. If any section, paragraph, sentence,
19 clause, word or phrase of this ordinance is for any reason held to be invalid or
20 unenforceable by any court of competent jurisdiction, such decision shall not
21 affect the validity of the remaining provisions of this ordinance. The Council
22 hereby declares that it would have passed this ordinance and each section,
23 paragraph, sentence, clause, word or phrase thereof irrespective of any
24 provisions being declared unconstitutional or otherwise invalid.


25 Section 8. EFFECTIVE DATE AND PUBLICATION. This ordinance shall
26 become effective five or more days after publication in summary when a copy
27 of the ordinance and a plat of the territory hereby annexed is filed in the office
28 of the County Clerk.

30
31
32 X:\SHARE\Legislation\Fifteen\O-28fin.doc



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5679555
Page: 3 of 5
06/05/2002 04:05P
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1 PASSED AND ADOPTED THIS 6th DAY OF MAY, 2002
2 BY A VOTE OF: 9 FOR 0 AGAINST.

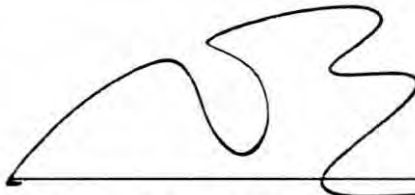
10 

11
12 Brad Winter, President

13 City Council

16
17 APPROVED THIS 22 DAY OF May, 2002

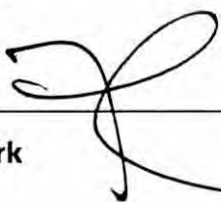
18
19 Bill No. O-02-28

20
21 

22 Martin Chávez, Mayor

23 City of Albuquerque

24
25 ATTEST:

26
27 
28 City Clerk



Mary Herrera

Bern. Co. ANEX

R 15.00

2002072415
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Page: 4 of 5
06/05/2002 04:05P
Bk-A37 Pg-2214

EAGLE II
AIRPORT

JIM MCDOWELL RD

SOIL AMENDMENT FACILITY
RD

HOLPIN RANGE ACCESS RD

PASEO DEL VOLCAN

PETROGLYPH NATIONAL MONUMENT

PETROGLYPH NATIONAL MONUMENT

PETROGLYPH NATIONAL MONUMENT

Proposed annexation in Zone Map F-6
Sec. 2, T10N, R1E & Sec. 36, T11N, R1E



----- MUNICIPAL LIMITS



AREA PROPOSED FOR



GRAY SHADING INDICATES COUNTY



Mary Herrera

Bern. Co. ANEX

R 15.00

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Map Printed March 06, 2002



City of Albuquerque
Planning Department
Development Services Division
P.O. Box 1293
Albuquerque, New Mexico 87103

Date: February 21, 2003

OFFICIAL NOTIFICATION OF DECISION

FILE: **Project # 1001656**
02EPC-01956 EPC Site Development Plan
-Building Permit

City of Albuquerque
Aviation Department
P.O. Box 9948
Albuq. NM 87119

LEGAL DESCRIPTION: for all or a portion of Tract(s) A & E Sec. 34 T11N R1E, Sec 36 T11N R1E, **Double Eagle II Airport**, zoned SU-1 Airport, located on DOUBLE EAGLE II AIRPORT NW, between PASEO DEL VOLCAN and SHOOTING RANGE PARK RD. (E-6)
Chris Hyer, Staff Planner

On February 20, 2003 the Environmental Planning Commission voted to approve Project 1001656/ 2EPC 01156 a site development plan for building permit, for a portion of Section 34, Township 11-N, Range 1-E NMPM, located on the un-platted Lands of Double Eagle-II Airport between Paseo del Volcan and Shooting Range Park Road, based on the following Findings and subject to the following Conditions:

FINDINGS:

1. This is a request for approval for a site development plan for building permit for an approximately a 3,869 acre site for the construction of a water and waste-water facility to serve the *Double Eagle -II* (DE-II) Airport and adjacent City facilities. The water system consists of three different locations on the lands of the airport for *Double Eagle-II* Airport.
2. The site development plan furthers the Goals and policies of the *Comprehensive Plan* by proposing the development of a public utility that will facilitate expansion of activity at the DE-II airport, an employment center, and surrounding facilities. (Goal D.6, Policies a, b, c, f, g)

3. The site development plan also furthers the applicable Goals and policies of the *Comprehensive Plan* by proposing the development of a public utility water system to allow opportunity for future development of high quality, mixed-use, largely self-sufficient planned communities, bounded by permanent open-space, in appropriate outlying areas, and to protect the non-urban metropolitan areas as rural unless such planned communities are developed. (Goal B.2)
4. The site development plan furthers the applicable policies and intent of the *West Side Strategic Plan* by proposing a public utility use that will allow future growth for the City as well as providing potable water to the *Double Eagle-II Airport*. The additional components of the existing facility will be sensitive to visual impacts on the National Monument and the Open Space area. (Policies 1.6, 3.76, 3.77, 3.78, 3.79, 3.80, 3.81, 3.82)
5. The development of the water utility system will help the City achieve it's commitment to the lease agreement it negotiated with Eclipse Aviation of providing all infrastructure necessary for the development of their aircraft manufacturing facility.

CONDITIONS:

1. The submittal of this site plan to the DRB shall meet all EPC conditions. A letter shall accompany the submittal, specifying all modifications that have been made to the site plan since the EPC hearing, including how the site plan has been modified to meet each of the EPC conditions. Unauthorized changes to this site plan, including before or after DRB final sign-off, may result in forfeiture of approvals.
2. Conditions of approval for the proposed Site Development Plan for Building Permit shall include:
 - a. The Developer is responsible for permanent improvements to the transportation facilities adjacent to the proposed site development plan. Those improvements will include any additional right-of-way requirements, paving, curb and gutter and sidewalk that have not already been provided for, if applicable.
 - b. Site shall comply and be designed per DPM Standards and current COA Zoning Code.
 - c. An approved conceptual grading and drainage plan is required for Site Plan sign-off by the City Engineer. Approval of drainage plan required prior to placement on DRB agenda.

IF YOU WISH TO APPEAL/PROTEST THIS DECISION, YOU MUST DO SO BY **MARCH 7, 2003** IN THE MANNER DESCRIBED BELOW. A NON-REFUNDABLE FILING FEE WILL BE CALCULATED AT THE LAND DEVELOPMENT COORDINATION COUNTER AND IS REQUIRED AT THE TIME THE APPEAL IS FILED.

OFFICIAL NOTICE OF DECISION
FEBRUARY 20, 2003
PROJECT #1001656
PAGE 3

Appeal to the City Council: Persons aggrieved with any determination of the Environmental Planning Commission acting under this ordinance and who have legal standing as defined in Section 14-16-4-4.B.2 of the City of Albuquerque Comprehensive Zoning Code may file an appeal to the City Council by submitting written application on the Planning Department form to the Planning Department within 15 days of the Planning Commission's decision. The date the determination in question is issued is not included in the 15-day period for filing an appeal, and if the fifteenth day falls on a Saturday, Sunday or holiday as listed in the Merit System Ordinance, the next working day is considered as the deadline for filing the appeal. The City Council may decline to hear the appeal if it finds that all City plans, policies and ordinances have been properly followed. If it decides that all City plans, policies and ordinances have not been properly followed, it shall hear the appeal. Such appeal, if heard, shall be heard within 45 days of its filing.

YOU WILL RECEIVE NOTIFICATION IF ANY OTHER PERSON FILES AN APPEAL. IF THERE IS NO APPEAL, YOU CAN RECEIVE BUILDING PERMITS AT ANY TIME AFTER THE APPEAL DEADLINE QUOTED ABOVE, PROVIDED ALL CONDITIONS IMPOSED AT THE TIME OF APPROVAL HAVE BEEN MET. SUCCESSFUL APPLICANTS ARE REMINDED THAT OTHER REGULATIONS OF THE CITY MUST BE COMPLIED WITH, EVEN AFTER APPROVAL OF THE REFERENCED APPLICATION(S).

Successful applicants should be aware of the termination provisions for Site Development Plans specified in Section 14-16-3-11 of the Comprehensive Zoning Code. Generally plan approval is terminated 7 years after approval by the EPC

Sincerely,


Victor J. Chavez
Planning Director

VJC/CH/ac

cc: Molzen-Corbin & Associates, 2701 Miles Road SE, Albuquerque, NM 87106



City of Albuquerque
Planning Department
Development Services Division
P.O. Box 1293
Albuquerque, New Mexico 87103

Date: July 18, 2003

OFFICIAL NOTIFICATION OF DECISION

FILE: **Project # 1001656 ***
03EPC-00915 Zone Map Amendment
03EPC-00916 EPC Site Development Plan-
Subdivision

City of Albuquerque, Real Property
P.O. Box 1293
Albuquerque, NM 87103

LEGAL DESCRIPTION: for all or a portion of Tract(s) N, O, and Lots 4, 3 & 2, **BULK LAND PLAT DOUBLE EAGLE II AIRPORT AND ADJACENT LANDS**, a zone map amendment from SU-1 OPEN SPACE to SU-1 AIRPORT RELATED FACILITIES, located on NW OF DOUBLE EAGLE AIRPORT, between and, containing approximately 590 acre(s). (C-4) Chris Hyer, Staff Planner

On July 17, 2003 the Environmental Planning Commission voted to approve Project 1001656/ 03EPC 00915, a request a zone map amendment from SU-1 for Major Public Open Space to SU-1 for Airport and related uses, for a 103.88-acre portion of Tracts "N" and "O" and Lots 4, 3 and 2 of the Bulk Land Plat of Double Eagle II Airport and Adjacent Lands consisting of 590.08 acres, located northwest of *Double Eagle II* Airport between the Soils Amendment Facility and the Alameda Land Grant, based on the following Findings and subject to the following Conditions:

FINDINGS:

1. This is a request for a zone map amendment from SU-1 for Major Public Open Space to SU-1 for airport and related uses for a 103.88-acre portion of a 590.08 acre site consisting of Tracts "N" and "O" and Lots 4, 3 and 2 as shown on the Bulk Land Plat of Double Eagle II Airport and Adjacent Lands, located northwest of Double Eagle II Airport, between the Soils Amendment Facility and the Alameda Land Grant Boundary Line.

OFFICIAL NOTICE OF DECISION
JULY 17, 2003
PROJECT #1001656
PAGE 2 OF 4

2. The subject site is located in the "Reserve Area" as designated by the *Comprehensive Plan*. The Goal for this land-use designation is specific to future planned communities, not the existing airport. The airport is a designated Activity Center. Therefore, this request does not contradict the *Comprehensive Plan* Reserve Area Goal by not incorporating a residential component. Consequently, the reference to the planned communities is not applicable to the site. The request proposes improvements to and the expansion of an existing facility that predated the "Reserve Area" designation.
3. This request furthers Part II, Section D. of the *Comprehensive Plan*, Community Resource Management, Goal 6, Economic Development by helping to achieve steady and diversified economic development balanced with other important social, cultural and environmental goals. This request is appropriate as this Goal speaks to economic activity and it's development.
4. The *Comprehensive Plan* designates *Double Eagle II Airport* and surrounding lands as an Activity Center.
5. The *West Side Strategic Plan* designates the airport and surrounding lands as an employment center. (Policy 1.6)
6. The request for a zone change meets the requirements of *Resolution 270-1980* because a different use category is more advantageous to the community, as articulated in the *Comprehensive Plan* and the *West Side Strategic Plan*.
7. A site development plan for subdivision accompanies this request.

CONDITION:

1. The new zoning lines shall correspond to the accompanying site development plan for subdivision (03EPC 00916).

On July 17, 2003 the Environmental Planning Commission voted to approve Project 1001656/03EPC 00916, a request for site development plan for subdivision, for a portion of Tracts "N" and "O" and Lots 4, 3 and 2 of the Bulk Land Plat of Double Eagle II Airport and Adjacent Lands consisting of 590.08 acres, located northwest of *Double Eagle II Airport* between the Soils Amendment Facility and the Alameda Land Grant, based on the preceding Findings and subject to the following Conditions:

FINDINGS:

1. This is a request for Site Development Plan for Subdivision for portions of a 590.08 acre site consisting of Tracts "N" and "O" and Lots 4,3 and 2 as shown on the Bulk Land Plat of Double Eagle II Airport and Adjacent Lands, located northwest of Double Eagle II Airport, between the Soils Amendment Facility and the Alameda Land Grant Boundary Line.
2. The site will be subdivided into four Parcels (Parcels 1, 2, 3 and 4) containing 103.88 acres, Tract "N-1" that is a 100-foot Right-Of-Way along the northern border of the site with a 100-foot easement on the upper eastern border that provides private access to the Tract south of the four Parcels and Tract "O-1" (containing 451.52 acres) on the southern portion of the site. Tract "O-1" will contain the majority of the acreage of the overall site.
3. The request furthers Part II, Section D. of the *Comprehensive Plan, Community Resource Management*, Goal 6, Economic Development by helping to achieve steady and diversified economic development balanced with other important social, cultural and environmental goals. This request is appropriate as this Goal speaks to economic activity and it's development.
4. The *Comprehensive Plan* designates the *Double Eagle II Airport* and its surrounding lands as an Activity Center. Part II, Section D. of the *Comprehensive Plan, Community Resource Management*, Goal 6, Economic Development, policies a, b and g are appropriate in support of developing the Activity Center to located at the Airport.
5. The airport and its surrounding lands are designated in the *West Side Strategic Plan* as an employment center that shall emphasize the creation of base or export jobs and regionally-based jobs on the West Side. (Policy 3.85) Eclipse Aviation will be building and operating their manufacturing facility at DE-II in the year 2006-2007. The *West Side Strategic Plan* section, "Encouraging Employment Growth on the West Side" is applicable to the lands surrounding the airport and the airport itself because of Eclipse Aviation and its respective related supporting industries.
6. The NMSHTD has approved a new alignment of *Paseo del Volcan* that will run north-south on the western most portion of the site. A reserved easement of 400-feet must remain on the site until the NMSHTD can place the exact location of *Paseo del Volcan*.
7. This site plan submittal accompanies a related zone map amendment.

OFFICIAL NOTICE OF DECISION
JULY 17, 2003
PROJECT #1001656
PAGE 4 OF 4

CONDITIONS:

1. The EPC delegates final sign-off authority of this site development plan to the Development Review Board (DRB). The DRB is responsible for ensuring that all EPC Conditions have been satisfied and that other applicable City requirements have been met.
2. Reservation for future dedication of a *minimum* 400-foot right-of-way width along the western boundary of the site for Paseo del Volcan, a limited access, principal arterial as designated on the Long Range Roadway System, and more specifically described on the attached right-of-way map.

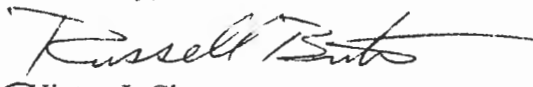
IF YOU WISH TO APPEAL/PROTEST THIS DECISION, YOU MUST DO SO BY **AUGUST 1, 2003** IN THE MANNER DESCRIBED BELOW. A NON-REFUNDABLE FILING FEE WILL BE CALCULATED AT THE LAND DEVELOPMENT COORDINATION COUNTER AND IS REQUIRED AT THE TIME THE APPEAL IS FILED.

Appeal to the City Council: Persons aggrieved with any determination of the Environmental Planning Commission acting under this ordinance and who have legal standing as defined in Section 14-16-4-4.B.2 of the City of Albuquerque Comprehensive Zoning Code may file an appeal to the City Council by submitting written application on the Planning Department form to the Planning Department within 15 days of the Planning Commission's decision. The date the determination in question is issued is not included in the 15-day period for filing an appeal, and if the fifteenth day falls on a Saturday, Sunday or holiday as listed in the Merit System Ordinance, the next working day is considered as the deadline for filing the appeal. The City Council may decline to hear the appeal if it finds that all City plans, policies and ordinances have been properly followed. If it decides that all City plans, policies and ordinances have not been properly followed, it shall hear the appeal. Such appeal, if heard, shall be heard within 45 days of its filing.

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Successful applicants should be aware of the termination provisions for Site Development Plans specified in Section 14-16-3-11 of the Comprehensive Zoning Code. Generally plan approval is terminated 7 years after approval by the EPC

Sincerely,


for Victor J. Chavez
Planning Director

VJC/CH/ac

cc: Santiago Romero Jr., 4004 Carlisle Blvd. NE, Suite C4, Albuquerque, NM 87107

CITY of ALBUQUERQUE
FIFTEENTH COUNCIL

COUNCIL BILL NO. R-02-170 ENACTMENT NO. 16-2003

SPONSORED BY:

RESOLUTION

2 THE *DOUBLE EAGLE II* AIRPORT MASTER PLAN, 02EPC 00989, PROJECT
3 NUMBER 1001656, ADOPTING THE MASTER PLAN TO BECOME AN OFFICIAL
4 CITY DOCUMENT.

5 **WHEREAS, the Double Eagle II Airport is one of two airports under the**
6 **City's jurisdiction; and**

7 WHEREAS, the *Comprehensive Plan* designates the *Double Eagle II Airport*
8 as an Activity Center; and

9 WHEREAS, the *West Side Strategic Plan* designates the *Double Eagle II*
10 *Airport* as an employment center; and

11 WHEREAS, the land use of the airport is compatible with the surrounding
12 open space; and

13 WHEREAS, the open space to the west of the airport creates a buffer to the
14 *Petroglyph National Park, Volcano Park* and the open space contained within
15 these parks boundaries; and

16 WHEREAS, improvements made to facilities at the airport are integral to the
17 lease agreement between the City and *Eclipse Aviation*; and

18 WHEREAS, the City of Albuquerque Open Space Division, the City of
19 Albuquerque's Aviation Department, the New Mexico State Land Office and the
20 Black family entered into a land exchange arrangement finalized July 2002,
21 which was to facilitate land owner ship allowing *Eclipse Aviation* to locate at
22 the airport.

23 BE IT RESOLVED BY THE COUNCIL, THE GOVERNING BODY OF THE CITY OF
24 ALBUQUERQUE THAT:

25 Section 1. The Master Plan for *Double Eagle II Airport* is adopted by the
26 City of Albuquerque.

1 Section 2. FINDINGS AND CONDITIONS ACCEPTED. The following
2 Environmental Planning Commission findings and conditions of the Master
3 Plan approval are adopted by the City Council:

4 (A) This is a request for review and adoption of the *Double Eagle II*
5 *Airport* Master Plan. The *Double Eagle II* airport site
6 encompasses 4,044 acres of unplatted land situated north of the
7 Atrisco Land Grant Boundary, south of the Alameda Land Grant
8 Boundary and west of the Petroglyph National monument. The
9 Master Plan specifies future developments and expansion of the
10 existing airport facility, *e.g.*, the extension of runways, location of
11 future buildings and the location of the *Eclipse Aviation*
12 manufacturing facility.

13 (B) Development of the Master Plan for *Double Eagle II Airport* is
14 primarily a response to the requirements set forth by the Federal
15 Aviation Administration (FAA). Since the FAA has jurisdiction
16 over the airside related operations, any expansion or
17 development of an airport is required to abide by FAA
18 regulations.

19 (C) The FAA requires City approval of a master plan to allow for
20 future development and expansion of *Double Eagle II Airport*.
21 Master Plan adoption is required for the allocation of grant
22 monies awarded by the federal government. The FAA regulations
23 require the evaluation of wind, noise, drainage of the site and
24 temperature (in regards to density altitude). The Master Plan
25 appears to address these issues thoroughly and satisfactorily.

26 (D) The EPC is reviewing this Master Plan for advisory purposes to
27 the City Council. The FAA requires that the City Council (and its
28 Land Use, Planning and Zoning component) must review this
29 master plan and adopt it in order to be eligible for available grant
30 monies for the airport's expansion.

31 (E) The submitted Master Plan does not contradict the
32 *Comprehensive Plan's* Reserve Area Goal by not incorporating a
33 residential component (none is mentioned in the Master Plan).

- 1 Consequently, the reference to the planned communities is not
- 2 applicable to this site. This goal is specific to future planned
- 3 communities, not the existing airport. The Master Plan proposes
- 4 improvements to and the expansion of an existing facility that
- 5 predated the "Reserve Area" designation.
- 6 (F) The submitted master plan furthers the *Comprehensive Plan's*
- 7 Reserve Area Goal by demonstrating that the *Double Eagle II*
- 8 *Airport* is surrounded by open space and the airport use is
- 9 compatible with the open space lands in the vicinity. Further, the
- 10 open space provides adequate buffers for the impacts of noise to
- 11 the surrounding areas. The buffer is adequate for both the
- 12 existing facility and future expansion as proposed by the Master
- 13 Plan.
- 14 (G) The *Double Eagle II Airport* Master Plan conforms with the *West*
- 15 *Side Strategic Plan* and its policies that are targeted specifically
- 16 to the airport in providing a framework to manage future growth
- 17 and development at the airport that furthers developments on the
- 18 west side. Policies 1.6 and 3.83 are specific to the creation of
- 19 base jobs and the location of an employment center at *Double*
- 20 *Eagle II Airport*. Policies 3.74, 3.75, 3.76 and 3.79 are specific to
- 21 the land use of this area and open space policies. Policy 6.27 is
- 22 specific to the study of the alignment of Paseo del Volcan; the
- 23 Master Plan shows the alignment west of the airport that was
- 24 approved by the New Mexico State Highway and Transportation
- 25 Department (NMSHTD) in 2001.
- 26 (H) *Double Eagle II Airport* is currently served with the limited access
- 27 Paseo del Volcan road that provides adequate access for the
- 28 foreseeable future. The new Paseo del Volcan, which will be
- 29 aligned to the west of the airport, is part of the planned loop road
- 30 connecting Interstate 40 to Paseo del Norte. The existing Paseo
- 31 del Volcan will remain dedicated to serving the airport.
- 32 (I) With the recent annexation of the 899 acre portion of *Double*
- 33 *Eagle II* airport, the water and sewer lines dedicated to *Double*

- 1 *Eagle II* airport were approved by the City Council (August 19,
- 2 2002) as well as the provision to fund them.
- 3 (J) The Master Plan for *Double Eagle II Airport* sets the stage for
- 4 future development projects within the plan's boundaries. The
- 5 *Double Eagle II Airport* is zoned SU-1, giving the Environmental
- 6 Planning Commission site development plan authority for future
- 7 projects.
- 8 (K) The Federal Highway Administration on March 2002, approved
- 9 the selection of the western A-1 alignment (bordering the west
- 10 edge of the Double Eagle II Airport) as the preferred location for
- 11 the future Paseo del Volcan freeway. With this decision,
- 12 however, there remains the need to define the remainder of the
- 13 future major street system in the vicinity of the Double Eagle II
- 14 Airport. Most critical at this time is the need for a continuous,
- 15 multi-lane public access across the airport site on the east side
- 16 of the runways and support facilities. Without this additional
- 17 north-south crossing, the Double Eagle II Airport and Petroglyph
- 18 National Monument will present a seven mile wide barrier to the
- 19 traveling public.
- 20 (L) The Double Eagle II Airport Master Plan document and
- 21 supporting figures be modified to show a future major arterial
- 22 alignment on the east side of the airport property (east of the
- 23 runways and support facilities), to provide a continuous north-
- 24 south public access through the airport site.
- 25 (M) Traffic Impact Study required as part of Landside development
- 26 (future).
- 27 (N) The Developer is responsible for permanent improvements to the
- 28 transportation facilities adjacent to the Airport when Landside
- 29 site development occurs. Those improvements will include any
- 30 additional right-of-way requirements, paving, curb and gutter and
- 31 sidewalk that have not already been provided for.
- 32 (O) The Double Eagle II Airport Master Plan document and
- 33 supporting figures need to be modified to show a future major

arterial alignment on the east side of the airport property (east of the runways and support facilities), to provide a continuous north-south public access through the airport site.

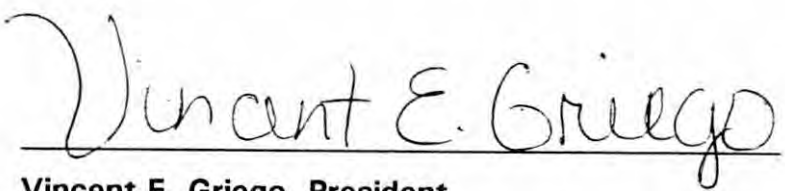
Section 3. SEVERABILITY CLAUSE. If any section, paragraph, sentence, clause, word or phrase of this resolution is for any reason held to be invalid or unenforceable by any court of competent jurisdiction, such decision shall not affect the validity of the remaining provisions of this resolution. The Council hereby declares that it would have passed this resolution and each section, paragraph, sentence, clause, word or phrase thereof irrespective of any provisions being declared unconstitutional or otherwise invalid.

[+Bracketed/Underscored Material+] - New
[-Bracketed/Strikethrough Material-] - Deletion

1 PASSED AND ADOPTED THIS 10TH DAY OF FEBRUARY, 2003
2 BY A VOTE OF: 7 FOR 0 AGAINST.

4 Yes: 7

5 Excused: E. Griego, S. Mayer

8
9
10 

11 Vincent E. Griego, President
12 City Council

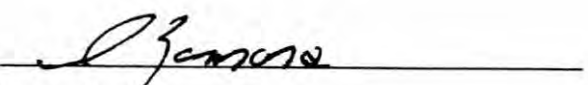
15
16 APPROVED THIS 26 DAY OF February, 2003

18 Bill No. R-02-170

19
20
21 

22 Martin Chávez, Mayor
23 City of Albuquerque

24 ATTEST:

25
26 

27 Acting City Clerk

[+Bracketed Material+] - New
[-Bracketed Material-] - Deletion

R-2019-065

CITY OF ALBUQUERQUE
CITY COUNCIL

INTEROFFICE MEMORANDUM

TO: Timothy M. Keller, Mayor

FROM: Stephanie M. Yara, Director of Council Services *SY 9/16/19*

SUBJECT: Transmittal of Legislation

Transmitted herewith is Bill No. R-19-169 Adopting The Double Eagle II Airport Master Plan, Project Number 1001656, 18EPC-40028, As An Official City Document (Sanchez and Borrego, by request), which was passed at the Council meeting of September 4, 2019, by a vote of 9 FOR AND 0 AGAINST.

In accordance with the provisions of the City Charter, your action is respectfully requested.

SY:dm
Attachment
9/9/2019

CITY of ALBUQUERQUE

TWENTY THIRD COUNCIL

COUNCIL BILL NO. R-19-169 ENACTMENT NO. R-2019-065

SPONSORED BY: Ken Sanchez, Cynthia D. Borrego, by request

1 RESOLUTION

2 ADOPTING THE DOUBLE EAGLE II AIRPORT MASTER PLAN, PROJECT
3 NUMBER 1001656, 18EPC-40028, AS AN OFFICIAL CITY DOCUMENT.

4 WHEREAS, the Double Eagle II Airport is one of two airports under the
5 City's jurisdiction; and

6 WHEREAS, the Double Eagle II Airport is classified as a reliever airport
7 for the Albuquerque International Sunport; and

8 WHEREAS, Double Eagle II Airport is classified as a Regional General
9 Aviation Airport, the purpose of which is primarily to serve general aviation
10 activity, with a focus on business activity; and

11 WHEREAS, the Comprehensive Plan designates the Double Eagle II
12 Airport as a Developing Employment Center; and

13 WHEREAS, air travel demand and general aviation use has increased
14 steadily and this trend is expected to continue into the future; and

15 WHEREAS, improvements made to airport facilities are instrumental to
16 the increase in demand for airside and landside operations.

17 BE IT RESOLVED BY THE COUNCIL, THE GOVERNING BODY OF THE CITY
18 OF ALBUQUERQUE:

19 Section 1. The Double Eagle II Airport Master Plan is adopted by the City
20 of Albuquerque.

21 Section 2. FINDINGS ACCEPTED. The City Council adopts the following
22 findings recommended by the Environmental Planning Commission (EPC):

1 (A) The request is for a master plan for the Double Eagle II (DEII)
2 Airport, and approximately (≈) 4,500 acre site located on Atrisco Vista Blvd.,
3 between Atrisco Vista Blvd. and Shooting Range Park, bounded by the
4 Petroglyph National Monument, the City of Rio Rancho, and unincorporated
5 Bernalillo County land (the “subject site”). The subject site is twenty miles
6 north by northwest of Downtown Albuquerque.

7 (B) The Double Eagle II Airport Master Plan (2018, the “DEII
8 Master Plan”) would supersede the existing Airport Master Plan: Double
9 Eagle II Airport (2002).

10 (C) The 2018 DEII Master Plan contains standard elements such
11 as a facilities inventory, a needs assessment, a forecast, development
12 alternatives, and a Capital Implementation Plan (CIP) with a 20 year horizon.

13 (D) The majority of the subject site (approximately three-
14 quarters) is located in an area that the 2017 Albuquerque/Bernalillo County
15 Comprehensive Plan has designated an Area of Change. The exception is
16 the airfield areas, which are an Area of Consistency and account for
17 approximately one-quarter of the subject site.

18 (E) The Albuquerque/Bernalillo County Comprehensive Plan and
19 the City of Albuquerque Integrated Development Ordinance (IDO) are
20 incorporated herein by reference and made part of the record for all
21 purposes.

22 (F) The request furthers the following Land Use Goal and policy
23 (Chapter 5):

24 (i) Goal 5.3- Efficient Development Patterns: Promote
25 development patterns that maximize the utility of existing infrastructure and
26 public facilities and the efficient use of land to support the public good.

27 The DEII Airport Master Plan (2018) addresses landside
28 facilities that support aviation functions, such as the administration
29 buildings, fuel farm, and hangars. These maximize the utility of existing

1 infrastructure at the airport, which was installed in 2003 (see History section
2 of this report), and contributes to the efficient use of land to generally
3 support the public good.

4 (ii) Policy 5.1.5- Employment Centers: Create Centers that
5 prioritize employment opportunities and foster synergy among businesses.

6 The DEII Airport is designated as a Developing
7 Employment Center by the Comprehensive Plan. It is envisioned to continue
8 to be used as a commercial airport, a heliport, and to expand its usage as a
9 training center, which would prioritize employment opportunities and foster
10 synergy among aviation and aviation support businesses.

11 (G) The request furthers the following Comprehensive Plan Goal
12 and policies regarding City development areas:

13 (i) Goal 5.6- City Development Areas: Encourage and direct
14 growth to Areas of Change where it is expected and desired and ensure that
15 development in and near Areas of Consistency reinforces the character and
16 intensity of the surrounding area.

17 In general, airport and non-aviation growth would be
18 directed to Areas of Change where it is expected and desired, and the Areas
19 of Consistency would remain constant because growth is not desired there.

20 (ii) Policy 5.6.2- Areas of Change: Direct growth and more
21 intense development to Centers, Corridors, industrial and business parks,
22 and Metropolitan Redevelopment Areas where change is encouraged.

23 The subject site is primarily designated as an Area of
24 Change. The majority of the subject site is undeveloped and is envisioned
25 to be used for airport and airport-related expansion and businesses in the
26 future.

27 (iii) Policy 5.6.3- Areas of Consistency: Protect and enhance
28 the character of existing single-family neighborhoods, areas outside of
29 Centers and Corridors, parks, and Major Public Open Space.

1 The airfield areas, which are a relatively small portion
2 of the subject site, are not envisioned to change much except for updates
3 to meet Federal safety requirements.

4 (H) The request furthers the following Comprehensive Plan
5 Goal and policies regarding transportation (Chapter 6):

6 (i) Goal 6.6- Economy: Invest in a transportation system that
7 stimulates and supports job creation and business development and
8 improves the movement of people, goods, and services.

9 As discussed in the Master Plan, air travel is part of a
10 larger transportation system that facilitates the movement of people, goods,
11 and services. The DEll Airport has the potential to stimulate and support job
12 creation because expansion of the landside facilities would provide jobs
13 that support aviation functions (such as administration and facilities used
14 to train pilots). It's possible that some of the property in the subject site
15 could develop with office, commercial, and industrial uses as is the case
16 with the Sunport, which would support business development and job
17 creation.

18 (ii) Policy 6.2.10- Aviation: Provide adequate accommodations
19 for domestic travel, shipping, and military purposes.

20 A major purpose of the DEll Master Plan is to ensure
21 that future demand for aviation (airside) and aviation support (landside)
22 facilities can be accommodated. The master plan evaluates existing
23 facilities, uses forecasts to determine future needs, and proposes
24 strategies to ensure that these functions can be adequately accommodated.

25 (I) The request furthers Goal 12.1 Infrastructure: Plan,
26 coordinate, and provide for efficient, equitable, and environmentally sound
27 infrastructure to support existing communities and the Comp Plan's vision
28 for future growth (Chapter 12- Infrastructure, Community Facilities, and
29 Services).

1 The 2018 DEII Master Plan focuses on airport infrastructure
2 (runways, terminals, support services) needed to support existing and
3 anticipated aviation and aviation related needs. This supports the
4 community because, as a general aviation airport, DEII accommodates
5 smaller aircraft (planes and helicopters) used by businesses and
6 individuals. Should additional infrastructure be needed in the future, it
7 would be planned and coordinated with the City's Capital Improvement
8 (CIP) program and with the Water Utility Authority.

9 (J) The applicant and Planning Department Staff will coordinate
10 in the future to discuss proposed revisions to the Comprehensive Plan to
11 be consistent with the Updated DEI II Master Plan.

12 (K) Minor conditions and amendments are recommended to
13 provide clarification.

14 (L) The Westside Coalition is the affected neighborhood
15 organization, which was notified as required (see attachments). Property
16 owners within 100 feet of the subject site were also notified as required. A
17 facilitated meeting was neither requested nor held. Staff has not been
18 contacted or received any written comments. There is no known opposition.

19 Section 3. SEVERABILITY CLAUSE. If any section, paragraph, sentence,
20 clause, word or phrase of this resolution is for any reason held to be invalid or
21 unenforceable by any court of competent jurisdiction, such a decision shall not
22 affect the validity of the remaining provisions of this resolution. The Council
23 hereby declares that it would have passed this resolution and each section,
24 paragraph, sentence, clause, word or phrase thereof irrespective of any
25 provision being declared unconstitutional or otherwise invalid.

26
27
28
29 x:\cl\share\cl-staff_legislative staff\legislation\23 council\r-169final.docx

1 PASSED AND ADOPTED THIS 4th DAY OF September, 2019
2 BY A VOTE OF: 9 FOR 0 AGAINST.

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8 Klarissa J. Peña, President
9 City Council

10
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12 APPROVED THIS 23 DAY OF September, 2019

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15
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17 Bill No. R-19-169

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19
20
21 Timothy M. Keller, Mayor
22 City of Albuquerque

23
24
25
26 ATTEST:

27
28
29 Katy Duhigg, City Clerk

PLANNING DEPARTMENT
URBAN DESIGN & DEVELOPMENT DIVISION
600 2nd Street NW, 3rd Floor, Albuquerque, NM 87102
P.O. Box 1293, Albuquerque, NM 87103
Office (505) 924-3860 Fax (505) 924-3339



OFFICIAL NOTIFICATION OF DECISION

May 18, 2023

Gravity Pad Towers, LLC
440 Alamos Road
Corrales, NM 87048

Project # PR-2018-001577
SI-2023-00537 - EPC - Major Amendment

LEGAL DESCRIPTION:

ABQ Land Use Consulting LLC c/o Carl Garcia, agent for Gravity Pad Towers LLC, requests a Site Plan- Major Amendment for all or a portion of Tracts A, C, D-1-A2, E, F, K, N, S-1-A, S-2, Lands of Double Eagle II Airport, located at 7401 Atrisco Vista Blvd. NW, at the intersection of Atrisco Vista Blvd. NW and Shooting Range Access Rd., zoned NR-SU, approximately 4500 acres (E-5)
Staff Planner: Megan Jones

On May 18, 2023 the Environmental Planning Commission (EPC) voted to APPROVE Project # PR-2018-001577, SI-2023-00537 - Site Plan- Major Amendment, based on the following Findings and subject to the following Conditions of approval:

FINDINGS - Double Eagle II Text Amendment

1. The request is for text amendments to the Double Eagle II (DEII) Airport Master Plan. DEII is located on an approximately (\approx) 4,500-acre site at 7401 Atrisco Vista Blvd., between Atrisco Vista Blvd. and Shooting Range Access Rd., bounded by the Petroglyph National Monument, the City of Rio Rancho, and unincorporated Bernalillo County land (the "subject site"). The subject site is twenty miles north by northwest of Downtown Albuquerque.
2. The DEII Airport Master Plan is a Rank 3 Plan and contains standard elements such as a facilities inventory, a needs assessment, a forecast, development alternatives, and a Capital Implementation Plan (CIP) with a 20-year horizon. A Rank 3 Plan is developed and approved by an implementing City department to guide the development maintenance, and operation of individual public resources or facilities.
3. The DEII Airport is zoned Non-residential – Sensitive use (NR-SU). Prior to the effective date of the IDO in May 2018, the subject site was zoned SU-1 for Airport and Related Facilities.
4. The proposed text amendments would add Wireless Telecommunication Facilities (WTFs) as a Facility Requirement and land use in the Master Plan and Airport Layout. WTFs are a permissive accessory use in the NR-SU zone district.

5. Approval of this request would facilitate development of a future WTF, which is subject to a separate administrative review and approval process.
6. The request (the text amendments) constitutes a Major Amendment to a prior approval (the DEII Master Plan). Pursuant to IDO 14-16-6-4(Y), Amendment of Prior Approvals, if a request exceeds the thresholds for Minor Amendments in 14-16-6-4(Y)(2)(a), then it is a Major Amendment.

In this case, the request would affect property in the Airport Protection Overlay Zone and the existing Employment Center District [6-4(Y)(2)(a)(11)], and approves a land use that was not originally authorized by the original approval on the subject site [6-4(Y)(2)(a)(12)]. Therefore, it is a Major Amendment.

7. The EPC was the original reviewing body of the controlling 2018 Master Plan and the City Council was the approving body. Pursuant to the Federal Aviation Administration (FAA), Master Plans are required to be approved through a public hearing process by the governmental agency or authority that owns or operates the airport (See FAA Land Use Compatibility & Airports). Pursuant to IDO 14-16-6-3(C), implementing City departments may choose to have Rank 3 Plans reviewed by the EPC and/or accepted by the City Council when additional input is desired.

In this case, the Aviation Dept. is requesting the EPC as the review and approval body for the proposed amendments.

8. The subject site is located in the developing DEII Airport Employment Center and along the Arroyo Vista Commuter Corridor as designated by the Comp Plan. The majority of the subject site (approximately three-quarters) is located in an Area of Change. The exception is the airfield areas, which are an Area of Consistency and account for approximately one-quarter of the subject site.
9. The Albuquerque/Bernalillo County Comprehensive Plan and the City of Albuquerque Integrated Development Ordinance (IDO) are incorporated herein by reference and made part of the record for all purposes.
10. The request is generally consistent with the following Comprehensive Plan Goals and policies regarding Development Patterns from Chapter 5- Land Use:
 - A. Goal 5.3- Efficient Development Patterns: Promote development patterns that maximize the utility of existing infrastructure and public facilities and the efficient use of land to support the public good.

The DEII Airport Master Plan addresses landside facilities that support aviation functions, such as the administration buildings, fuel farm, and hangars. The request to add the WTF land use as a landside facility could maximize the utility of existing infrastructure at the airport, and contribute to the efficient use of land to generally support the public good.

- B. Policy 5.3.1 - Infill Development: Support additional growth in areas with existing infrastructure and public facilities.

The request would facilitate development of a future WTF, which would support growth at the DEII airport which has existing infrastructure to support the use. DEII is a public facility that may benefit from the WTF land use for the needs of the Airport and surrounding area.

- C. Policy 5.3.7 - Locally Unwanted Land Uses: Ensure that land uses that are objectionable to immediate neighbors but may be useful to society are located carefully and equitably to ensure that social assets are distributed evenly and social responsibilities are borne fairly across the Albuquerque area.

The request would help ensure that the WTF land use, an objectionable use to immediate neighbors, is located at the DEII Airport. DEII is located away from immediate development and is surrounded by undeveloped land, aside from the ABQ Shooting Range. Additionally, the request would facilitate development of a WTF on the West side of Albuquerque, which is useful to society.

11. The request is consistent with the following Comprehensive Plan Goal and policies regarding City development areas from Chapter 5:

- A. Goal 5.6- City Development Areas: Encourage and direct growth to Areas of Change where it is expected and desired and ensure that development in and near Areas of Consistency reinforces the character and intensity of the surrounding area.

The request to add WTFs as a land use and Facility Requirement in the Master plan would facilitate development of a WTF in the proposed area of change at the DEII Airport. The Airfield areas are in an Area of Consistency, which is not envisioned to change.

- B. Policy 5.6.2- Areas of Change: Direct growth and more intense development to Centers, Corridors, industrial and business parks, and Metropolitan Redevelopment Areas where change is encouraged.

Policy 5.6.3- Areas of Consistency: Protect and enhance the character of existing single-family neighborhoods, areas outside of Centers and Corridors, parks, and Major Public Open Space.

The subject site is primarily designated as an Area of Change. The majority of the subject site is undeveloped and is to be used for airport and airport-related expansion and businesses in the future and is designated as an Area of Change. The airfield areas, which are a relatively small portion of the subject site, are not envisioned to change much except for updates to meet Federal safety requirements and is designated as an Area of Consistency. In general, airport expansion and non-aviation growth would be directed to Areas of Change where it is expected and desired, and the Areas of Consistency would remain constant because growth is not desired there. The request would add the WTF land use as a facility requirement, in an Area of Change.

12. The request is generally consistent with Goal 12.1 Infrastructure: Plan, coordinate, and provide for efficient, equitable, and environmentally sound infrastructure to support existing communities and the Comp Plan's vision for future growth (Chapter 12-Infrastructure, Community Facilities, and Services).

The request to add a WTF land use as a landside requirement in the DEII Master Plan would provide infrastructure that would support the existing DEII Airport and surrounding area because the Master plan does not currently include WTFs as a land use. The purpose of the request would approve the use and facilitate development of a future WTF, which is subject to a separate approval process which would ensure efficient, equitable and environmentally sound infrastructure. This request generally supports the Comp Plans vision for growth.

13. The request for a Major Amendment meets the Site Plan-EPC Review & Decision Criteria in IDO Section 14-16-6-6(J)(3):

- A. 6-6(J)(3)(a) The Site Plan is consistent with the ABC Comp Plan, as amended.

As demonstrated by the policy analysis (see above), the request is generally consistent with applicable Comprehensive Plan Goals and policies.

- B. 6-6(J)(3)(b) The Site Plan is consistent with any applicable terms and conditions in any previously approved NR-SU or PD zoning covering the property and any related development agreements and/or regulations.

The subject site is zoned NR-SU and is within the Double Eagle II Airport Master Plan. The Master Plan is silent on development standards and regulations for Wireless Telecommunication Facilities (WTF). Therefore, the request to allow a WTF land use in the DEII Master Plan shall be consistent with all applicable regulations of the NR-SU zone district, IDO Wireless Telecommunication Facility Regulations, and FAA regulations. The request would not change any existing development agreements or regulations associate with the DEII Master Plan.

- C. 6-6(J)(3)(c) The Site Plan complies with all applicable provisions of this IDO, the DPM, other adopted City regulations, and any terms and conditions specifically applied to development of the property in a prior permit or approval affecting the property.

The proposed text amendment would allow the WTF land use as a facility requirement in the DEII Master Plan, which is required to comply with the NR-SU standards in the IDO. The request would facilitate future development of a WTF which would be subject to a separate application process and all applicable provisions of the IDO, DPM, and all FAA and WTF regulations. Conditions of approval are recommended to ensure compliance with NR-SU standards, DEII development standards, and to create consistency within the DEII Master Plan exhibits/maps.

- D. 6-6(J)(3)(d) The City's existing infrastructure and public improvements, including but not limited to its street, trail, drainage, and sidewalk systems, have adequate capacity to serve the proposed development, and any burdens on those systems have been mitigated to the extent practicable.

The requested amendments would not impact existing infrastructure and public improvements. The applicant has agreed to coordinate with utility providers to mitigate any potential burdens on the power grid and will be responsible for the construction of the proposed access road for the proposed future WTF made possible by this request, and will ensure that all DPM and IDO guidelines are met.

- E. 6-6(J)(3)(e) The application mitigates any significant adverse impacts on the surrounding area to the maximum extent practicable.

The proposed text amendments to allow a WTF land use as a landside facility at DEII Airport would not significantly impact the subject site or surrounding area. As the applicant has demonstrated, the future WTF made possible by this request would be required to meet all WTF

and FAA regulations, which will generally ensure provision of adequate infrastructure to mitigate site-specific impacts on the surrounding area.

- F. 6-6(J)(3)(f) If the subject property is within an approved Master Development Plan, the Site Plan meets any relevant standards in the Master Development Plan in addition to any standards applicable in the zone district the subject property is in.

Not applicable. The subject site is not a part of a Master Development Plan; it is within the DEII Master Plan which is a Rank 3 Plan.

- G. 6-6(J)(3)(g) If a cumulative impact analysis is required in the Railroad and Spur Area pursuant to Subsections 14-16-5-2(E) (Cumulative Impacts) and 14-16-6-4(H) (Cumulative Impacts Analysis Requirements), the Site Plan incorporates mitigation for all identified cumulative impacts. The proposed development will not create material adverse impacts on water quality or other land in the surrounding area through increases in traffic congestion, parking congestion, noise, vibration, light spillover, or other nuisances without sufficient mitigation or civic or environmental benefits that outweigh the expected impacts.

Not applicable. The Level A Plan area is not in a designated Railroad and Spur Area.

14. Upon approval of this request WTFs are an allowable use in the DEII Master plan and any future WTF applications would not be subject to another amendment of the DEII Master Plan.
15. Minor conditions and amendments are needed to provide clarification and create consistency with the DEII Master Plan moving forward.
16. There are no affected neighborhood organizations. Property owners within 100 feet of the subject site were notified as required. A facilitated meeting was neither requested nor held. Staff has not been contacted or received any written comments. There is no known opposition.

CONDITIONS OF APPROVAL

1. Upon approval of the Major Amendment and the subsequent approval of a Wireless Telecommunication Facility (WTF) at DEII Airport, the applicant shall update the Master Plan maps/exhibits with the final approved WTF location.
2. Map/Exhibits shall be updated with the approved, final WTF location within 6 months of the approval.

APPEAL: If you wish to appeal this decision, you must do so within 15 days of the EPC's decision or by **June 2, 2023**. The date of the EPC's decision is not included in the 15-day period for filing an appeal, and if the 15th day falls on a Saturday, Sunday or Holiday, the next working day is considered as the deadline for filing the appeal.

For more information regarding the appeal process, please refer to Section 14-16-6-4(V) of the Integrated Development Ordinance (IDO), Administration and Enforcement. A Non-Refundable filing fee will be calculated at the Land Development Coordination Counter and is required at the time the

appeal is filed. It is not possible to appeal an EPC Recommendation to the City Council since this is not a final decision.

You will receive notification if any person files an appeal. If there is no appeal, you can receive Building Permits at any time after the appeal deadline quoted above, provided all conditions imposed at the time of approval have been met. Successful applicants are reminded that other regulations of the IDO must be complied with, even after approval of the referenced application(s).

Sincerely,

Catalina Lehner

for Alan M. Varela,
Planning Director

AV/CL/MJ

cc: Gravity Pad Towers, LLC, sean@thefsg.com
ABQ Land Use Consulting LLC - Carl Garcia, carl@abqlanduse.com
Legal, dking@cabq.gov
EPC File

C) APPLICATION INFORMATION



Please check the appropriate box and refer to supplemental forms for submittal requirements. All fees must be paid at the time of application.

Administrative Decisions	Decisions Requiring a Public Meeting or Hearing	Policy Decisions
<input type="checkbox"/> Archaeological Certificate (Form P3)	<input checked="" type="checkbox"/> Site Plan – EPC including any Variances – EPC (Form P1)	<input type="checkbox"/> Adoption or Amendment of Comprehensive Plan or Facility Plan (Form Z)
<input type="checkbox"/> Historic Certificate of Appropriateness – Minor (Form L)	<input type="checkbox"/> Master Development Plan (Form P1)	<input type="checkbox"/> Adoption or Amendment of Historic Designation (Form L)
<input type="checkbox"/> Alternative Signage Plan (Form P3)	<input type="checkbox"/> Historic Certificate of Appropriateness – Major (Form L)	<input type="checkbox"/> Amendment of IDO Text (Form Z)
<input type="checkbox"/> Minor Amendment to Site Plan (Form P3)	<input type="checkbox"/> Demolition Outside of HPO (Form L)	<input type="checkbox"/> Annexation of Land (Form Z)
<input type="checkbox"/> WTF Approval (Form W1)	<input type="checkbox"/> Historic Design Standards and Guidelines (Form L)	<input type="checkbox"/> Amendment to Zoning Map – EPC (Form Z)
<input type="checkbox"/> Alternative Landscaping Plan (Form P3)	<input type="checkbox"/> Wireless Telecommunications Facility Waiver (Form W2)	<input type="checkbox"/> Amendment to Zoning Map – Council (Form Z)
		Appeals
		<input type="checkbox"/> Decision by EPC, DHO, LC, ZHE, or City Staff (Form A)

APPLICATION INFORMATION

Applicant: Manny Manriquez - City of Albuquerque Sunport		Phone: (505) 916-4341
Address: 2200 Sunport SE, Albuquerque, NM 87120		Email: mmanriquez@cabq.gov
City: Albuquerque	State: NM	Zip: 87120
Professional/Agent (if any): Will Gleason at Dekker Perich Sabatini		Phone: (505) 228-1493
Address: 7601 Jefferson St NE Suite 100		Email: WillG@dpsdesign.org
City: Albuquerque	State: NM	Zip: 87109
Proprietary Interest in Site:	List <u>all</u> owners:	

BRIEF DESCRIPTION OF REQUEST

SITE INFORMATION (Accuracy of the existing legal description is crucial! Attach a separate sheet if necessary.)

Lot or Tract No.:	Block:	Unit:
Subdivision/Addition:	MRGCD Map No.:	UPC Code:
Zone Atlas Page(s): C4/5/6, D4/5/6, E4/5/6, F4/5/6, G 4/5/6	Existing Zoning: NR-SU	Proposed Zoning: NR-SU
# of Existing Lots: 12	# of Proposed Lots: 12	Total Area of Site (acres): +/- 3,906.3013 AC

LOCATION OF PROPERTY BY STREETS

Site Address/Street: 7401 Paseo Del Volcan NW, Albuquerque, NM 87120	Between: Atrisco Vista Blvd	and: Double Eagle Road
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CASE HISTORY (List any current or prior project and case number(s) that may be relevant to your request.)

1001656, PR-2018-001577, SI-2024-00321

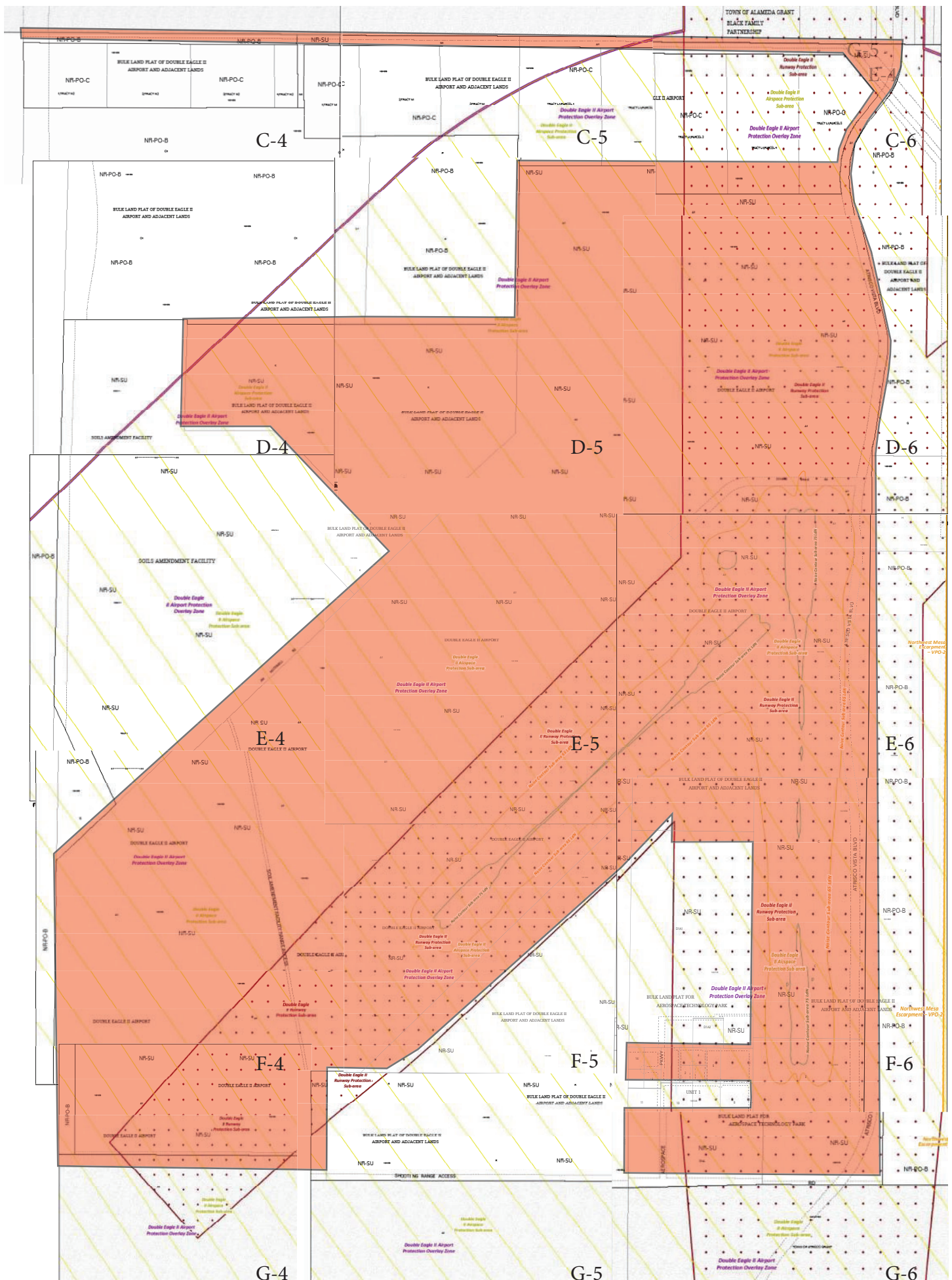
Signature: 	Date: July 3, 2024
Printed Name: Will Gleason	<input type="checkbox"/> Applicant or <input checked="" type="checkbox"/> Agent

FOR OFFICIAL USE ONLY

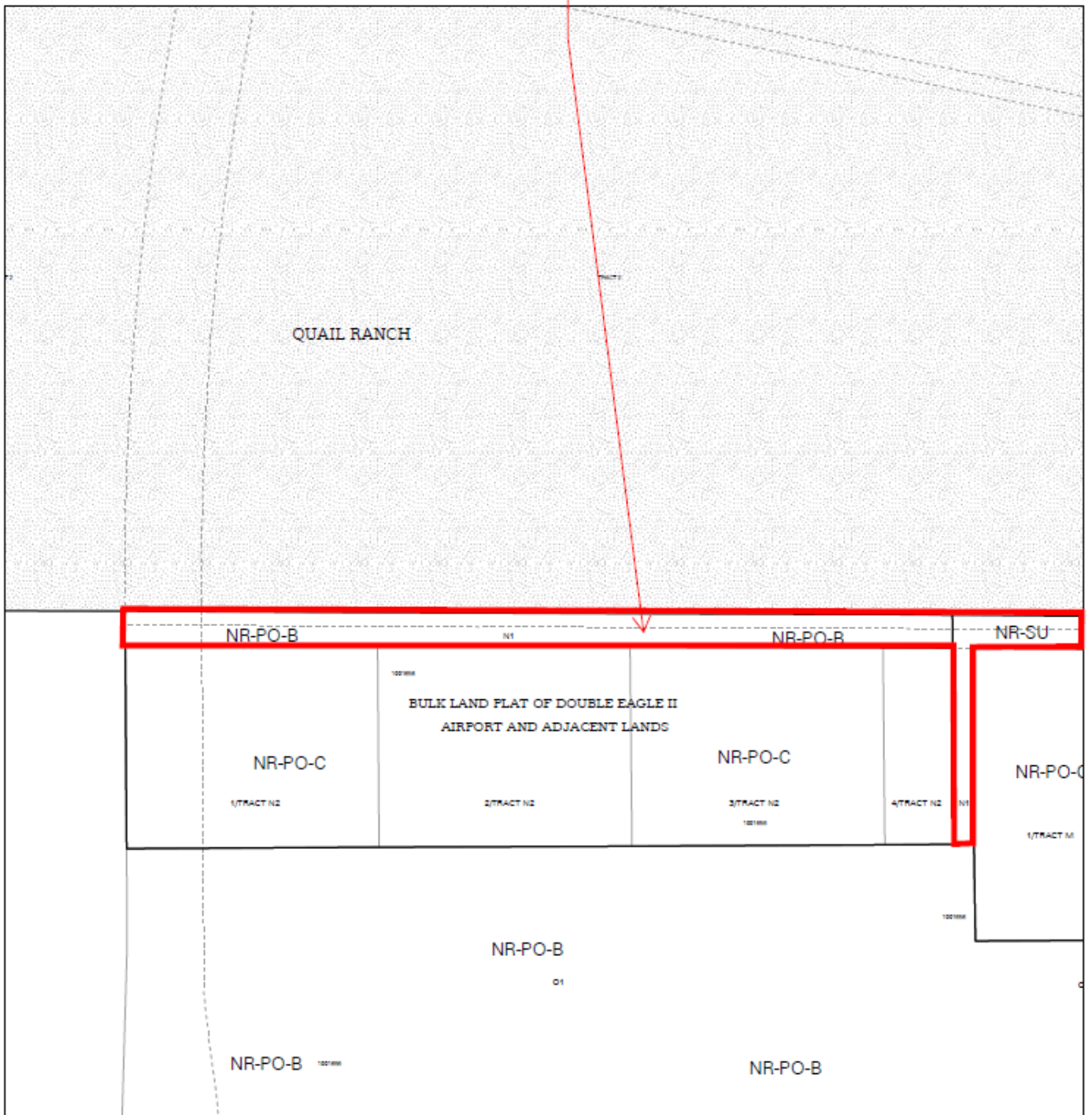
Case Numbers	Action	Fees	Case Numbers	Action	Fees

Meeting/Hearing Date:	Fee Total:
Staff Signature:	Date: Project #

Zoning Atlas Key Map



Subject Property



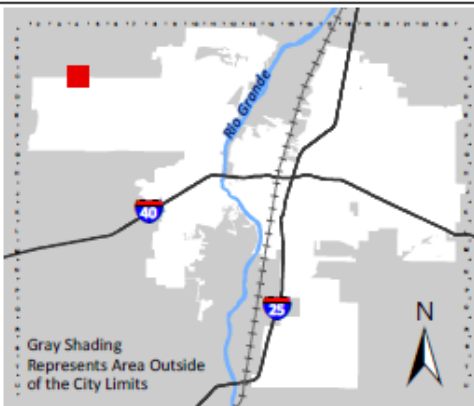
For more details about the Integrated Development Ordinance visit: <http://www.cabq.gov/planning/codes-policies-regulations/integrated-development-ordinance>

IDO Zone Atlas

May 2018











IDO Zoning information as of May 17, 2018
The Zone Districts and Overlay Zones
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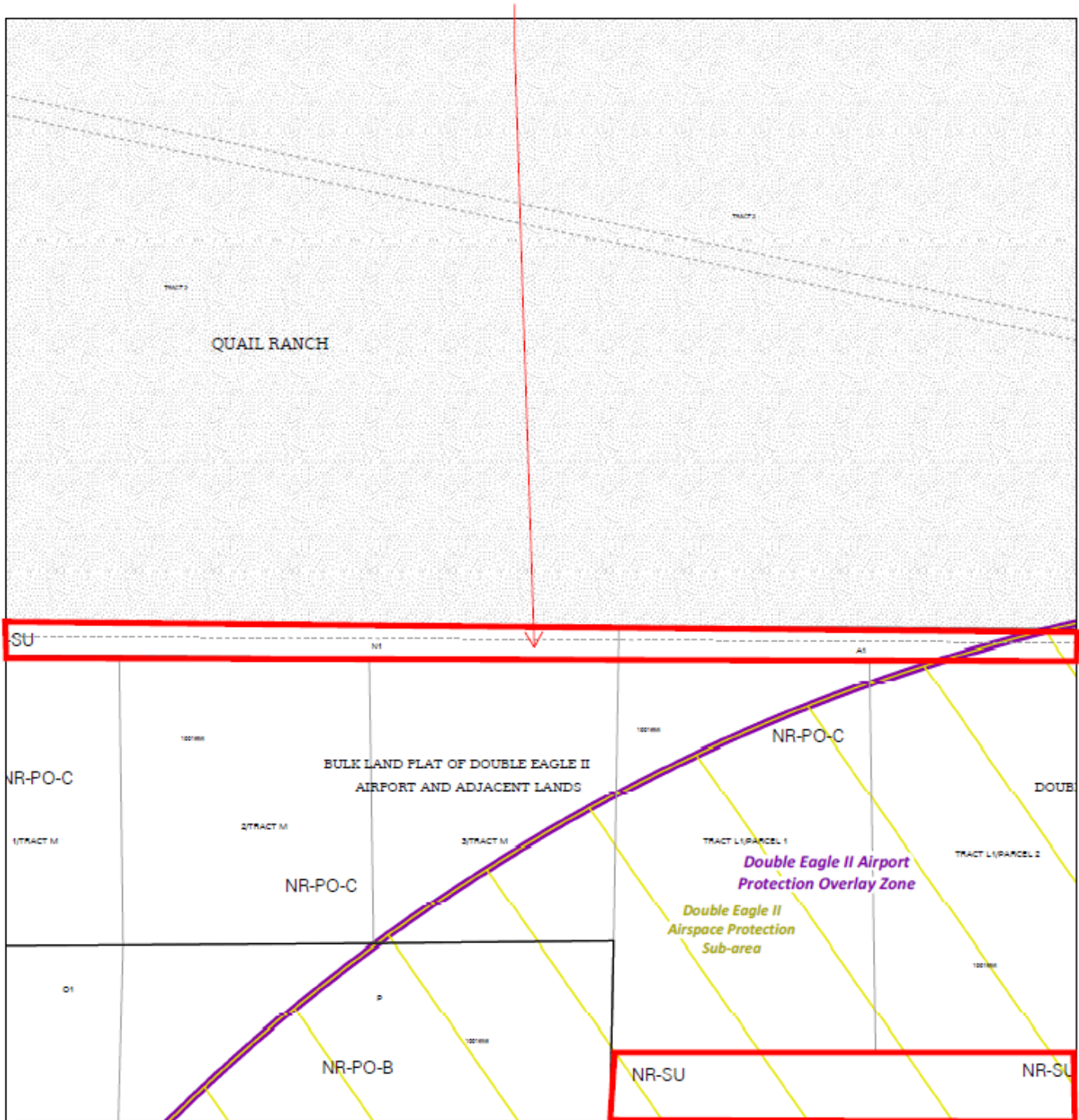
Zone Atlas Page:

C-04-Z

-  Easement
  Escarpment
-  Petroglyph National Monument
-  Areas Outside of City Limits
-  Airport Protection Overlay (APO) Zone
-  Character Protection Overlay (CPO) Zone
-  Historic Protection Overlay (HPO) Zone
-  View Protection Overlay (VPO) Zone

A horizontal number line is shown with tick marks at 0, 250, 500, and 1,000. The word "Feet" is written at the right end of the line. The line is divided into four equal segments by vertical lines at 250, 500, and 1,000.

Subject Property

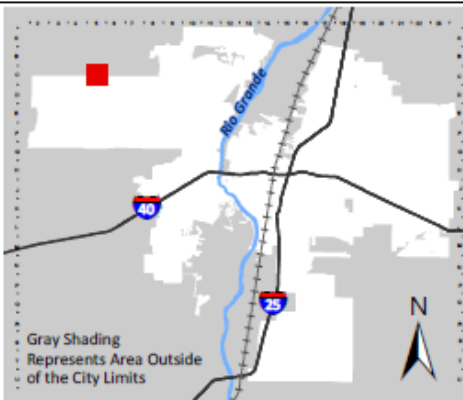


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IDO Zone Atlas May 2018



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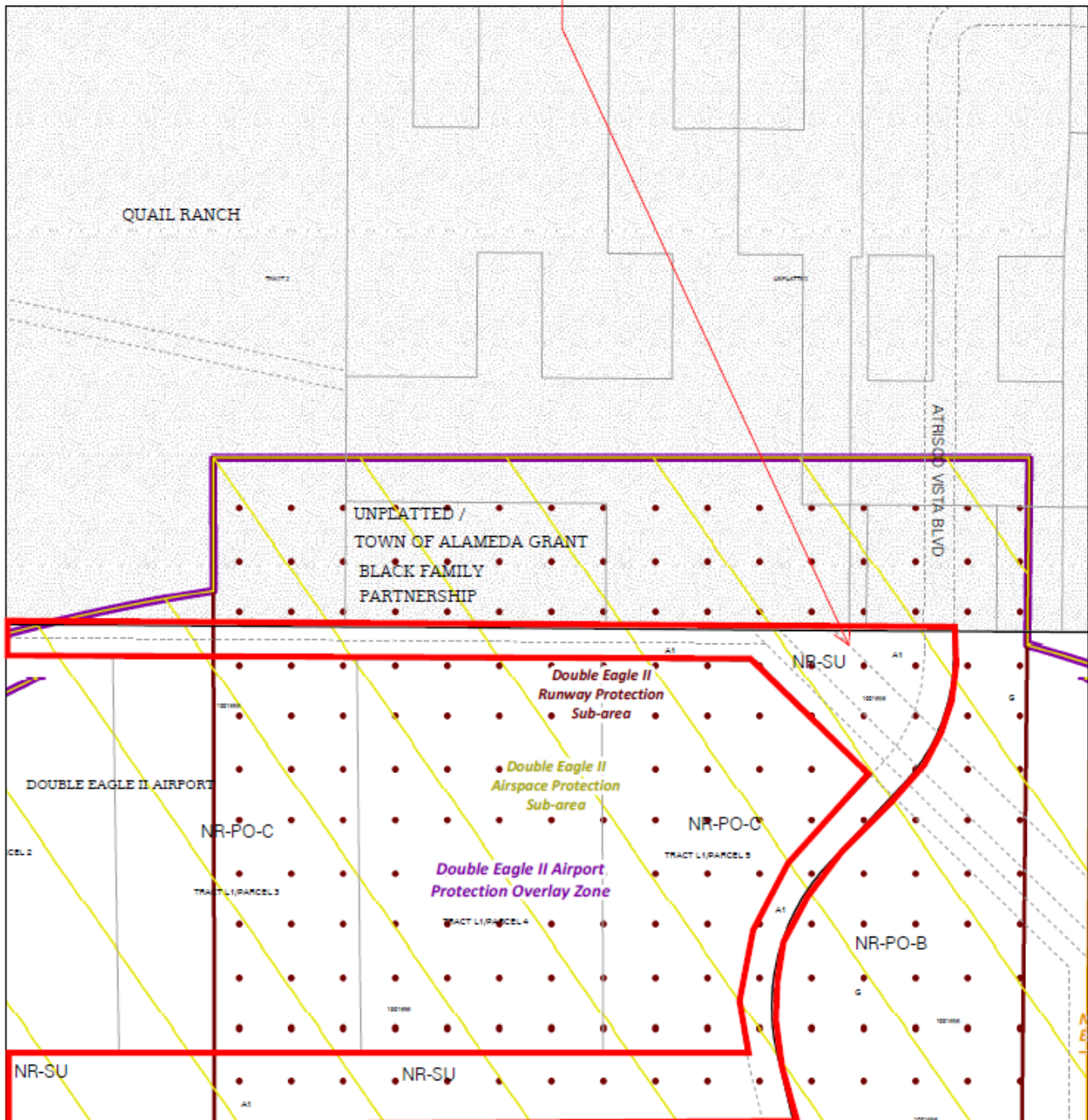
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C-05-Z

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0 250 500 1,000 Feet

Subject Property

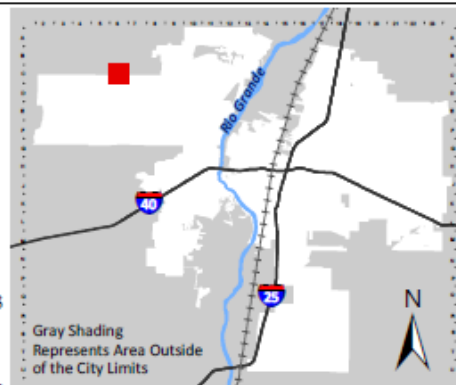


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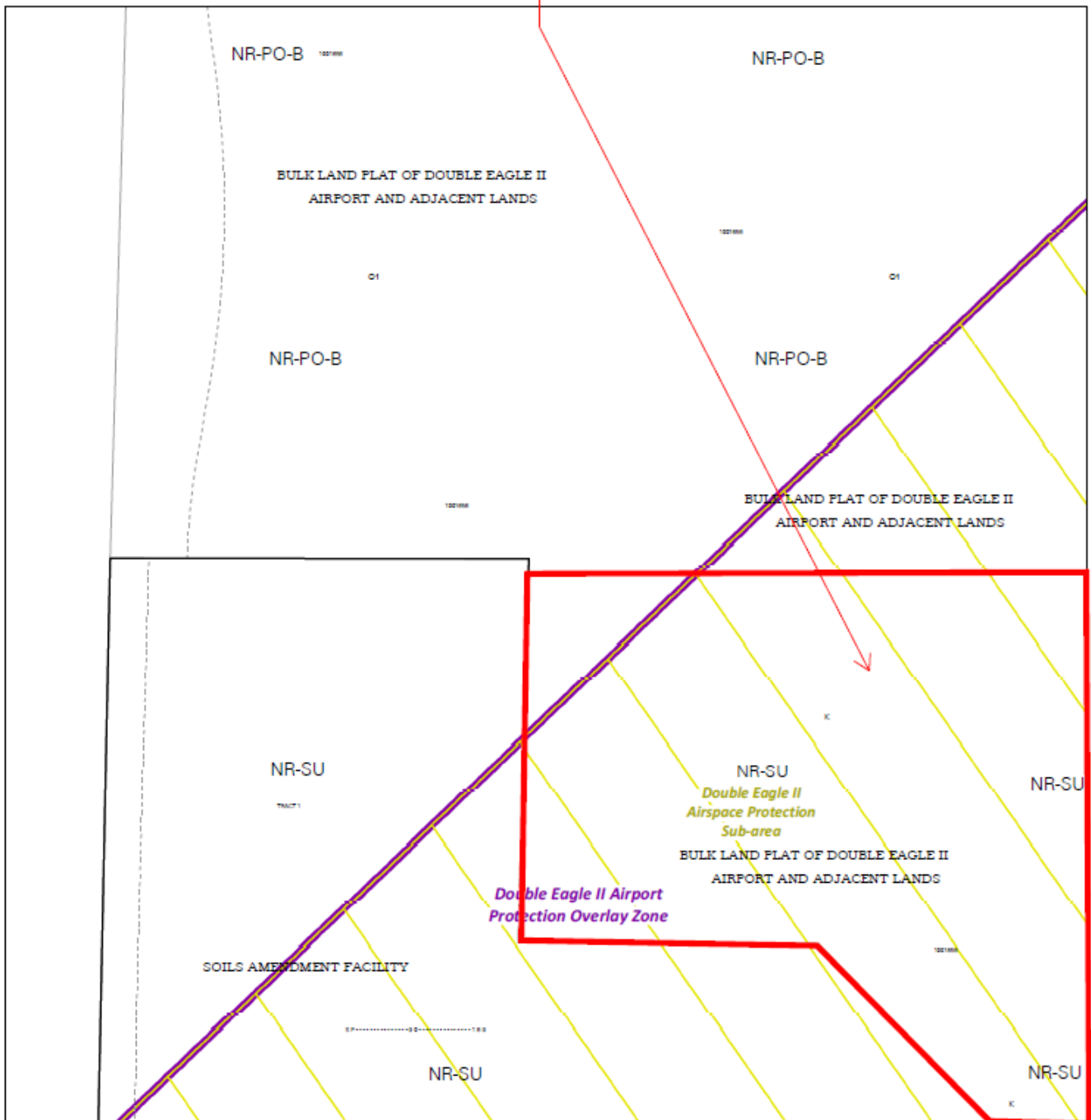


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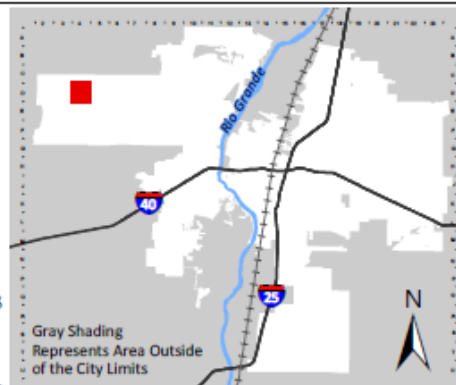


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IDO Zone Atlas May 2018



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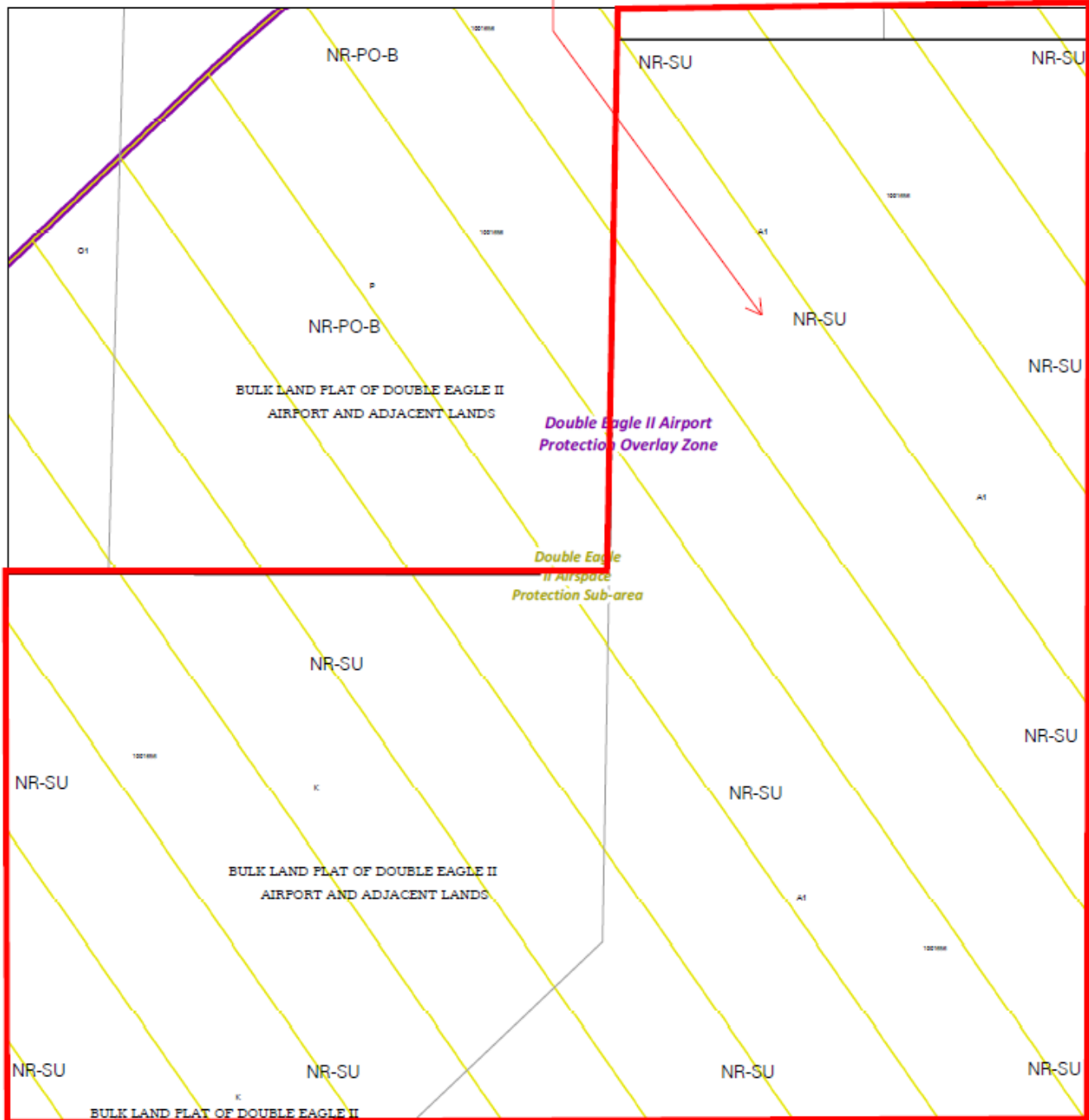


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D-04-Z

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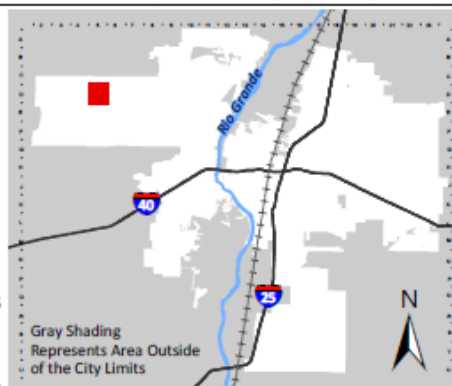
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IDO Zone Atlas May 2018



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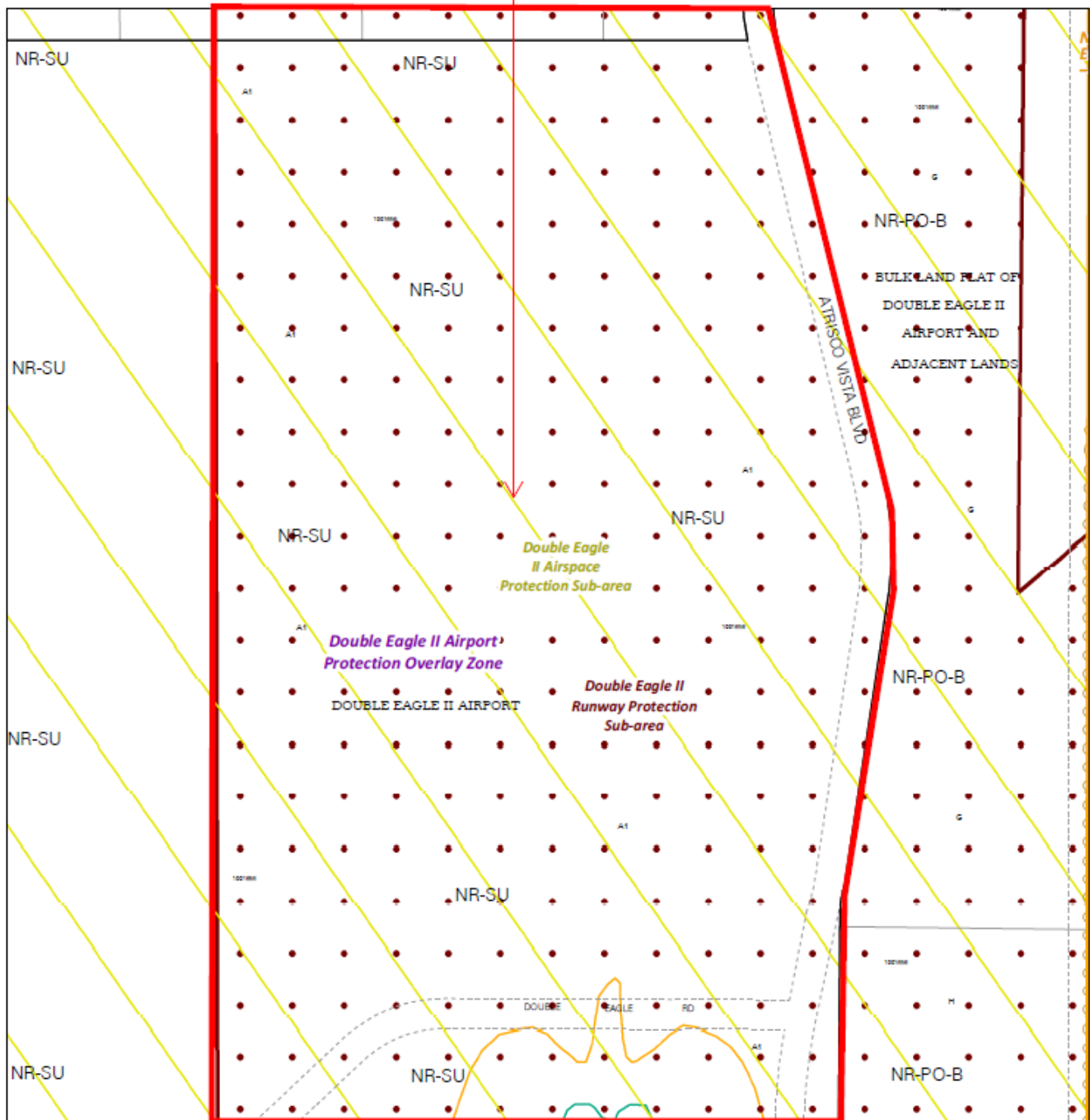


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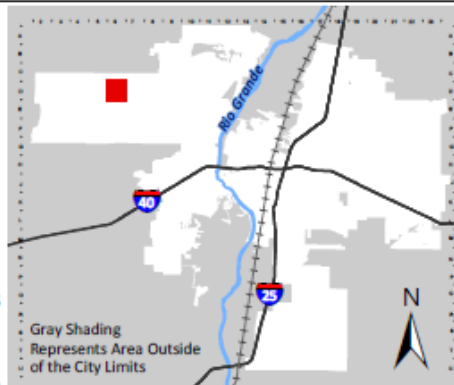


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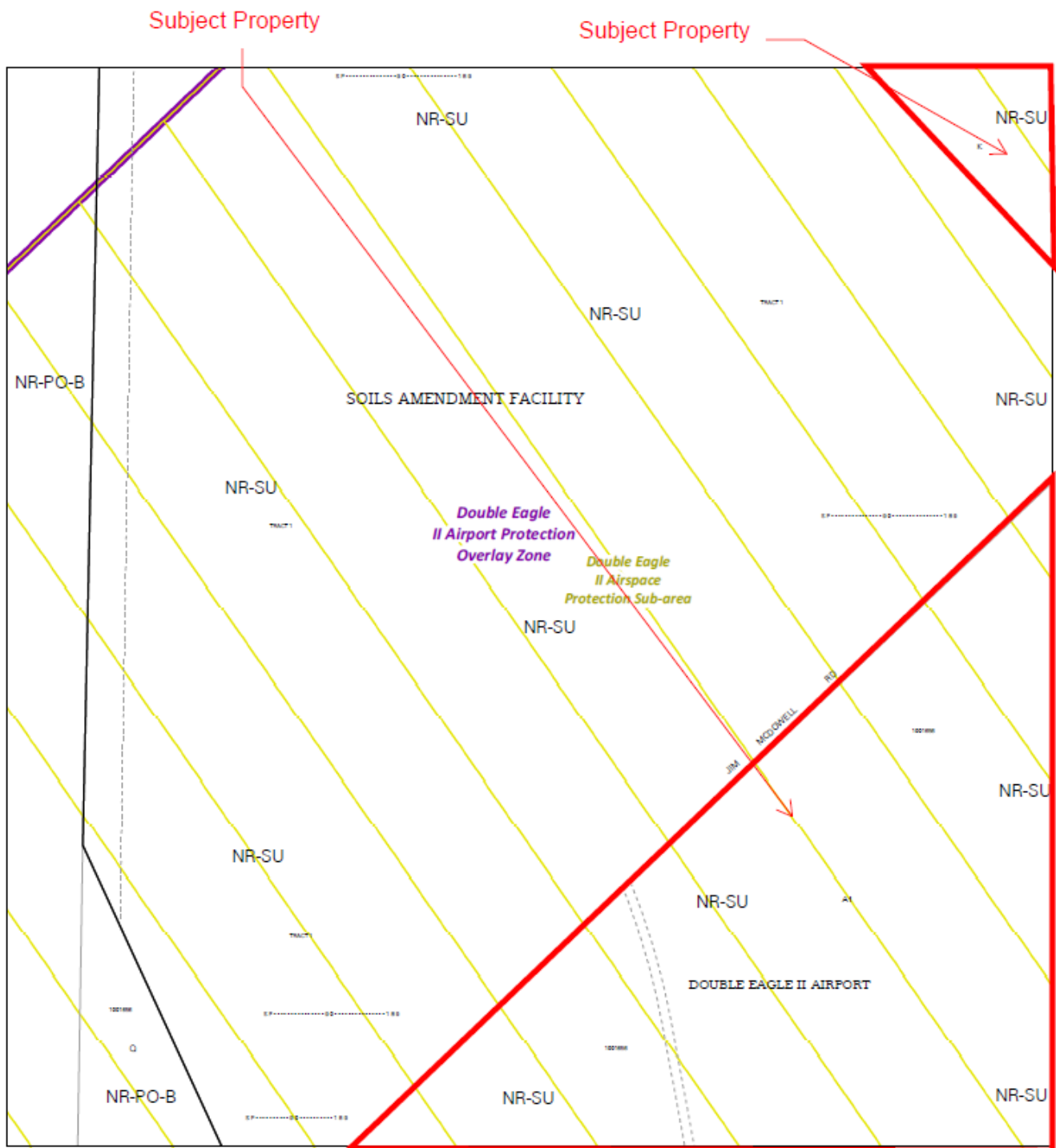


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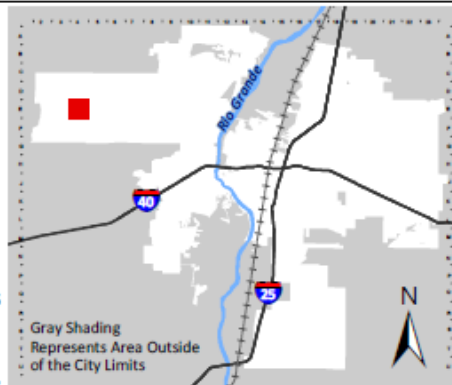


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IDO Zone Atlas May 2018



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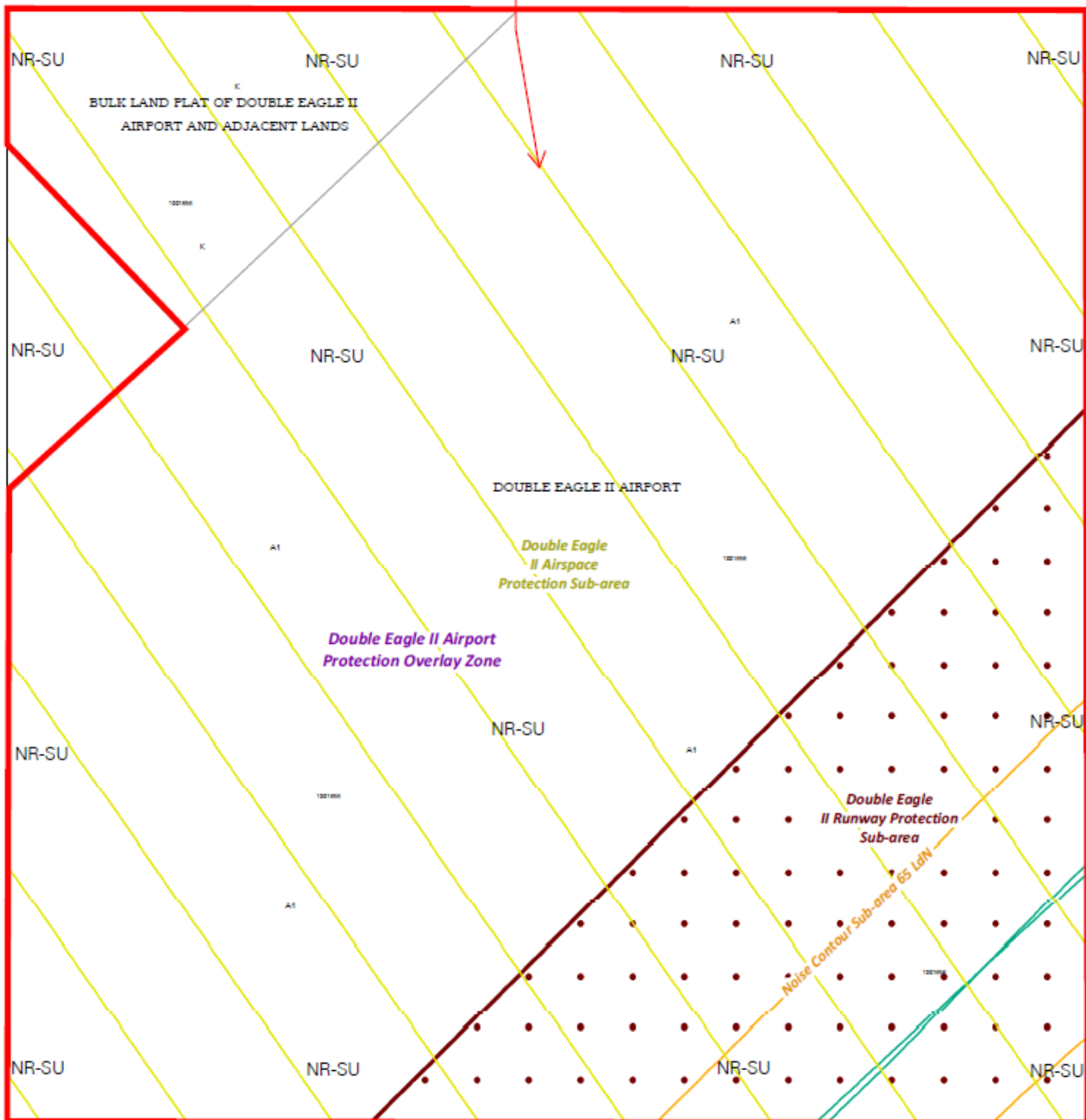


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E-04-Z

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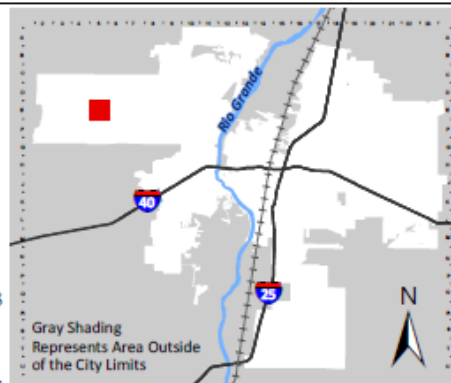
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IDO Zone Atlas May 2018



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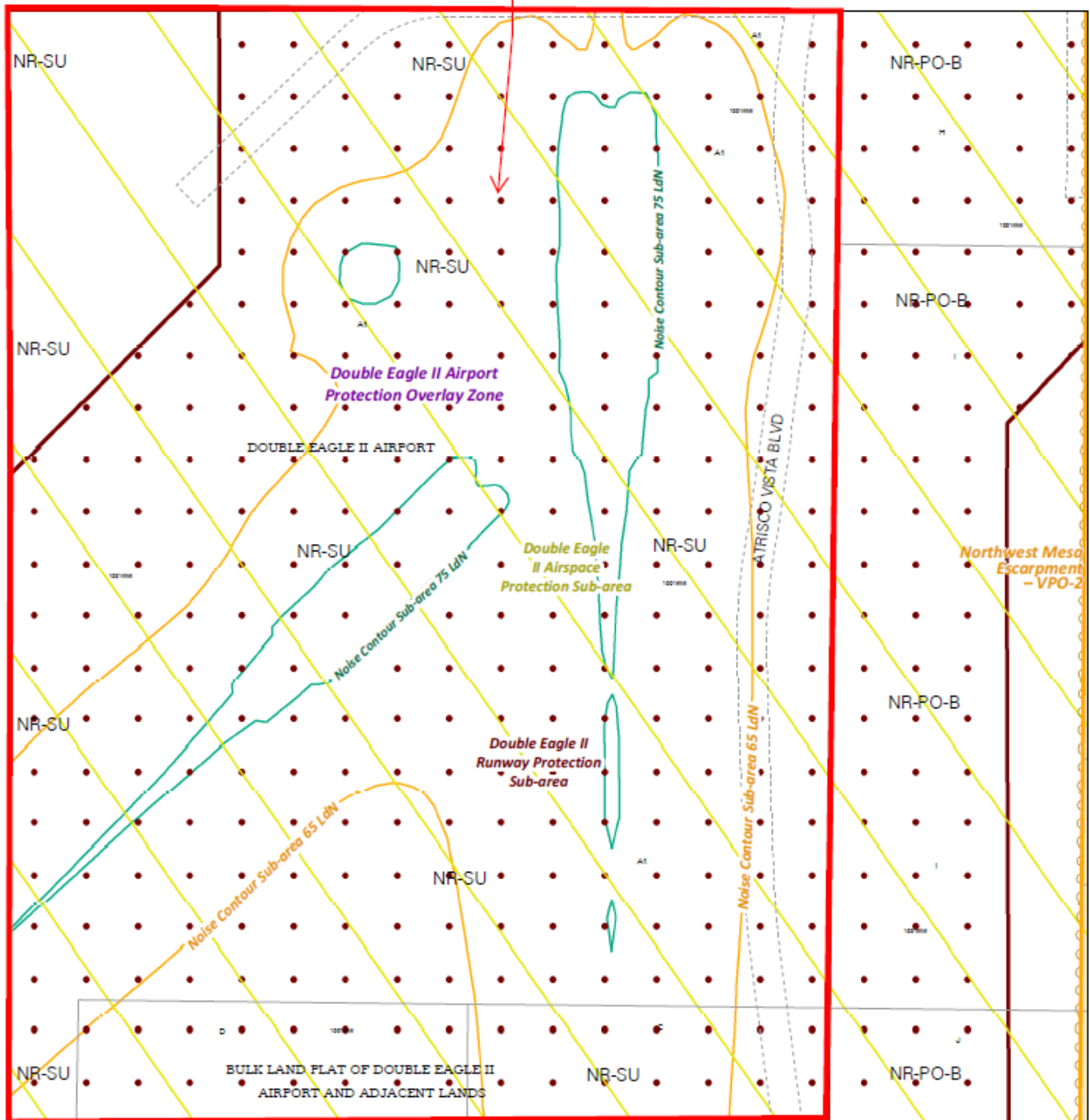


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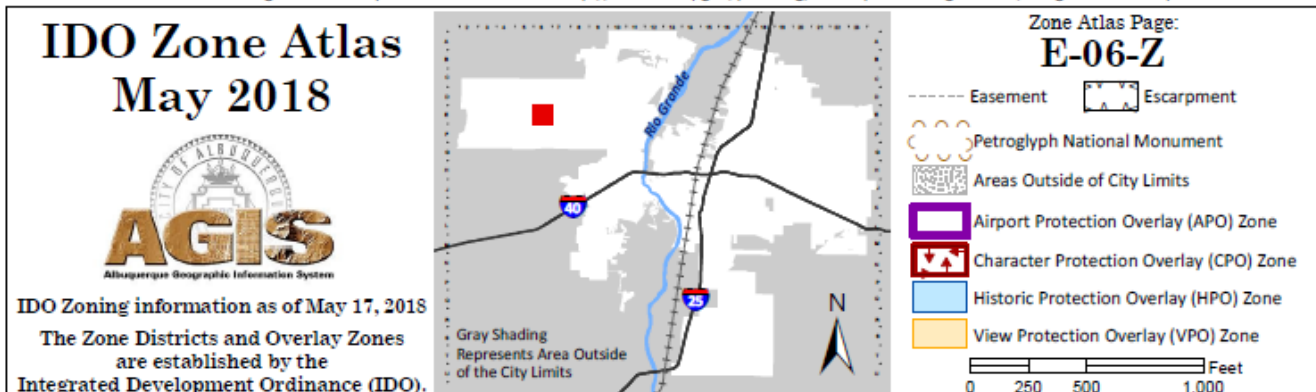
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- Areas Outside of City Limits
- Airport Protection Overlay (APO) Zone
- Character Protection Overlay (CPO) Zone
- Historic Protection Overlay (HPO) Zone
- View Protection Overlay (VPO) Zone

0 250 500 1,000 Feet

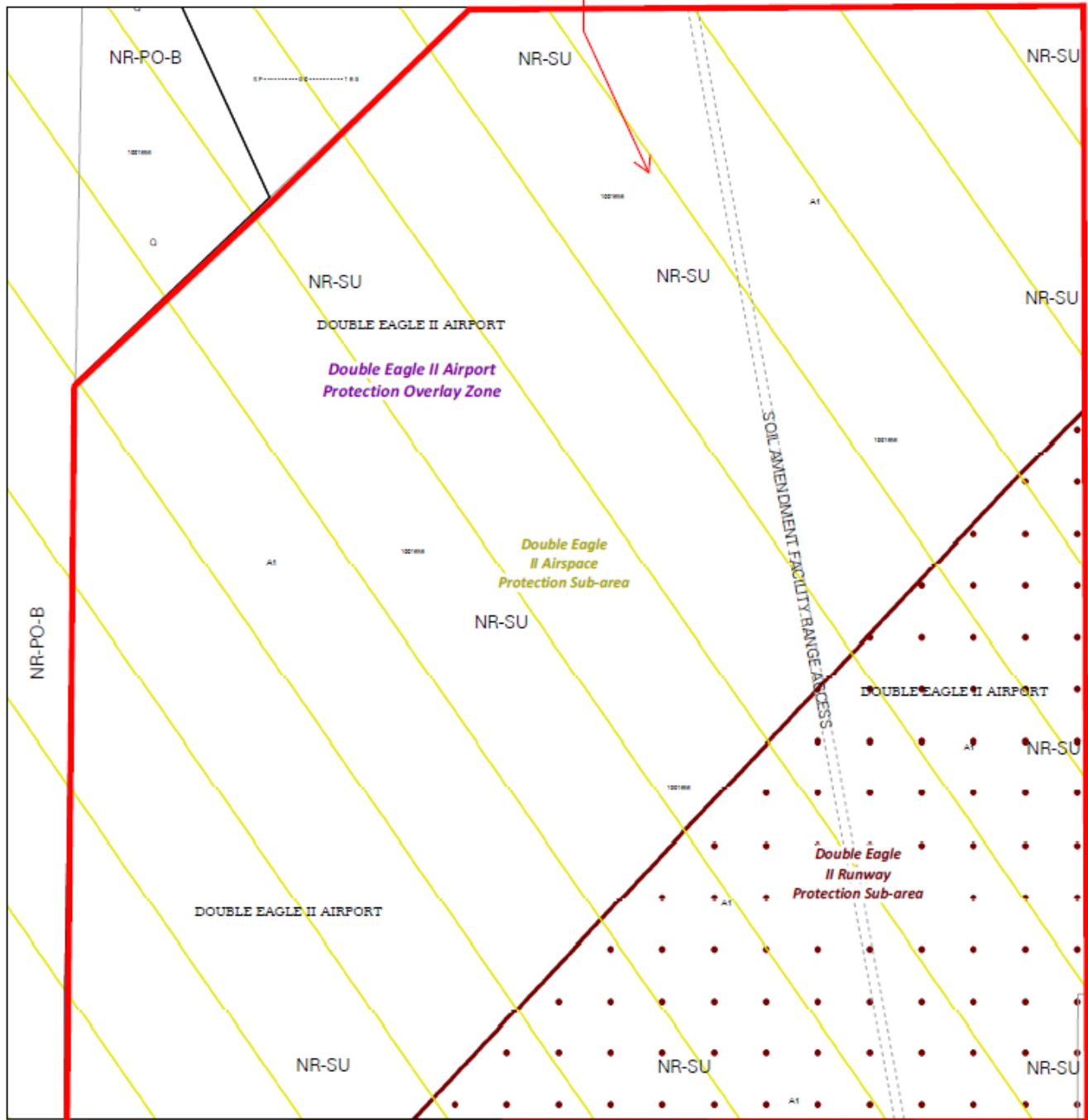
Subject Property



For more details about the Integrated Development Ordinance visit: <http://www.cabq.gov/planning/codes-policies-regulations/integrated-development-ordinance>



Subject Property



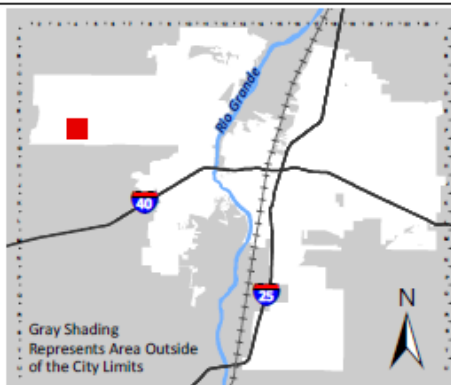
For more details about the Integrated Development Ordinance visit: <http://www.cabq.gov/planning/codes-policies-regulations/integrated-development-ordinance>

IDO Zone Atlas May 2018



IDO Zoning information as of May 17, 2018

The Zone Districts and Overlay Zones are established by the Integrated Development Ordinance (IDO).



Zone Atlas Page:

F-04-Z

- Easement
- Escarpment
- Petroglyph National Monument
- Areas Outside of City Limits
- Airport Protection Overlay (APO) Zone
- Character Protection Overlay (CPO) Zone
- Historic Protection Overlay (HPO) Zone
- View Protection Overlay (VPO) Zone

0 250 500 1,000 Feet

The map displays the Double Eagle II Airport and its surrounding lands. Key features include:

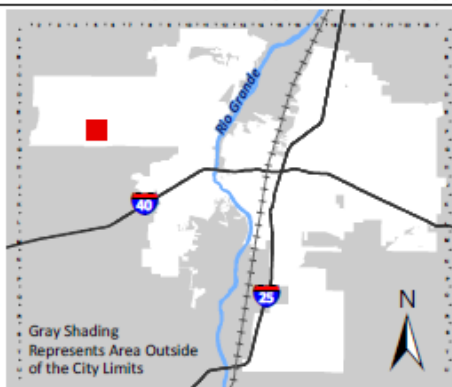
- Noise Contours:** Labeled as "Noise Contour Subarea 65 Ldn" (orange line) and "Noise Contour Subarea 75 Ldn" (green line).
- Protection Zones:**
 - Double Eagle II Runway Protection Sub-area:** Indicated by a yellow line.
 - Double Eagle II Airspace Protection Sub-area:** Indicated by a yellow line.
 - Double Eagle II Airport Protection Overlay Zone:** Indicated by a purple line.
- Land Use Designations:** NR-SU (Neighborhood Residential - Single-Unit) and B (Business).
- Infrastructure:** A red line representing a major road or boundary runs diagonally across the map.
- Geographic Labels:** "DOUBLE EAGLE II AIRPORT", "BULK LAND FLAT OF DOUBLE EAGLE II AIRPORT AND ADJACENT LANDS", and "BULK LAND FLAT OF DOUBLE EAGLE II" are present.

IDO Zone Atlas









May 2018

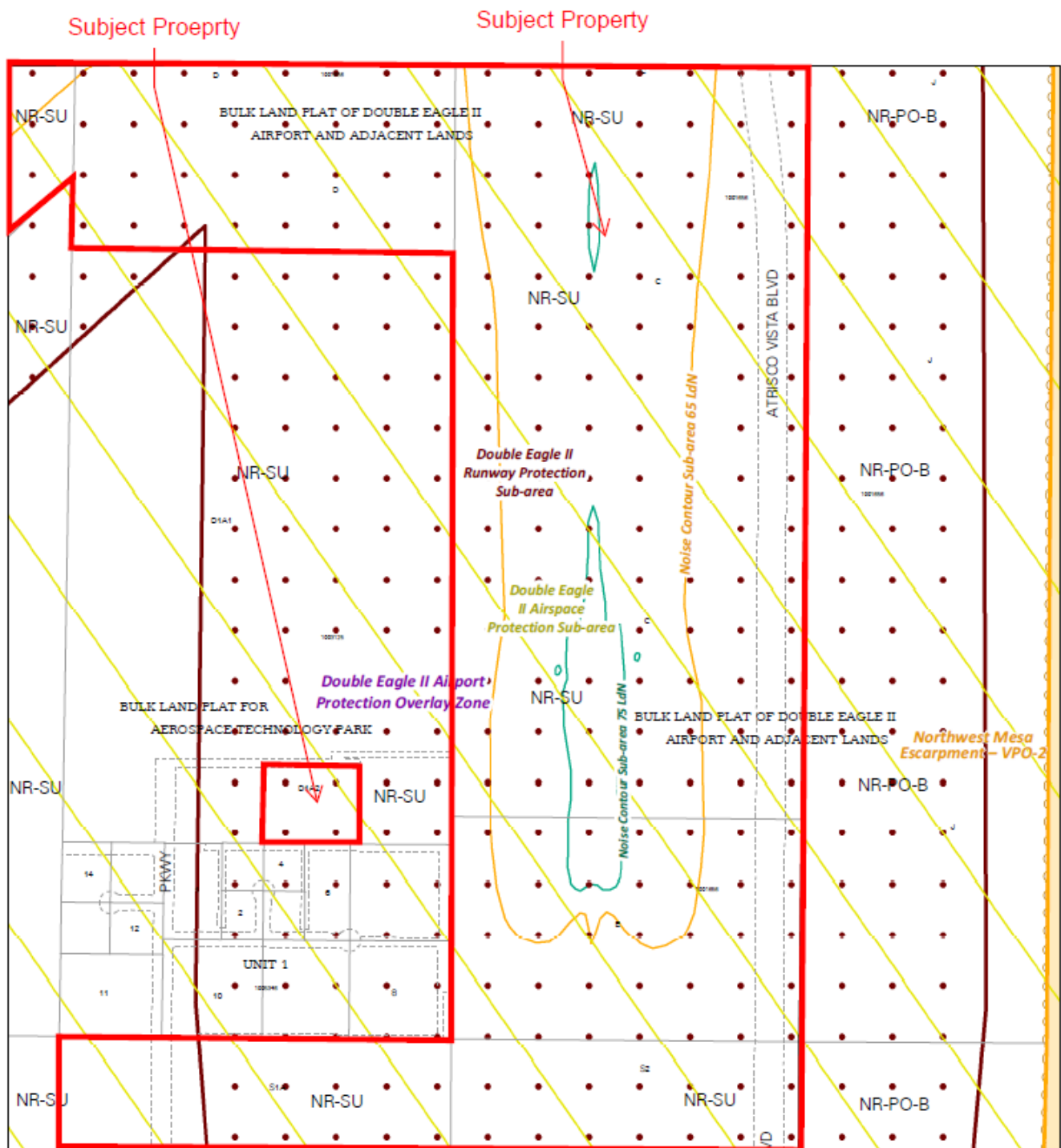


The Zone Districts and Overlay Zones are established by the Integrated Development Ordinance (IDO).

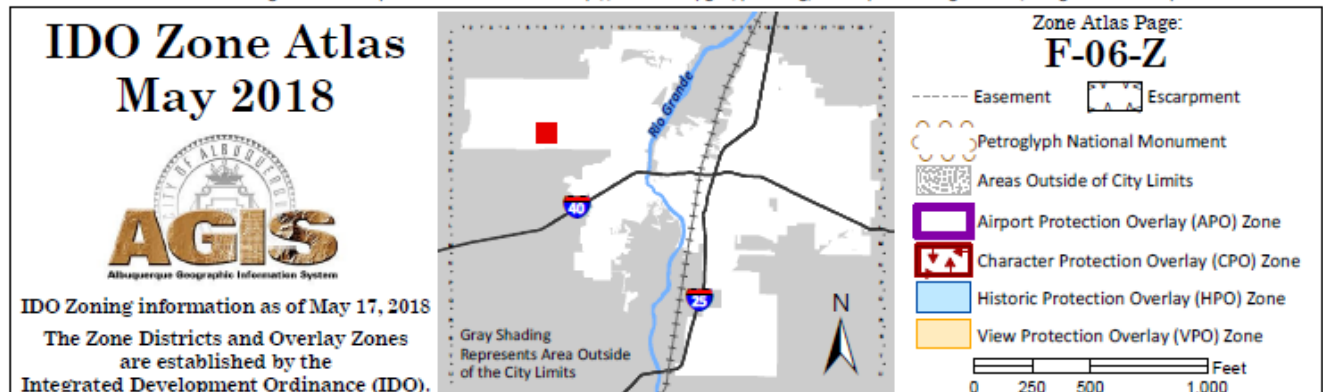


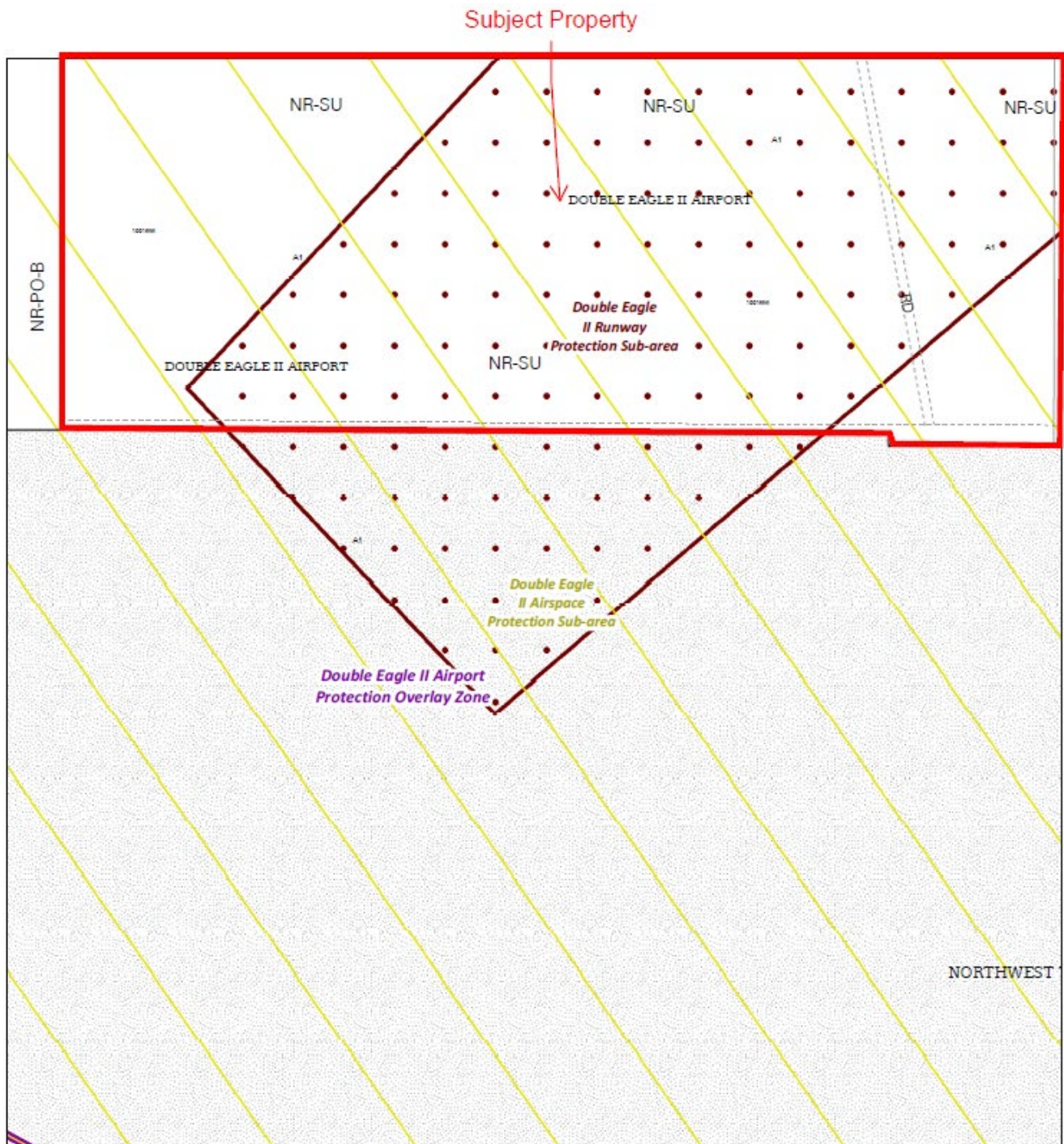
F-05-Z

-  Easement
  Escarpment
-  Petroglyph National Monument
-  Areas Outside of City Limits
-  Airport Protection Overlay (APO) Zone
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For more details about the Integrated Development Ordinance visit: <http://www.cabq.gov/planning/codes-policies-regulations/integrated-development-ordinance>





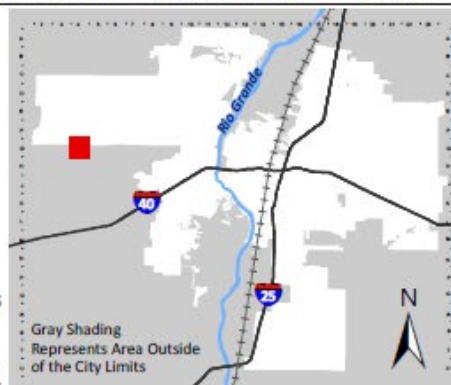
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IDO Zone Atlas May 2018



IDO Zoning information as of May 17, 2018

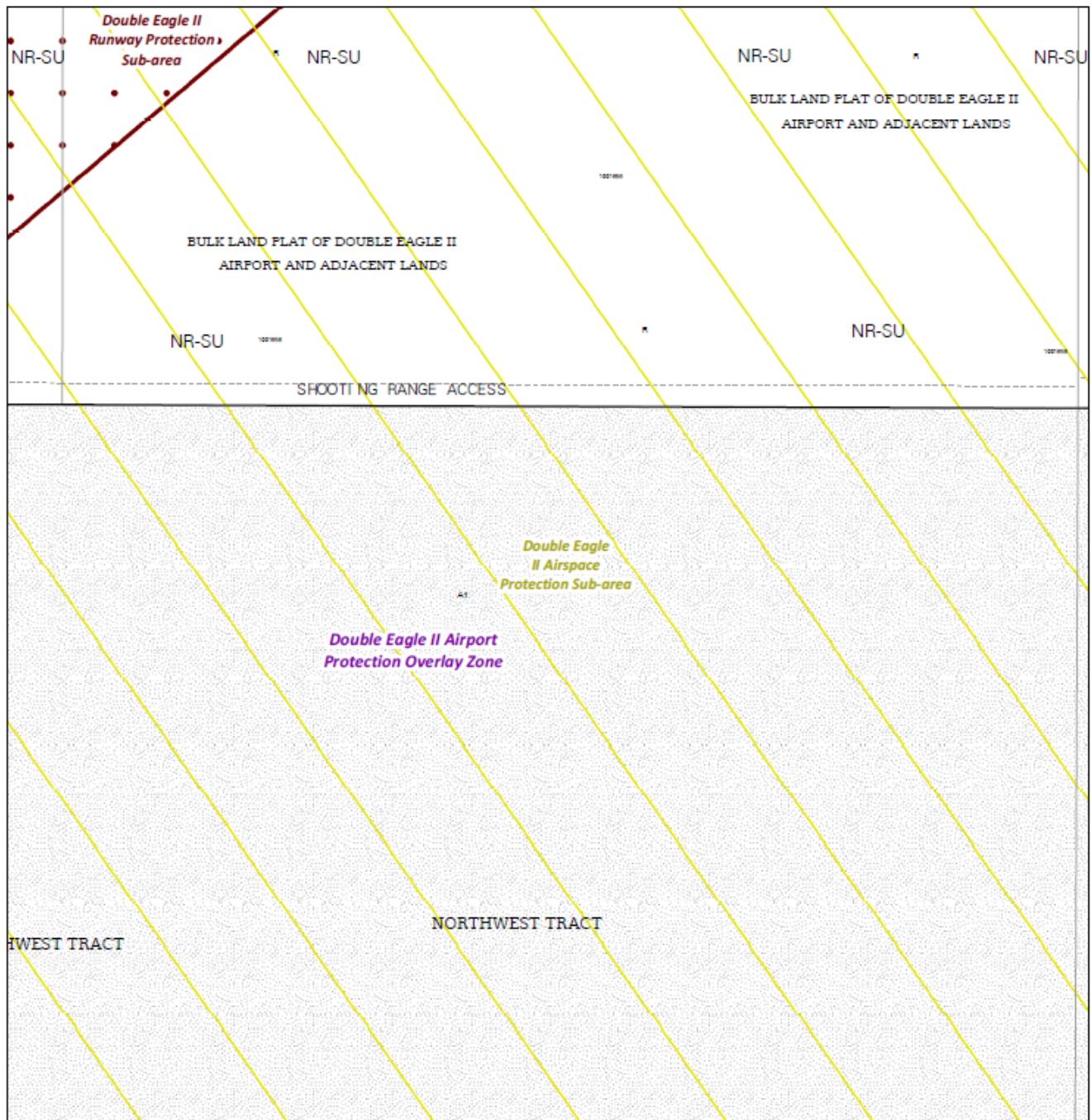
The Zone Districts and Overlay Zones are established by the Integrated Development Ordinance (IDO).



Zone Atlas Page:
G-04-Z

- Easement
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0 250 500 1,000 Feet



For more details about the Integrated Development Ordinance visit: <http://www.cabq.gov/planning/codes-policies-regulations/integrated-development-ordinance>

IDO Zone Atlas

May 2018

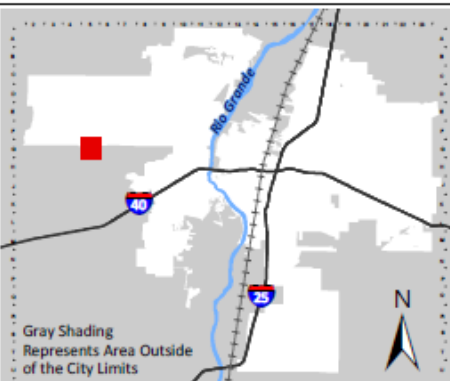


IDO Zoning information as of May 17, 2018

The Zone Districts and Overlay Zones are established by the Integrated Development Ordinance (IDO).

Zone Atlas Page:

G-05-Z



Gray Shading Represents Area Outside of the City Limits

----- Easement [] Escarpment

○ ○ ○ Petroglyph National Monument

[] Areas Outside of City Limits

[] Airport Protection Overlay (APO) Zone

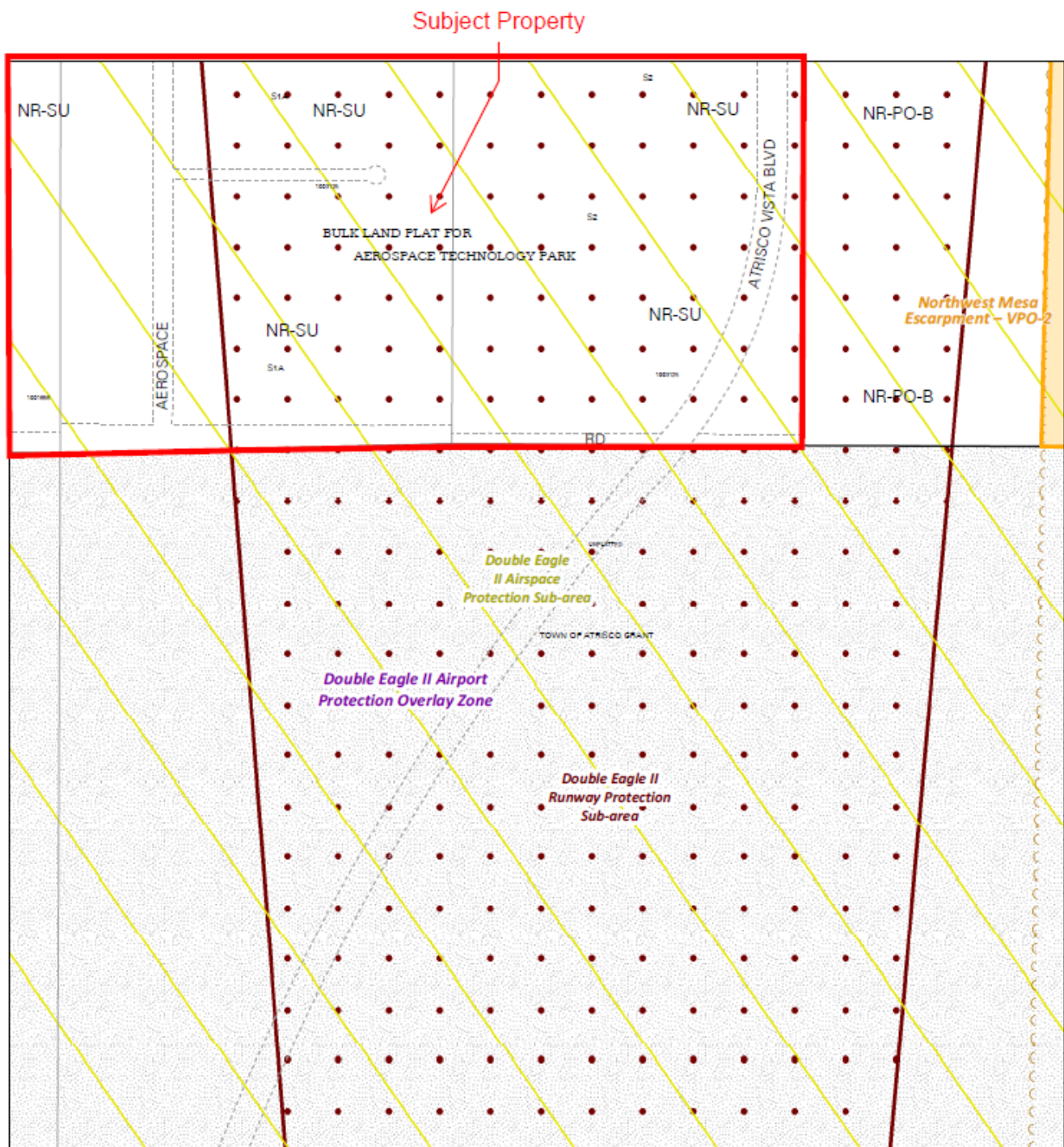
[] Character Protection Overlay (CPO) Zone

[] Historic Protection Overlay (HPO) Zone

[] View Protection Overlay (VPO) Zone

Feet


0 250 500 1,000



For more details about the Integrated Development Ordinance visit: <http://www.cabq.gov/planning/codes-policies-regulations/integrated-development-ordinance>

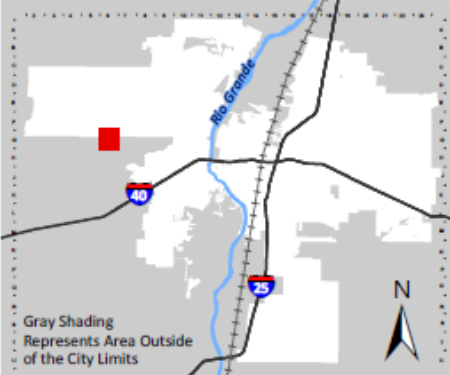
IDO Zone Atlas

May 2018



IDO Zoning information as of May 17, 2018

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Gray Shading Represents Area Outside of the City Limits

Zone Atlas Page:
G-06-Z

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0 250 500 1,000 Feet





MEMO

DATE: January 8, 2024
TO: COA Planning Department
FROM: Richard McCurley, Director of Aviation
SUBJECT: Double Eagle II Airport 2024 Master Plan Text Amendments to allow Non- Aeronautical Uses

DS
RM

To Whom It May Concern,

This letter authorizes Dekker Perich Sabatini LLC to represent The City of Albuquerque Aviation Department with regard to necessary entitlements for the Double Eagle II Airport Master Plan. This letter authorizes Dekker Perich Sabatini LLC to act as Aviation Department's agent as necessary with the associated approval processes required for the proposed amendment to the Double Eagle Master Plan on the property referenced above.

Sincerely,

DocuSigned by:
X Richard McCurley 1/9/2024 | 4:28 PM MST
Richard McCurley,
Director of Aviation



PRE-APPLICATION REVIEW NOTES

PA#: 24-035 Notes Provided (date): 5/16/2024

Site Address and/or Location: 7401 Paseo del Volcan NW 87120 (Double Eagle II Airport)

Pre-application notes are for informational purposes only and are non-binding and do not constitute any type of approval and are not certificates of zoning. Additional research may be necessary to determine the exact type of process and/or application required. Factors unknown and/or thought of as minor at this time could become significant as a case progresses.

Request: NR-SU Site Plan for the existing Airport.

Basic Site Information

Current Use(s): Airport

Size (acreage): 4500

Zoning: NR-SU

Overlay Zone(s): APO 3-3

Comprehensive Plan Designations

Corridor(s): NA

Development Area: change and consistency

Near Major Public Open Space (MPOS)?: Yes

Center: Employment Center

Integrated Development Ordinance (IDO)

Please refer to the IDO for requirements regarding dimensional standards, parking, landscaping, walls, signage, etc.
<https://www.cabq.gov/planning/codes-policies-regulations/integrated-development-ordinance>

Proposed Use(s): Airport and Adding several non aeronautical uses unknown at this time

Use Specific Standards: standards specified per the NR-SU site plan

Applicable Definition(s):

Site Plan

An accurate plan that includes all information required for that type of application, structure, or development.

Sensitive Lands: *Please see IDO Section 14-16-5-2 for information about required analysis, development standards, and changes to process that may result if this Section applies.*

Notice

Neighborhood Meeting Offer Required? (see IDO Table 6-1-1). If yes, please refer to:

<https://www.cabq.gov/planning/urban-design-development/neighborhood-meeting-requirement-in-the-integrated-development-ordinance>

Process

Decision Type(s) (see IDO Table 6-1-1): New Site Plan-EPC

Specific Procedure(s)*: 14-16-6-6(I)

**Please refer to specific procedures for relevant decision criteria required to be addressed.*

Decision Making Body: EPC Is this a PRT requirement? No

Handouts Provided

- | | | | |
|--|---|--|---|
| <input type="checkbox"/> Zoning Map Amendment | <input type="checkbox"/> Amendments | <input checked="" type="checkbox"/> Site Plan- EPC | <input type="checkbox"/> Site Plan- DHO |
| <input type="checkbox"/> Site Plan- Administrative | <input type="checkbox"/> Variance-ZHE | <input type="checkbox"/> Conditional Use | <input type="checkbox"/> Subdivision |
| <input type="checkbox"/> Site History/Research | <input type="checkbox"/> Transportation | <input type="checkbox"/> Hydrology | <input type="checkbox"/> Fire |

If you have additional questions after reviewing these notes, or would like to schedule a follow up call or meeting, please contact Staff at planningprt@cabq.gov. Please include the PA# with your inquiry.

Additional Notes:

- Although the Master Plan document can be amended through a text amendment, there is not an existing approved Site Plan for the DEII Airport. The NR-SU zone district requires that a Site Plan is reviewed and decided by the EPC. The NR-SU Site Plan would establish specific uses, site standards, and regulations for the Airport site as negotiated at the EPC through EPC application and the Site Plan.
 - There should not be a Master Plan without a Site Plan.
 - Master Plans and land uses are voluntary for the facility, but the Master Plan and Site Plan are meant to be complimentary. As it stands the master plan only includes an “Airport Layout Drawing.” There have been several Administrative Site Plans for the airport which have been approved, but this is not consist with the IDO nor the NR-SU zone district.
 - Moving forward, the Airport Site will need one cohesive NR-SU Site Plan pursuant to IDO Section 2-5(E). The site plans that have been approved administratively can be included as pullouts or “parts” of the site plan.
- Pursuant to IDO section 2-5(E) NON-RESIDENTIAL – SENSITIVE USE ZONE DISTRICT (NR-SU), an Airport use is required to be reviewed by the EPC with a Site Plan that specifies uses, site standards, and development standards, reviewed and decided by the EPC. Where the Site Plan is silent, IDO standards apply. The use requires additional review of location, site design, and impact mitigation to protect the safety and character of surrounding properties.
- This is not an amendment to the Master Site Plan, since there is not one, this will be reviewed as an as-built site plan with new elements.
- Planning staff can set a time to meet with City Aviation and DPS to review the following information if desired.

Applicant Questions:

1. How do we document the existing conditions for the site plan of the 4,000 +/- acres? Numerous structures are old and records on construction dates/plans are no longer available or illegible. Specifically, what level of detail is required for structures to be shown on the Site Plan? There are various communication towers, water tanks, fuel storage, utility buildings, pump stations, and road salt storage area. For utility-related structures, we propose showing the building footprint but not quantifying square footage or elevations.
2. Form P1 states: “Master Development Plans should include general building and parking locations, as well as design requirements for buildings, landscaping, lighting, and signage.” What is considered general building and parking information? We propose to show building footprints and generally indicate designated parking areas showing approximate individual parking spaces. The same approach for Landscaping: Landscaping is currently limited to

certain areas around more office or visitor related functions. We propose to show this on the site plan diagrammatically, rather than showing specific plants and plant types.

- The Site Plan will need to be submitted as an “As-built” site plan that gives as much detail and documentation as possible with all of the proposed new uses and development known at the time.
 - This includes all dimensions and SF of existing structures, so that future actions are easier to determine. We also encourage the applicant to include existing materials and colors to show compliant with the Airport Protection Overlay zone. Future amendments, whether major or minor, will be determined based on existing dimensional standards and SF documented. If they are not given, the amendments will be sent to the EPC for review for any proposed future changes. If Aviation would like to go to EPC for each amendment, they can note that, otherwise we strongly recommend including the greater detail, which for some proposed future changes may only require a minor amendment.
 - Existing landscaping should be documented, but does not need to be detailed as far as planting counts.
 - Parking spaces currently provided at the facilities should be listed in a table.
3. Building elevations: do we need to provide any existing building elevations? Most of the existing buildings are utilitarian in nature.
- Yes, elevations of existing buildings should be provided. These can be photos of existing buildings with tables to document dimensional standards (heights, lengths and widths) . We also encourage the applicant to include existing materials and colors to show compliant with the Airport Protection Overlay zone.
 - All new development will require fully dimensioned and detailed elevations
4. For the Design Guidelines, we propose to include as a stand-alone page of the Master Plan document, rather than attach directly to the overall site plan.
- This is fine, as long as it is a sheet that’s numbered as part of the site plan set, it cannot be an exhibit.

SITE PLAN – EPC

The Integrated Development Ordinance (IDO) establishes review and approval processes for various types of development requests in Part 6: Administration and Enforcement.

The IDO is available for review online:

- Interactive IDO: <https://ido.abc-zone.com/>
- IDO PDF for download: <https://www.cabq.gov/planning/codes-policies-regulations/integrated-development-ordinance>

DISCLAIMER: This handout provides a general summary of answers to frequently asked questions. Please refer to the IDO for complete and up-to-date information.

How do I know whether I need a Site Plan – EPC?

A proposed site plan is required to be reviewed by the Environmental Planning Commission (EPC) if the request meets any of the criteria for applicability in Subsection 14-16-6-6(I). Generally, Site Plan-EPC is required for development:

1. in an NR-PO zone district that does not qualify for review under Site Plan-Admin.
2. in a zone district that requires a site plan simultaneously, including but not limited to MX-FB, MR-SU, and PD.
3. on a lot 5 acres or greater adjacent to Major Public Open Space (MPOS)
4. that has not avoided sensitive lands identified in Subsection 14-16-5-2(C)
5. is in a Railroad and Spur Small area requiring cumulative impact analysis
6. if the applicant requests EPC review and the Planning Director agrees
7. of an electric utility for which the Facility Plan for Electric Transmission and Generation requires EPC review
8. of a major utility as a primary use of the site unless specified otherwise in an adopted Facility Plan.

Please refer to Subsection 14-16-6-6-(I) for additional details.

Who do I have to notify *before* I apply?

The applicant must offer to meet with representatives of affected Neighborhood Associations before submitting an application. This process can take up to 45 days, so please plan your timeline accordingly. IDO Subsection 14-16-6-4(C) describes the process and requirements for this Pre-submittal Neighborhood Meeting. More information, including required forms and content, is online here:

<https://www.cabq.gov/planning/urban-design-development/neighborhood-meeting-requirement-in-the-integrated-development-ordinance>

The applicant must send notice to neighborhood organization representatives and property owners. IDO Subsection 14-16-6-4(K) explains public notice requirements. More information, including required forms and content, is online here: <https://www.cabq.gov/planning/urban-design-development/public-notice>

What is required in the application?

The Site Plan Checklist includes the requirements for the submittal:
<https://www.cabq.gov/planning/online-forms>

The application requires a written/detailed response to the criteria (a – g) in IDO Subsection 14-16-6-6(I)(3), which contains the Review and Decision Criteria that the EPC will use.

What are the deadlines, and how soon can I expect a decision?

For more information about the EPC, please visit: <https://www.cabq.gov/planning/boards-commissions/environmental-planning-commission>

A calendar of submittal deadlines, general information, and agendas and staff reports from past meetings are available.

The EPC process is about six weeks from submittal of the application, provided the application is complete and that the response to the decision criteria is adequate. If not, delays could result. The EPC hearing occurs on the third Thursday of the month following the submittal date.

SITE PLAN CHECKLIST

Project #: _____ Application #: _____

This checklist will be used to verify the completeness of site plans submitted for review by the Planning Department. Because development proposals vary in type and scale, there may be submittal requirements that are not specified here. Also there may additional requirements if a site is located in CPO, HPO, and/or VPO or if located in DT-UC-MS or PT areas. See the IDO or AGIS for boundaries. Nonetheless, applicants are responsible for providing a complete submittal. Certification as specified below is required.

I CERTIFY THAT THE SUBMITTED SITE PLAN IS COMPLETE AND ACCURATE, AND THAT ALL APPLICABLE INFORMATION AS SPECIFIED IN THIS CHECKLIST IS PROVIDED. FURTHER, I UNDERSTAND THAT THIS APPLICATION IS BEING ACCEPTED PROVISIONALLY AND THAT INACCURATE AND/OR INCOMPLETE INFORMATION MAY RESULT IN THE SUBSEQUENT REJECTION OF THE APPLICATION OR IN A DELAY OF ONE MONTH OR MORE IN THE DATE THE APPLICATION IS SCHEDULED FOR PUBLIC HEARING.



Applicant or Agent Signature / Date

Site plan packets shall be composed of the following plan sheets (unless otherwise approved in writing prior to submittal by the Planning Department):

1. Site Plan (including utilities and easements)
2. Landscaping Plan
3. Grading and Drainage Plan
4. Utility Plan
5. Building and Structure Elevations
6. Previously approved Development Plan (if applicable)

The electronic format must be organized in the above manner.

The following checklist describes the minimum information necessary for each plan element. **The Applicant must include all checklist items on their site plan drawings and confirm inclusion by checking off the items below. Non-applicable items must be labeled "N/A."** Each non-applicable designation must be explained by notation on the Checklist.

NOTE: There may be addition information required if site is located with a CPO, VPO or HPO and/or any other special areas as defined by the IDO.

NOTE: If there requests for deviations (Section 14-16-6-4(O), they must be clearly labelled on the site plan (Sheet 1) as well as addressed in the application letter made with the submittal.

SHEET #1 - SITE PLAN

A. General Information

- ☒ 1. Date of drawing and/or last revision
- ☒ 2. Scale: 1.0 acre or less 1" = 10'
- 1.0 - 5.0 acres 1" = 20'
- Over 5 acres 1" = 50'
- Over 20 acres 1" = 100'

SITE PLAN CHECKLIST

- ~~X~~ 3. Bar scale
- ~~X~~ 4. North arrow
- ~~X~~ 5. Legend
- ~~X~~ 6. Scaled vicinity map
- ~~X~~ 7. Property lines (clearly identify)
- ~~X~~ 8. Existing and proposed easements (identify each)
- N/A 9. Phases of development, if applicable

B. Proposed Development

1. Structural

- ~~X~~ A. Location of existing and proposed structures (distinguish between existing & proposed) and include any accessory structures
- ~~X~~ B. Square footage of each structure
- ~~X~~ C. Proposed use of each structure
- ~~X~~ D. Signs (freestanding) and other improvements
- E. Walls, fences, and screening: indicate height, length, color and materials
- ~~X~~ F. Dimensions of all principal site elements or typical dimensions
- G. Loading facilities
- ~~X~~ H. Site lighting (indicate height & fixture type)
- I. Indicate structures within 20 feet of site
- J. Elevation drawing of refuse container and enclosure, if applicable.
- ~~X~~ K. Existing zoning/land use of all abutting properties

2. Parking, Loading and Internal Circulation

- ~~X~~ A. Parking layout with spaces numbered per aisle and totaled.
 - ~~X~~ 1. Location and typical dimensions, including motorcycle spaces, bicycle spaces, ADA accessible spaces, and compact spaces
 - N/A 2. Calculations: spaces required and proposed (include any reduction calculations) for motorcycle, bicycle, compact and ADA spaces
 - N/A 3. On street parking spaces
- N/A B. Bicycle parking & facilities
 - N/A 1. Bicycle racks – location and detail
 - N/A 2. Other bicycle facilities, if applicable
- N/A C. Vehicular Circulation (Refer to DPM and IDO)
 - 1. Ingress and egress locations, including width and curve radii dimensions
 - 2. Drive aisle locations, including width and curve radii dimensions
 - 3. End aisle locations, including width and curve radii dimensions
 - 4. Location & orientation of refuse enclosure, with dimensions
 - 5. Loading, service area, and refuse service locations and dimensions
- N/A D. Pedestrian Circulation
 - 1. Location and dimensions of all sidewalks and pedestrian paths (including ADA connection from ROW to building and from ADA parking to building)

SITE PLAN CHECKLIST

- ___ 2. Location and dimension of drive aisle crossings, including paving treatment
- ___ 3. Location and description of amenities, including patios, benches, tables, etc.

N/A E. Off-Street Loading

- ___ 1. Location and dimensions of all off-street loading areas

N/A F. Vehicle Stacking and Drive-Through or Drive-Up Facilities

- ___ 1. Location and dimensions of vehicle stacking spaces and queuing lanes
- ___ 2. Landscaped buffer area if drive-through lanes are adjacent to public R/W
- ___ 3. Striping and Sign details for one-way drive through facilities

3. Streets and Circulation

☒ A. Locate and identify adjacent public and private streets and alleys.

- N/A ___ 1. Existing and proposed pavement widths, right-of-way widths and curve radii
- N/A ___ 2. Identify existing and proposed turn lanes, deceleration lanes and similar features related to the functioning of the proposal, with dimensions
- N/A ___ 3. Location of traffic signs and signals related to the functioning of the proposal
- N/A ___ 4. Identify existing and proposed medians and median cuts
- N/A ___ 5. Sidewalk widths and locations, existing and proposed
- ☒ ___ 6. Location of street lights
- N/A ___ 7. Show and dimension clear sight triangle at each site access point
- ☒ ___ 8. Show location of all existing driveways fronting and near the subject site.

N/A B. Identify Alternate transportation facilities within site or adjacent to site

- N/A ___ 1. Bikeways and bike-related facilities
- N/A ___ 2. Pedestrian trails and linkages
- N/A ___ 3. Transit facilities, including routes, bus bays and shelters existing or required

4. Phasing

- N/A A. Proposed phasing of improvements and provision for interim facilities. Indicate phasing plan, including location and square footage of structures and associated improvements including circulation, parking and landscaping.

SHEET #2 - LANDSCAPING PLAN

- ☒ 1. Scale - must be same as scale on sheet #1 - Site plan
- ☒ 2. Bar Scale
- ☒ 3. North Arrow
- ☒ 4. Property Lines
- ☒ 5. Existing and proposed easements
- ☒ 6. Identify nature of ground cover materials
 - ☒ A. Impervious areas (pavement, sidewalks, slope pavings, curb and gutters, etc.)
 - ☒ B. Pervious areas (planting beds, gravel areas, grass, ground cover vegetation, etc.)
 - ☒ C. Ponding areas either for drainage or landscaping/recreational use

SITE PLAN CHECKLIST

- N/A 7. Identify type, location and size of plantings (common and/or botanical names).
 N/A A. Existing, indicating whether it is to be preserved or removed.
 N/A B. Proposed, to be established for general landscaping.
 N/A C. Proposed, to be established for screening/buffering.
- N/A 8. Describe irrigation system – Phase I & II . . .
- X 9. Planting Beds, indicating square footage of each bed
- X 10. Turf Area - only 20% of landscaped area can be high water turf; provide square footage and percentage.
- X 11. Responsibility for Maintenance (statement)
- N/A 12. Landscaped area requirement; square footage and percent (specify clearly on plan)
- N/A 13. Landscaped buffer areas provided; dimensions, label clearly that it is a landscape buffer, square footage and percent (specify clearly on plan)
- N/A 14. Planting or tree well detail
- N/A 15. Street Trees (only trees from the Official Albuquerque Plant Palette and Sizing list or 8 inch caliper or larger will be counted)
- N/A 16. Parking lot edges and interior – calculations, dimensions and locations including tree requirements
- N/A 17. Show Edge Buffer Landscaping (14-16-5-6(D)) – location, dimensions and plant material

SHEET #3 –GRADING AND DRAINAGE PLAN

A separate grading and drainage plan (and drainage report) must be submitted to the DRS Hydrology Section prior to the DRB submittal for a site plan (See DRWS Form).

A. General Information

- N/A 1. Scale - must be same as Sheet #1 - Site Plan
- N/A 2. Bar Scale
- N/A 3. North Arrow
- N/A 4. Property Lines
- N/A 5. Existing and proposed easements
- N/A 6. Building footprints
- N/A 7. Location of Retaining walls

B. Grading Information

- N/A 1. On the plan sheet, provide a narrative description of existing site topography, proposed grading improvements and topography within 100 feet of the site.
- N/A 2. Indicate finished floor elevation and provide spot elevations for all corners of the site (existing and proposed) and points of maximum cut or fill exceeding 1 foot.
- N/A 3. Identify ponding areas, erosion and sediment control facilities.
- N/A 4. Cross Sections
Provide cross section for all perimeter property lines where the grade change is greater than 4 feet at the point of the greatest grade change. Provide one additional cross section in each direction within no more than 100 feet of the reference point.

SITE PLAN CHECKLIST

SHEET #4- UTILITY PLAN

- N/A A. Fire hydrant locations, existing and proposed. (or submit signed off Fire One Plan)
- N/A B. Distribution lines
- N/A C. Right-of-Way and easements, existing and proposed, on the property and adjacent to the boundaries, with identification of types and dimensions.
- N/A D. Existing water, sewer, storm drainage facilities (public and/or private).
- N/A E. Proposed water, sewer, storm drainage facilities (public and/or private)
- N/A F. Existing electric lines both overhead and underground. Power Poles shown with dimensions to proposed buildings and structures must be clearly shown.

SHEET #5 - BUILDING AND STRUCTURE ELEVATIONS

A. General Information

- ☒ A. Scale
- ☒ B. Bar Scale
- ☒ C. Detailed Building Elevations for each facade
 - ☒ 1. Identify facade orientation
 - ☒ 2. Dimensions of facade elements, including overall height and width
 - ☒ 3. Location, material and colors of windows, doors and framing
 - ☒ 4. Materials and colors of all building elements and structures
 - ☒ 5. Location and dimensions of mechanical equipment (roof and/or ground mounted)

B. Building Mounted Signage

- N/A 1. Site location(s)
- N/A 2. Sign elevations to scale
- N/A 3. Dimensions, including height and width
- N/A 4. Sign face area - dimensions and square footage clearly indicated
- N/A 5. Lighting
- N/A 6. Materials and colors for sign face and structural elements.
- N/A 7. List the sign restrictions per the IDO

July 31, 2024

Environmental Planning Commission
City of Albuquerque
600 2nd St NW, Albuquerque, NM 87102

RE: Double Eagle II Site Plan - EPC

Dear Planning Commission Members,

Dekker, as the agent for City of Albuquerque Aviation Department, is seeking EPC approval of the Double Eagle II as-built Site Plan and proposed Design Standards. The intent is to document existing site conditions and establish design standards that can be used to guide and evaluate future site plan submittals.

Location:

The subject site, known as Double Eagle II Airport (DEII), is located at 7401 Paseo Del Volcan NW, Albuquerque, NM 87120. Comprised of 12 parcels, the site has operated for decades as a general aviation airport for the City of Albuquerque. Most of the land is owned by the City, but there are some privately held tracts within the boundaries of DEII.

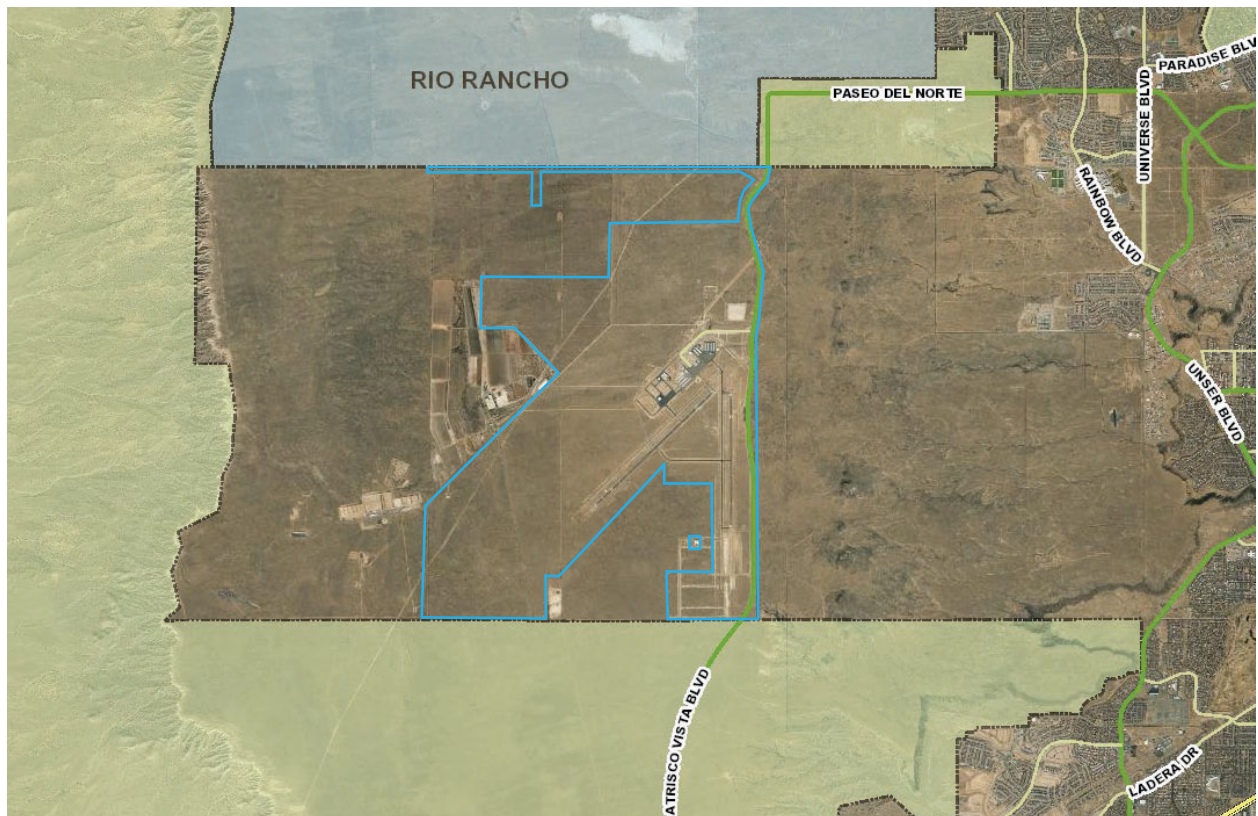


Figure 1. Location: Blue outline approximate boundary of DEII

Parcels:

Together, the 12 parcels are approximately 4134 acres based on City of Albuquerque's [CABQMaps - Advanced Map Viewer 2.0](#). The following chart breaks down the UPC numbers, Legal Descriptions, and acreage.

Parcels	UPC #	Legal Description	Acreage
1.	100606212040020101L1	TR A-1 PLAT OF TRACT A-1 & TRACT L-1 PARCELS 1-5 DOUBLEEAGLE II AIRPORT CONT 3196.8609 AC	3196.8609
2.	100606111049520101	TR F BULK LAND PLAT OF DOUBLE EAGLE II AIRPORT ANDADJACENT LANDS CONT 51.6910 AC	51.691
3.	100606027744410145	TR S-2 BULK LAND PLAT FOR AEROSPACE TECHNOLOGY PARKTRACTS D & S OF DOUBLE EAGLE II AIRPORT AND ADJACENTLANDS CONT 90.3889 AC	90.3889
4.	100606008144720144	TR S-1-A PLAT OF TRACTS D-1-A-1, D-1-A-2 & S-1-A OF AMENDED BULK LAND PLAT FOR AEROSPACE TECHNOLOGY PARKCONT 101.9724 AC	101.9724
5.	100406351009540116	TR K BULK LAND PLAT OF DOUBLE EAGLE II AIRPORT ANDADJACENT LANDS CONT 352.4366 AC	352.4366
6.	100606112712430102	TR D-1-A-2 PLAT OF TRACTS D-1-A-1, D-1-A-2 & S-1-A OFAMENDED BULK LAND PLAT FOR AEROSPACE TECHNOLOGYPARK CONT 5.0406 AC	5.0406
7.	100406444017540112	TR N-1 BULK LANDS PLAT OF TRACTS N-1, O-1 & N-2 PARCELS1, 2, 3 & 4 DOUBLE EAGLE II AIRPORT AND ADJACENT LANDSCONT 33.5154 AC	33.5154
8.	100506333252510101	S'LY PORT OF PARCEL 1 PLAT OF TRACT A-1 & TRACT L-1 PARCELS1-5 DOUBLE EAGLE II AIRPORT CONT 18.9764 AC	18.9764
9.	100506347052410102	S'LY PORT OF PARCEL 2 PLAT OF TRACT A-1 & TRACT L-1 PARCELS1-5 DOUBLE EAGLE II AIRPORT CONT 18.4392 AC	18.4392
10.	100606306752020101	S'LY PORT OF PARCEL 3 PLAT OF TRACT A-1 & TRACT L-1 PARCELS1-5 DOUBLE EAGLE II AIRPORT CONT 16.4607 AC	16.4607
11.	100606320251920102	S'LY PORT OF PARCEL 4 PLAT OF TRACT A-1 & TRACT L-1 PARCELS1-5 DOUBLE EAGLE II AIRPORT CONT 13.7885 AC	13.7885
12.	100606329951620103	S'LY PORT OF PARCEL 5 PLAT OF TRACT A-1 & TRACT L-1 PARCELS1-5 DOUBLE EAGLE II AIRPORT CONT 6.7307 AC	6.7307
13.	100606129529510163	Tract C Bulk Land Plat of Double Eagle II Airport and Adjacent Lands	177.4651
14.	100606126008540101	Tract E Bulk Land Plat of Double Eagle II Airport and Adjacent Lands	51.0468
Total Acreage:			4134.8132

Zoning:

The subject site is zoned Non-Residential – Sensitive Use Zone District (NR-SU) and is within the Airport Protection Overlay Zone (APO). The NR-SU zoning designation is required for highly specialized uses, such as airports, which is a permissive use within the NR-SU zone. IDO Subsection 14-16-4-1(A)(4)(b) requires any related development standards to be documented for NR-SU zoned properties with an associated Site Plan approved by the Environmental Planning Commission (EPC). DEII Airport does not have a comprehensive Site Plan on file and the existing DEII Master Plan does not have an established Site Plan or Design Standards aside from FAA safety regulations. Therefore, this request is required to have a Site Plan reviewed by EPC with site design standards negotiated at the EPC.

For any current or future improvements, a Site Plan is required for the subject site to come into compliance with the IDO. All existing development is reflected on the as-built Site Plan and the proposed Design Standards shall apply to current and future improvements. Where standards are silent, the IDO shall apply.

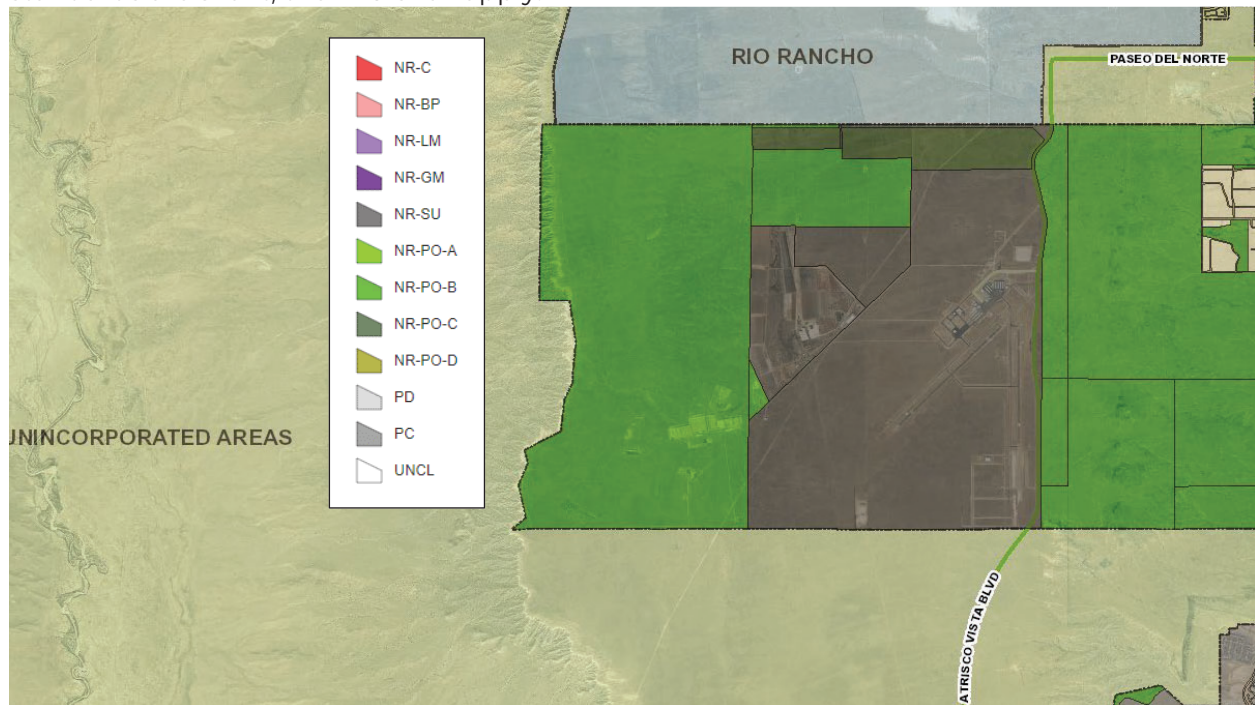


Figure 2. Zoning Map

Within the APO Overlay zone, the property is located in the DEII Air Space Protection Sub-area, the Runway Protection Sub-area, and the Noise Contour Sub-area which correspond to FAA “zones” and/or “surfaces”, detailed separately by FAA regulation.

Regulations	
Air Space Protection Sub-area	Maximum 150' high horizontal surface established at 6,028.0' in elevation
Runway Protection Sub-area	No build boundary
Noise Contour Sub-area	75 maximum DNL within runways, and 65 DNL within adjoining boundary

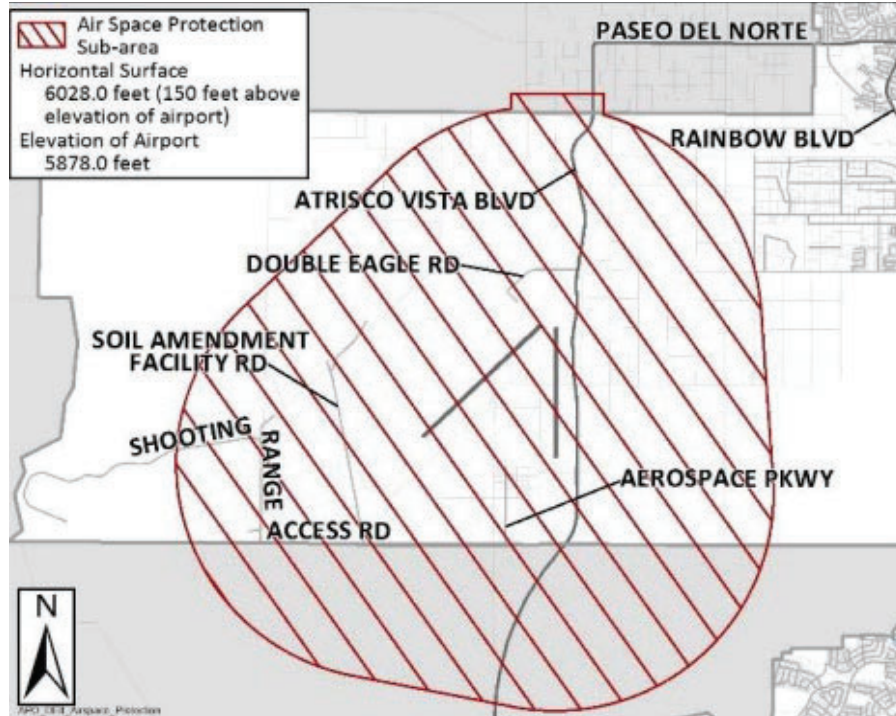


Figure 3. Air Space Protection Sub-Area

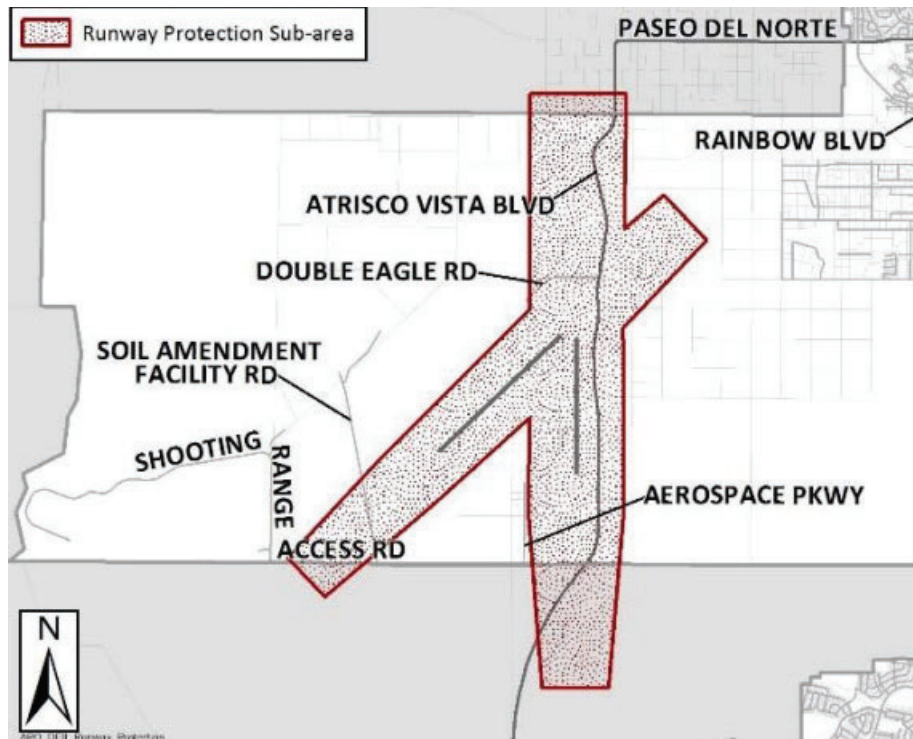


Figure 4. Runway Protection Sub-Area

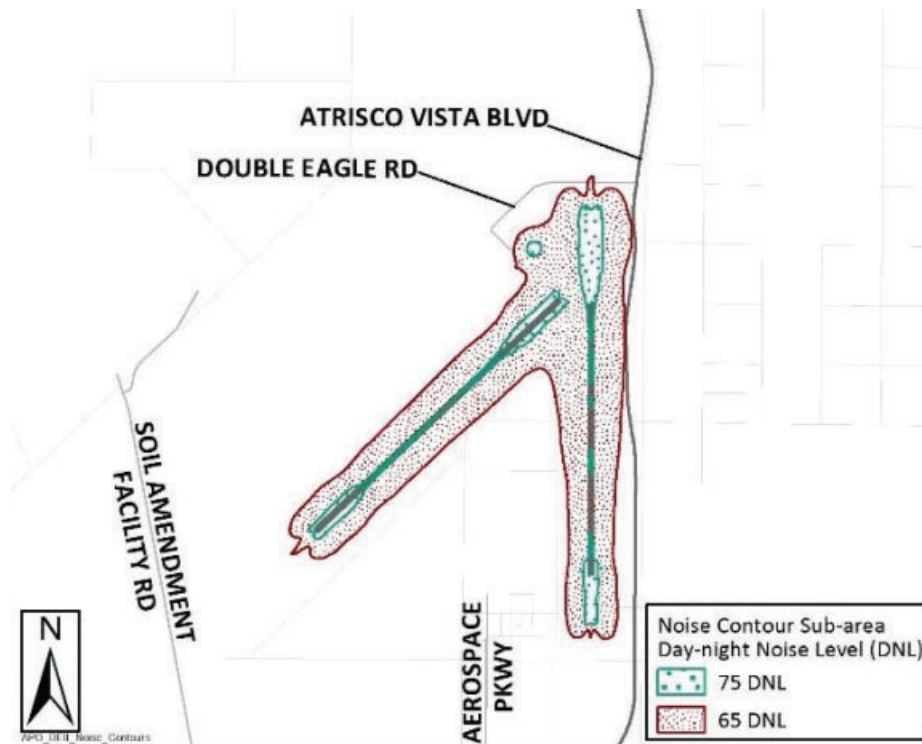


Figure 5. Noise Contour Sub-Area

Surrounding Area:

Depicted on the maps below, DEII is bounded by City of Albuquerque Open Space and the Petroglyph National Monument on the east side of Atrisco Vista Blvd, the City Shooting Range Park to the west, the City of Rio Rancho to the north, and Bernalillo County lands to the south.

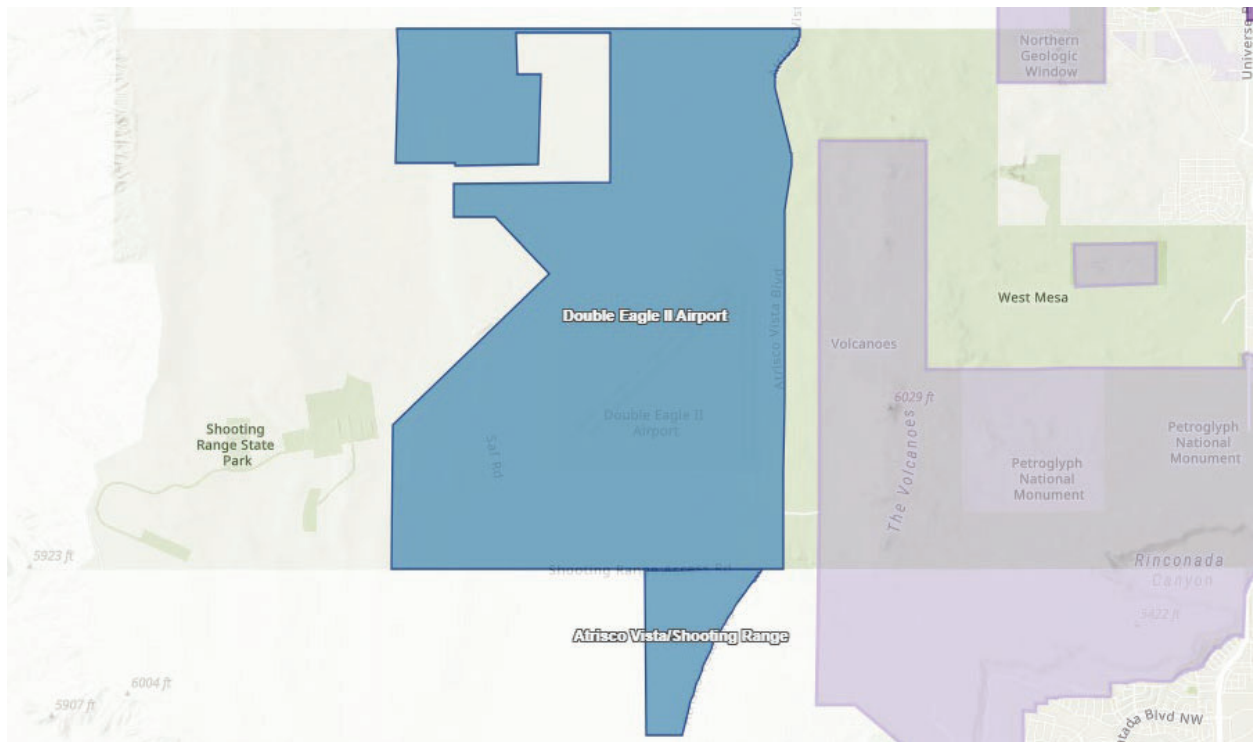


Figure 6. Surrounding Areas



Figure 7. Major Public Open Spaces

Context/Rationale for Site Plan – EPC and Design Standards:

The NR-SU Zone is designed to accommodate specialized uses, such as airports, that require additional review of location, site design, and impact mitigation to protect the safety and integrity of the surrounding properties, such as Major Public Open Space. Per IDO Section 14-16-2-5(E)(3)(c), the standards within the IDO would only apply if not specified on a Site Plan, as approved by the EPC. The Double Eagle II Airport has been in operation since approximately 1980. A Master Plan for the Airport was adopted in 2003, amended in 2018, and again in 2024. This Master Plan outlines a 20-year development vision for the Double Eagle Airport (DEII). The DEII Master Plan primarily addresses aviation operations and FAA safety regulations. The City of Aviation Department anticipates a major update to the overall Master Plan in approximately the next year. The NR-SU zone district established by the IDO, which was originally adopted in 2018, requires a NR-SU zoned property to establish a governing Site Plan and Design Standards. The City of Albuquerque Planning does not have a comprehensive Site Plan of DEII Airport on file, primarily because these requirements were adopted decades after DEII was initially constructed. This development application for a Site Plan – EPC seeks to develop an as-built Site Plan that documents the existing facilities of Double Eagle II Airport as they are the time of this application. Subsequent development applications will need to amend this site plan to properly document any development that occurs going forward. All future development will be held to the Design Standards established as part of this application and the IDO where they are silent.

DEII is home to over 100 based aircraft; in addition to general aviation aircraft, DEII is utilized by the military for training and itinerant services, the Albuquerque Aviation Academy, and public fly-in events. DEII operates as an enterprise, meaning that it is responsible for generating its annual operating budget without contributions from the City's General Fund. Currently, the operations are funded in part by contributions from airlines utilizing Sunport International Airport, but this arrangement is not sustainable for the long-term viability of the DEII Airport. With approximately 4,135 acres, DEII has adequate space for both aviation and non-aviation development. The Site Plan – EPC contains lease agreement areas intended to facilitate future development that will generate revenue to create a financially independent and sustainable DEII. The associated Design Standards will set the development parameters for new development on the site.

Proposed Site Plan - EPC:

The Applicant is requesting approval of the Double Eagle II as-built Site Plan - EPC and proposed Design Standards to meet the requirements of the NR-SU zone. The Site Plan-EPC (Site Plan) documents the existing conditions and provides a base map for as-built conditions while the Design Standards establish a framework to guide future growth. As mentioned above, the current DEII Master Plan lacks a comprehensive site plan that reflects conditions on the ground. The Airport Layout Plan contained in Chapter 6 of the approved Master Plan has up to now been utilized as the governing site plan. Initial consultations with City Planning about amendments to the Master Plan determined that the Airport Layout Plan lacked sufficient detail about existing development at DEII and a more comprehensive Site Plan was needed prior to approval of new developments on the property. Furthermore, the Airport Layout Plan did not establish Design Standards outside of compliance with FAA safety requirements. This as-built Site Plan – EPC and its associated Design Standards was the solution to documenting the existing development and meet the requirements

established by the site's NR-SU zoning and are the subject of this request. In order to ensure that future staff is properly able to determine which standards fall under the purview of the associate Design Standards vs the IDO, a table documenting application standards for future development was added to the overall site plan sheet. A copy of that table is provided below:

Table of Applicable Standards For Development on NR-SU Zoned Property	
Allowable Uses	Per DEII Master Plan
Use-Specific Standards	None Apply
Dimension Standards	Per DEII Design Standards
Site Design and Sensitive Lands	No Sensitive Lands Present
Access and Connectivity	Per IDO Section 5-3(E)(3)(c)
Subdivision of Land	Per IDO Section 5-4
Parking and Loading	None apply for Aeronautical Uses
Landscaping, Buffering, and Screening	Landscape per Landscape Plan and DEII Design Standards Buffering per DEII Design Standards Screening per DEII Design Standards and IDO Section 5-6(G) where silent
Walls and Fences	Per DEII Design Standards
Outdoor and site lighting	DEII Design Standards and IDO Section 5-8 where silent
Neighborhood edges	None apply
Solar access	Per IDO Section 5-10
Building Design	Per DEII Design Standards
Signs	Per IDO Table 5-12-3 for NR-GM where DEII Design Standards are silent
<i>FAA Regulations supersede all IDO standards where conflicts between the two exist.</i>	

This request for the approval of the DEII Site Plan - EPC and Design Standards complies with the criteria of IDO 6-6(I)(3)(a-g) Review and Decision Criteria for a Site Plan - EPC approval of the IDO as outlined below.

IDO Compliance:

6-6(I)(3)(a) The Site Plan is consistent with the ABC Comp Plan, as amended.

Applicant Response: The Double Eagle II - Site Plan - EPC request is consistent with the ABC Comp Plan, continuing the outlined goals and policies as follows:

P 4.2.2 Community Engagement: *Facilitate meaningful engagement opportunities and respectful interactions in order to identify and address the needs of all residents.*

- a) *Work with community leaders to identify and remove barriers to meaningful community engagement.*

Applicant Response: The Office of Neighborhood Coordination determined that there were no designated Neighborhood Associations within the required notification buffer areas. However, understanding the importance of public input, City Aviation made a concerted effort to inform neighbors, Neighborhood Associations/Coalitions, and other interested parties about the proposed as-built Site Plan and proposed Design Standards. City Aviation hosted a Public Open House, on June 20, 2024, to

provide project context in regard to the larger airport Master Plan, the proposed amendment and its status, and the purpose and specifics of this as-built Site Plans and the associated, proposed Design Standards. The meeting provided draft site plan sheets and copies of the draft Design Standards. Staff was available to answer any questions and to solicit feedback on the draft exhibits and standards. Subsequent to the Open House, the team reviewed the various comments received and revised the Design Standards as appropriate. In attendance included representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation, tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

Concerns voiced at the meeting revolved largely around the potential impact of future development on adjacent Major Public Open Space. Based on feedback, revisions included clearer provisions for screening of unsightly development and ensuring the use of view fencing for areas adjacent to Major Public Open Space. The proposed Design Standards also accommodate a larger building and parking setback for future development than required by the IDO. Documentation of the meeting is included in the application submittal.

G 5.1 Centers and Corridors: *Grow as a community of strong centers connected by a multi-modal network of Corridors.*

Applicant Response: The subject site is located within a Developing Employment Center along a Commuter Corridor, Atrisco Vista. With future development, the airport will likely attract more investment and generate jobs that will strengthen its role as an Employment Center. The Design Standards establish a consistent level of quality and help to strengthen the identity of the overall DEII property as a distinct Center with its own character. Further, the Site Plan facilitates infill development on vacant portions of an established Employment Center that can be accessed from Atrisco Vista. The proposed Design Standards also encourage connections between different developments on the property and access to shared amenities, reducing travel demand to other areas. This is particularly desirable on the west side of the city, as it helps improve the jobs and housing balance and does not contribute to the predominant commuter patterns of travel.

P 5.1.1 Desired Growth: *Capture regional growth in Centers and Corridors to help shape the built environment into a sustainable development pattern.*

- c) *Encourage employment density, compact development, redevelopment, and infill in Centers and Corridors as the most appropriate areas to accommodate growth over time and discourage the need for development at the urban edge.*

Applicant Response: DEII has been in operation since the 1980s and is a premium site for increasing employment given its access to the I-40 Commuter Corridor, the proximity to Paseo del Norte, and the proposed Paseo del Volcan extension. While the site is not in the urban core of the city, it is no longer at the urban edge but rather acts as a vital employment node on the west side of the City, reducing the jobs and housing imbalance west of the river. The DEII Design Standards encourage sustainable design for future development, particularly in terms of stormwater techniques and appropriate buffers to the adjacent open space.

P 5.1.2: Development Areas: *Direct more intense growth to Centers and Corridors and use Development Areas to establish and maintain appropriate density and scale of development within areas that should be more stable.*

Applicant Response: DEII is designated as an Employment Center and the vacant lands at DEII are designated as Areas of Change, while the runways and associated aviation uses are designated as Areas of Consistency. The vacant lands in DEII are ideal for more intense growth as desired by the Area of Change classification, due to their access via Atrisco Vista Blvd. Areas of Change are intended for higher intensity uses that can accommodate new residents and jobs. The as-built DEII Site Plan and its associated Design Standards are intended to ensure that future approved developments are established at an appropriate density and scale appropriate for areas adjacent to Areas of Consistency such as the adjacent Major Public Open Space. Standards establish building setbacks along Atrisco Vista Blvd right-of-way at 150 feet and lower maximum building heights within 250' of the ROW. More intense development would be sited on the interior of the property, away from Major Public Open Space. The Screening measures for parking along Atrisco Vista Blvd also ensure that new development has adequate screening and is setback from the road. The proposed Site Plan – EPC Design Standards maximum building height is significantly lower than the IDO AO- Airport Overlay District requirement of 150 feet. (85 feet vs. 150 feet, per IDO).

P 5.1.5 Employment Centers: *Create Centers that prioritize employment and foster synergy among businesses.*

- a) *Prioritize office and commercial employment in areas with good access via automobile and transit.*
- b) *Prioritize industrial employment in areas with good connectivity for freight routes.”*
- f) *Provide safe and convenient pedestrian and bicycle access to and mobility within Employment Centers.*

Applicant Response: DEII is located within a Developing Employment Center which is designed to prioritize uses associated with employment opportunities. The proposed DEII Design Standards are intended to provide cohesion between the mixed employment opportunities on the campus. Presently, much of the DEII land is undeveloped outside of the aviation areas. In the past, an approved Site Plan was established for the Aerospace Technology Park and the Eclipse Site Plan, north of Shooting Range Rd, but not the overall site. The SAMS building, located at 4100 Aerospace Pkwy is the only building within the DEII Boundary that was developed with a Site Plan and Design Guidelines. However, records are no longer available of the as-builts for this property due to the age of construction. The as-built DEII Site Plan establishes a framework for the entire property. Proposed Design Standards work to ensure cohesion between existing and future development and set the parameters for site infrastructure and elements on infill sites. Circulation and access standards minimize conflicts between the variety of transportation modes on site, while ensuring adequate access for freight from incoming commuter corridors. The proposed DEII Design Standards establish safe pedestrian and bicycle access and routes between existing and future lots.

G 5.3 Efficient Development Patterns: *Promote development patterns that*

maximize the utility of existing infrastructure and public facilities and the efficient use of land to support the public good.

Applicant Response: The Site Plan documents the extent of existing development and infrastructure so that future developments have a base upon which to assess the capacity of existing infrastructure and public facilities. The Design Standards promote efficient and compact development that leverages existing infrastructure, establishing a predictable growth pattern on designated parcels within an existing Center. Future development would submit Site Plans showing compatibility within the larger development framework through adherence to the Design Standards established request.

P 5.3.4 Conservation Development: *Encourage conservation development to promote private open space and preserve natural landscape, agricultural lands, and other features of the natural environment to encourage development that is sensitive to the open, natural character of the area and the geological and cultural conditions.*

- b) Encourage use of development envelopes to define areas in which buildings including accessory structures, landscaping, construction activity, walls and fences, and recreational activities are permitted.*

Applicant Response: Given the large area of DEII and the need to prioritize airport operations, the DEII Design Standards are intended to concentrate new development within a prescribed area and preserve the surrounding natural landscape. Presently there are no landscaping, screening or buffering standards in place. With the inclusion of the proposed Standards requiring landscaping along parking lots and building fronts, as well as perimeter wall screening, particularly for unsightly items like refuse areas or equipment storage, viewsheds are better protected. DEII had meetings with Petroglyph National Monument to discuss acceptable color schemes, and incorporated standards that utilize colors that blend with the surrounding natural environment; this includes earthtones that are appropriate for views from the Petroglyph National Monument. In general, colors for new development will take cues from the native New Mexican landscape palette, including but not limited to sand, terracotta, gray and charcoals, blues and teals, and sage green. With the proximity to the Petroglyph National Monument, the DEII Design Standards are intended to minimize the visual impacts of any new development by blending with the natural environment.

G 5.6 City Development Areas: *Encourage and direct growth to Areas of Change where it is expected and desired and ensure that development in and near Areas of Consistency reinforces the character and intensity of the surrounding area.*

Applicant Response: DEII is located within both a designated Area of Change and an Area of Consistency, as identified in the ABC Comp Plan. The vacant lands at DEII are designated as Areas of Change while the runways and associated aviation uses are designated as Areas of Consistency. Adjacent Major Public Open Spaces are designated as Areas of Consistency. Areas of Change are intended to accommodate employment opportunities and growth while Areas of Consistency are intended to be more stable. While the DEII Site Plan would allow designated lease areas to which direct future growth the Design Standards control the impacts of those developments on adjacent Areas of Consistency. DEII Design Standards ensure future development occurs in a manner that is aesthetically compatible with the general

aviation airport and the natural landscape. The proposed Standards ensure buffering between the more intensive uses of the airport, located in Areas of Change, and adjacent and adjoining Major Public Open Space, located in designated Areas of Consistency. The proposed Design Standards mitigates the impacts of future development on adjacent open space by establishing screening standards for unsightly uses adjacent to Major Public Open Space, , setting height setbacks in a certain proximity to public rights-of-way and Major Public Open Spaces, and building setback requirements from Major Public Open Spaces that exceed the IDO. It also sets standards for building design, such as approved building colors, that further reduce the visual impact on adjacent open space areas. As indicated earlier, the airport does not currently have adopted Design Standards as required by the NR-SU zone so this proposal offers more protections to adjacent Areas of Consistency than currently exist and these standards provide greater buffering than the IDO alone would provide.

P 5.6.2 Areas of Change: *Direct growth and more intense development to Centers, Corridors, industrial and business parks, and Metropolitan Redevelopment Areas where change is encouraged.*

- f) *Minimize potential negative impacts of development on existing residential uses with respect to noise, stormwater runoff, contaminants, lighting, air quality, and traffic.*
- h) *Encourage development in areas with a highly connected street grid and frequent transit service.*

Applicant Response: DEII Airport can help expand employment opportunities on the west side of the city with minimal impact to existing residential uses that are located more than a mile away to the east. The distance between the facility and established residential areas ensures their stability and helps minimize the impact of any new development. In addition, the proposed DEII Design Standards provide a variety of setback, buffering, and screening requirements intended to further minimize any potential impacts. While DEII Airport is not connected in a grid network to other parts of the City, there is the potential to generate sufficient economic activity and associated jobs to warrant some form of transit. As an airport, DEII provides air transport alternatives that may appeal to future developers which would negate the need for vehicle or transit miles necessary to reach the site.

G 5.7 Implementation Processes: *Employ procedures and processes to effectively and equitably implement the Comp Plan.*

Applicant Response: The DEII Site Plan and its associated Design Standards incorporate a variety of concepts that effectively implement the Comp Plan's Centers and Corridors vision. The as-built Site Plan designates lease areas that would direct future growth into an established Employment Center and Areas of Change and its associated Design Standards ensure that the impacts of that development will not negatively impact adjacent Areas of Consistency as outlined in our response to 5.2. The buffering standards are more stringent than what is currently specified in the IDO. Further, the as-built Site Plan and Design Standards bring the airport into compliance with the requirements of the NR-SU. The DEII Site Plan helps establish a more streamlined approval process that can be administered through the City's standard review process – DFT Administrative review.

P 5.7.4 Streamlined Development: *Encourage efficiencies in the development*

review process.

- a) *Encourage and facilitate meetings between developers and residents to identify and address issues prior to the official submittal of projects for approval.*

Applicant Response: DEII hosted a Public Open House June 20, 2024, to provide project context for the larger airport Master Plan, the purpose and specifics of this Site Plan-EPC request and the associated Design Standards. The meeting provided draft site plan sheets and copies of the draft Design Standards. Staff was available to answer any questions and to solicit feedback on the draft exhibits and standards. In attendance included representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation, tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature. Feedback received helped the Applicant clarify and establish standards that will help protect neighboring lands.

G 6.1 Land Use – Transportation Integration: *Plan, develop, operate, and maintain a transportation system to support the planned character of existing and future land uses.*

Applicant Response: DEII is within a Developing Employment Center along a Commuter Corridor, Atrisco Vista Blvd. The DEII Design Standards propose setbacks, maximum heights, and screening along Atrisco Vista and other rights-of-way that will support the desired character of the public roadways and the associated land uses.

P 6.1.3: Auto Demand: *Reduce the need for automobile travel by increasing mixed-use development, infill development within Centers, and travel demand management (TDM) programs.*

Applicant Response: DEII Airport patrons and commuters along Atrisco Vista Blvd currently have to drive miles to access amenities such as restaurants or retail. Future development at DEII Airport could reduce traffic demand by creating on-site amenities that reduce the need for patrons to travel outside of DEII Airport. The proposed Design Standards, have requirements pertaining to shared parking lots, cross access, and pedestrian walkways that could facilitate more of a mixed-use dynamic on the property. The as-built Site Plan will help future developers lay out development connections based on documented conditions and provide precedents to match the surroundings. The proposed Design Standards bring cohesion to future infill development. Travel demand management programs will be encouraged for future projects where there is a critical mass of employees commuting during peak demand hours.

P 6.2.10 Aviation: *Provide adequate accommodations for domestic travel, shipping, and military purposes.*

Applicant Response: The factors considered when evaluating appropriate development at DEII include noise level, overflight, safety zones as defined by the FAA, and Airspace Protection Overlay. The anticipated increase in aviation travel due to future development is expected to create a demand for lodging and restaurants which are presently not within proximity to DEII. The military currently uses DEII for training purposes and the proposed DEII Design Standards will not impact their

operations. The DEII Design Standards enforce general IDO and DPM standards, where feasible, for a mix of development types.

G 7.3 Sense of Place: Reinforce sense of place through context-sensitive design of development and streetscapes.

Applicant Response: The proposed DEII Design Standards would establish criteria for streetscaping along Double Eagle Rd to create a stronger sense of arrival and overall airport identity. The proposed Design Standards would also require tracts of land adjacent to Major Public Open Space to incorporate landscaping and screening into design to provide a visual buffer from sensitive viewsheds. Taken together, these measures would further the sense of place and context-sensitive design.

P 7.3.1 Natural and Cultural Features: Preserve, enhance, and leverage natural features and views of cultural landscapes.

- a) Minimize alteration of existing vegetation and topography in subdivision and site design.
- b) Provide appropriate transitions to Open Space

Applicant Response: The DEII Design Standards provide measures to help maintain the natural conditions of the land at DEII and adjoining Major Public Open Space. The Design Standards require building setbacks from major public rights-of-way and Major Public Open Space to create visual relief from building masses encroaching upon the distinct vistas associated with the area.. The standards also set acceptable building colors for future develop to ensure that new buildings are more harmonious with the surrounding natural features. The Design Standards also set building setbacks from Major Public Open Space areas that far exceed those of the IDO.

G 7.4 Context-Sensitive Parking: Design parking facilities to match the development context and complement the surrounding built environment.

Applicant Response: The Design Standards provide specific direction for parking areas, with allowances for the unique nature of an airport facility. The City's Aviation Department would review the design of most parking areas on DEII property to ensure compatibility with airport operations and compliance with FAA safety regulations. As per IDO requirements, parking would comply with Section 14-16-5-5 and the DPM where not otherwise specified. The DEII Design Standards would establish parking lot screening from Major Public Open Space, and other sensitive land uses. Permeable paving materials for parking areas are encouraged to minimize impervious areas and associated stormwater impacts.

P 7.4.3: Off-street Parking Design: Encourage well-designed, efficient, safe, and attractive parking facilities.

- a) Incorporate trees, vegetation, and pervious surfaces in parking areas to mitigate environmental impacts, minimize heat and glare, and improve aesthetics.
- b) Ensure safe pedestrian pathways in parking areas that connect to building entrances, adjacent roadways, and adjacent sites.

Applicant Response: The Design Standards would support safe circulation for future

developments, minimizing conflicts between vehicles, bicycles, and pedestrian pathways. Pervious surfaces are encouraged for parking areas and pedestrian pathways. Landscaping, where not in conflict with FAA safety regulations or exceptions, are proposed in locations to create a sense of presence, identity, and aesthetic interest. The City's Aviation Department would be responsible for designing delivery routes for the aviation areas of the property and all tenants would be required to submit commercial delivery routes to DEII for prior approval. The Design Standards propose a minimum six (6) foot clear pedestrian pathway to be provided from the primary entrance of the primary buildings to larger site circulation systems and to adjacent public rights-of-ways as feasible.

G7.5 Context-Sensitive Site Design: *Design sites, buildings, and landscape elements to respond to the high desert environment.*

Applicant Response: The terrain of DEII and surrounding area of the West Mesa is arid with natural vegetation and views of the Petroglyph National Monument Volcanos and the Sandia Mountains. Because of this, the DEII Design Standards prioritize maintaining natural landscape wherever possible and implementing sustainable practices for stormwater control, and permeable surfaces. When located next to Major Public Open Space, the DEII Design Standards utilize colors that blend with the surrounding natural environment and generally include yellow ochres, browns, dull reds, and grey greens. Design Standards would require a light reflective value (LRV) that is compatible with FAA safety regulations.

P 7.5.1 Landscape Design: *Encourage landscape treatments that are consistent with the high desert climate to enhance our sense of place.*

- a) *Design landscape and site improvements to complement the individual site, the overall appearance of the corridor, and surrounding land uses.*
- b) *Design landscapes and vegetation to be consistent with the microclimate of the site location as well as within the site.*

Applicant Response: Where not in conflict with FAA safety regulations or aviation functionality, the DEII Design Standards encourage passive water harvesting techniques to reduce potable water use and reduce stormwater runoff. The Design Standards encourage the use of native or regionally adapted plant materials that will thrive in local conditions with less fertilizer, water, and maintenance.

G 11.3 Cultural Landscapes: *Protect, reuse, and/or enhance significant cultural landscapes as important contributors to our heritage and rich and complex identities.*

Applicant Response: DEII Airport is located within proximity to the City of Albuquerque Major Public Open Space and Petroglyph National Monument. The proposed DEII Design Standards protect the cultural landscapes by requiring natural vegetation to be protected and take into consideration these protective viewshed standards outlined in the Major Public Open Space section of the IDO, where feasible. The heritage and rich and complex identity of the Petroglyph National Monument was of utmost concern when establishing maximum height requirements that exceeded the APO Overlay District and setback and buffering requirements within 330' of the Major Public Open Space and Edges.

P 11.3.1 Natural and Cultural Features: *Preserve and enhance the natural and*

cultural characteristics and features that contribute to the distinct identity of communities, neighborhoods, and cultural landscapes.

- a) *Minimize the visibility of structures in highly scenic areas and on the western horizon as seen throughout the city through building design and materials that blend with the natural colors of the landscape and limit reflectivity.*

Applicant Response: The Design Standards associated with this request aim to minimize the visibility of structures and protect the viewsheds from Major Public Open Space by establishing large setbacks that exceed those currently dictated by the IDO paired with building height restrictions within a certain distance of public rights-of-way or Open Space to retain views. The DEII Design Standards further establish restrictions on color, building materials that are highly reflective so that future buildings blend more holistically into their surrounding natural setting. The standards also establish screening standards that ensure orientation or screening of more utilitarian areas from adjacent Major Public Open Spaces. All of these standards will preserve and enhance the natural characteristics and features of the surrounding areas.

P 11.3.4 Petroglyph National Monument: *Regulate adjacent development to protect and preserve the Petroglyph National Monument – its volcanoes, petroglyphs, and Northwest Mesa Escarpment – as a priceless cultural landscape and community resource that provides physical, cultural, and economic benefits.*

- a) *Preserve and protect the Monument from growth and development pressures on the West Side.*
- b) *Conserve and protect the Monument and surrounding lands through regulations associated with the Volcano Mesa and Northwest Mesa Escarpment Areas.*
- c) *Minimize visual impact of adjacent development through design standards related to color, reflectivity, building materials, and screening.*
- d) *Protect views to and from the black Escarpment face, which gives physical order to the community and acts as a visual reference point.*
- e) **Encourage appropriate edge treatments, transitions, and buffers through site design and development.**

Applicant Response: The DEII Design Standards are intended to minimize negative impacts on the Petroglyph National Monument. All future developments adhering to the DEII Design Standards will be designed with the intent to protect and preserve the Petroglyph National Monument and nearby Major Open Public Space. DEII Design Standards considered IDO Subsection 5-2(J) Major Public Open Space Edges, related to color, reflectivity, building materials and screening, to ensure minimal visual impact on the Petroglyph National Monument. Future site design and development will provide appropriate edge treatments and buffers to further protect and preserve the Petroglyph National Monument and adjacent Major Public Open Space. These protections include maximum building heights, screening, landscaping and buffer requirements, site lighting limits, wall and fencing requirements. To the extent feasible, development would be setback farther than required by the IDO from Atrisco Vista Blvd to minimize negative impacts to Petroglyph National Monument.

6-6(I)(3)(b) The Site Plan is consistent with any applicable terms and conditions in any previously approved NR-SU or PD zoning covering the subject property and any related development agreements and/or regulations.

Applicant Response: The DEII Site Plan – EPC and Design Standards are consistent with the terminology and conditions of the NR-SU Zone District. Given that the subject property is an airport, any site plan must comply with FAA regulations and applicable City standards. The as-built DEII Site Plan established built context and the proposed Design Standards help establish a baseline for review of proposed developments within DEII Airport. Where not stated otherwise, IDO standards apply.

6-6(I)(3)(c) The Site Plan complies with all applicable provisions of this IDO, the DPM, other adopted City regulations, and any terms and conditions specifically applied to development of the property in a prior permit or approval affecting the property

Applicant Response: The as-built DEII Site Plan documents existing conditions and facilities that have been built over the course of decades. All prior developments at DEII have been reviewed by the City and by FAA for compliance with relevant regulations, at the time of development. Most of the DEII Airport facilities were built prior to the IDO and under a different zoning district and or zoning code and thus are likely not in compliance with current standards. This DEII Site Plan and the accompanying Design Standards help establish a baseline for future developments or renovations, which would be thus required to comply with the Design Guidelines or IDO, where silent. Given the specialized nature of an airport, FAA regulations apply to any existing or proposed development, as well as applicable IDO and DPM City regulations. The proposed DEII Design Standards comply with all applicable provisions of the IDO and DPM, where feasible. If the as-built DEII Site Plan and Design Standards are approved by EPC, the DFT would in turn review the Site Plan for final sign-off. The Design Standards establish that any modifications to existing development within the DEII Airport are subject to IDO standards, where the Design Standards are silent.

6-6(I)(3)(d) The City's existing infrastructure and public improvements, including but not limited to its street, trail, drainage, and sidewalk systems, have adequate capacity to serve the proposed development, and any burdens on those systems have been mitigated to the maximum extent practicable

Applicant Response: The subject site is served by existing infrastructure. The as-built DEII Site Plan and proposed Design Standards do not immediately impact infrastructure as no site-specific development plans are proposed as of the date of this application. Any future site-specific developments would have to demonstrate that there is adequate capacity to serve the site and mitigate any impacts to the City's infrastructure/public improvements.

6-6(I)(3)(e) The application mitigates any significant adverse impacts on the project site and the surrounding area to the maximum extent practicable.

Applicant Response: The as-built DEII Site Plan and proposed Design Standards have two main objectives: ensure that existing and future development at DEII maintain the viability of the airport while at the same time minimizing the impact on the surrounding area. The proposed DEII Design Standards are intended to work in concert with FAA requirements to ensure compatibility with aeronautical uses while also providing the appropriate setbacks, screening, and buffers to minimize the impact on the surrounding area.

6-6(I)(3)(f) If the subject property is within an approved Master Development Plan, the Site Plan meets any relevant standards in the Master Development Plan in addition to any standards applicable in the zone district the subject property is in.

Applicant Response: The property is located within the Double Eagle Master Plan which only has standards specific to FAA safety regulations. No other standards are established within the DEII Master Plan. The as-built DEII Site Plan and proposed Design Standards are required, per IDO standards, for NR-SU zoned properties. The property is also located within the APO Overlay district and borders to the east the VPO-2 Overlay Zoning District. The as-built DEII Site Plan establishes a baseline for existing development and the proposed Design Standards will guide future development, while also complying with FAA safety regulations and adhering to the functionality of general aviation use.

6-6(I)(3)(g) If a cumulative impact analysis is required in the Railroad and Spur Small Area pursuant to Subsections 14-16-5-2(E) (Cumulative Impacts) and 14-16-6-4(H) (Cumulative Impacts Analysis Requirements), the Site Plan incorporates mitigation for all identified cumulative impacts. The proposed development will not create material adverse impacts on water quality or other land in the surrounding area through increases in traffic congestion, parking congestion, noise, vibration, light spillover, or other nuisances without sufficient mitigation or civic or environmental benefits that outweigh the expected impacts.

Applicant Response: The subject property is not within a Railroad and Spur Small Area.

Conclusion:

Based upon the rationale presented in this letter, we respectfully request approval of Double Eagle II Site Plan - EPC and Design Standards. If you have any questions or need clarification of anything contained herein, please contact me at willg@dpsdesign.org or at (505) 761-9700.

Sincerely,



Will Gleason, AICP
Principal, Dekker
Agent for Aviation Department

July 31, 2024

To: Catherine Heyne, Planner
Megan Jones, Principal Planner
City of Albuquerque Planning Department

From: Dekker, Will Gleason, Jessica Lawlis, Rebecca Shank

RE: Double Eagle II (DEII) Site Plan – EPC (PR #2018-001577, Case # SI-2024-00994)

Dear Ms. Heyne,

This letter outlines the modifications that have been made to the DEII Site Plan – EPC application to address the staff comments received July 25, 2024 (Note: there is a discrepancy in the letter as it is dated April 25, 2024). Staff comments are in bold and applicant response in italics.

Discrepancies in the Legal description between this and the Master Plan applications.

The project team reviewed the excel comparison file provided by COA staff and confirm the following parcels and legal descriptions apply, changes are shown in red from previous submittal:

Parcels	UPC #	Legal Description	Acreage
1.	100606212040020101L1	TR A-1 PLAT OF TRACT A-1 & TRACT L-1 PARCELS 1-5 DOUBLEEAGLE II AIRPORT CONT 3196.8609 AC	3196.8609
2.	100606111049520101	TR F BULK LAND PLAT OF DOUBLE EAGLE II AIRPORT ANDADJACENT LANDS CONT 51.6910 AC	51.691
3.	100606027744410145	TR S-2 BULK LAND PLAT FOR AEROSPACE TECHNOLOGY PARKTRACTS D & S OF DOUBLE EAGLE II AIRPORT AND ADJACENTLANDS CONT 90.3889 AC	90.3889
4.	100606008144720144	TR S-1-A PLAT OF TRACTS D-1-A-1, D-1-A-2 & S-1-A OF AMENDED BULK LAND PLAT FOR AEROSPACE TECHNOLOGY PARKCONT 101.9724 AC	101.9724
5.	100406351009540116	TR K BULK LAND PLAT OF DOUBLE EAGLE II AIRPORT ANDADJACENT LANDS CONT 352.4366 AC	352.4366
6.	100606112712430102	TR D-1-A-2 PLAT OF TRACTS D-1-A-1, D-1-A-2 & S-1-A OFAMENDED BULK LAND PLAT FOR AEROSPACE TECHNOLOGY PARK CONT 5.0406 AC	5.0406
7.	100406444017540112	TR N-1 BULK LANDS PLAT OF TRACTS N-1, O-1 & N-2 PARCELS1, 2, 3 & 4 DOUBLE EAGLE II AIRPORT AND ADJACENT LANDSCONT 33.5154 AC	33.5154
8.	100506333252510101	S'LY PORT OF PARCEL 1 PLAT OF TRACT A-1 & TRACT L-1 PARCELS1-5 DOUBLE EAGLE II AIRPORT CONT 18.9764 AC	18.9764

9.	100506347052410102	S'LY PORT OF PARCEL 2 PLAT OF TRACT A-1 & TRACT L-1 PARCELS1-5 DOUBLE EAGLE II AIRPORT CONT 18.4392 AC	18.4392
10.	100606306752020101	S'LY PORT OF PARCEL 3 PLAT OF TRACT A-1 & TRACT L-1 PARCELS1-5 DOUBLE EAGLE II AIRPORT CONT 16.4607 AC	16.4607
11.	100606320251920102	S'LY PORT OF PARCEL 4 PLAT OF TRACT A-1 & TRACT L-1 PARCELS1-5 DOUBLE EAGLE II AIRPORT CONT 13.7885 AC	13.7885
12.	100606329951620103	S'LY PORT OF PARCEL 5 PLAT OF TRACT A-1 & TRACT L-1 PARCELS1-5 DOUBLE EAGLE II AIRPORT CONT 6.7307 AC	6.7307
13.	100606129529510163	Tract C Bulk Land Plat of Double Eagle II Airport and Adjacent Lands	177.4651
14.	100606126008540101	Tract E Bulk Land Plat of Double Eagle II Airport and Adjacent Lands	51.0468
Total Acreage:			4134.8132

4) JUSTIFICATION LETTER

The project letter is off to a good start, though we suggest:

- **Adding the request to the introduction. Why will this be heard at the EPC?**
The revised justification letter submitted as part of the supplementary materials that directly respond to the deficiency memo received on July 25th, dated April 25, included language to why this request will be heard at the EPC.
- **This is not a Master Plan Amendment, but an as-built Site Plan. Please amend this throughout the justification, e.g., Section: Proposed Amendment, and document as a whole.**
All Master Plan Amendment references were replaced with as-built Site Plan and proposed Design Standards.
- **All references to Site Uses that were part of the recent text amendment should be removed.**
All use reference and text amendment references removed.
- **The discussed Design Standards should be those currently in place in addition to the proposed design standards. Please make it clear what is existing versus what is proposed as part of the NR-SU Design Standards.**
Added language that there are no Design Standards in place except for those applicable to FAA safety regulations. Where IDO overlay regulations apply, information added that we comply or exceed their requirements.
- **Please more specifically explain that this is an NR-SU Site Plan, which is why the EPC is hearing this request. See IDO §14-16-2-5(E).**
 - Add a table and explanation of where IDO standards apply and do not apply. Please let us know if you would like to see an example.
 - Explain how these fit into the framework of Adjacency to MPOS.

The applicant added a table to the overall Site Plan sheet, SDP1.1, that summarizes were the Design Guidelines associated with the NR-SU Site Plan supersede the IDO or where the IDO governs. An explanation of how the guidelines deal with the adjacency to MPOS was also added to the revised Justification Letter submitted with the supplementary materials on July 31st, 2024.
- **Please add legends to Figures 3 (Surrounding Areas) and 4 (Major Public Open Space).**
The revised Justification Letter includes legends for Figures 3 and 4.

5) NOTIFICATION & NEIGHBORHOOD ISSUES

Notification requirements for a Major Amendment are explained in Section 14-16-6-4(K) Public Notice (IDO, p. 409). The required notification consists of: (1) an offer of neighborhood meeting (6-4(B)), (2) An emailed letter to neighborhood representatives indicated by the ONC (6-4(K)(2)), (3) a mailed letter (first-class) to property owners within 100 feet of the subject site (6-4(K)(3)), and (4) a yellow sign posting (6-4(K)(4)).

- A. According to the ONC, there are no affected registered neighborhood organizations.
- B. Property owners within a 100 ft buffer were notified as required, however, since this was presented as “The request is for an amendment to the existing Master Plan (i.e., Development Plan) for Double Eagle II Airport.”, which it is not, property owners shall be re-notified with the correct information.

The applicant mailed revised Notice Letter, forms, and site plans that indicated the correct development application request and stated that the previous notice was incorrect. The applicant sent the revised notifications via certified mail sent July 29, 2024. Provided in Resubmitted Documents. Proof of the revised mailing is included with the supplementary materials provided to staff along with this letter on July 31st, 2024.

- C. The West Side Coalition of Neighborhood Association was notified about and invited to a DEII Open House held June 20, 2024. The stated reason was for an “upcoming EPC hearing on a proposed amendment to the airport’s Master Plan”. Please send the WSCONA an updated email with the correct application request, just to inform them on the change since it is different from the original meeting.

The applicant sent an email to the Wes Side Coalition of Neighborhoods July 30, 2024 to clarify the error in the application type and that the application is actually a Site Plan - EPC. Proof of the revised mailing is included with the supplementary materials provided to staff along with this letter on July 31st, 2024.

- D. The sign posting period is 15 days prior to and after the August 15, 2024 EPC hearing date, not counting that date: Wednesday, July 31, 2024 to Friday, August 30, 2024. The Sign Posting Agreement was included, but needs to be dated and signed.

The applicant has dated and signed the Sign Posting Agreement. The applicant posted signs on July 31, 2024. Proof of sign posting will be provided to staff.

6) REVIEW AND DECISION CRITERIA

The Site Plan – EPC shall be approved if it meets all of the criteria presented in 6-6(I)(3)(a-g). Please amend responses in a revised Justification Letter based upon the points below.

A. Responses to 6-6(I)(3)(a):

1. The task in a justification is to choose applicable Goals and Policies from the Comprehensive Plan that directly relate to the circumstances of the subject site and demonstrate how the request is consistent with (makes a reality) each applicable Goal and Policy.
 - Re-phrasing the requirement itself in the response by tailoring the response to match the wording of the Goal or Policy.
 - It is also important not to simply restate the Goal, Policy, or Sub-policy, but make supporting statements as to how allowing additional uses would be consistent with the presented Goals/Policies.
 - Please read through your responses to the criteria and be confident that you have provided sound justification for the proposed amendment and tie it back to your request and reasoning. To strengthen all provided responses, it will help to include the language directly from the goals and/ or policies:

2. **All mention of or reference to recent Master Plan text amendments shall be removed and updated with the correct request for an NR-SU site plan because there is no site plan on file for the airport (required per the IDO).**
All reference to recent Master Plan text amendment have been removed and corrected with responses for a NR-S site plan.
 3. **Is Aviation still requesting the Design Standards as part of this request? That is the purpose of NR-SU, but all references to new uses that have not been approved by City Council shall be removed.**
Yes, Design Standards are part of this request and all references to unapproved use in the Standards and Justification Letter have been removed.
 4. **After review of the provided materials, was the Open House a presentation of the as-built Site Plan or future development ideas and Design Standards? Please clarify throughout.**
The Justification Letter has been updated to more clearly reflect the Open house discussions that provided information on the as-built Site Plan and draft proposed Design Standards.
 5. **The request documents an as-built Site Plan. Several included goals and policies don't apply to this request, and therefore, are not applicable and do not need to be included, e.g., Goal 6.2 and Policies 4.1.5 and 5.7.2.**
Goal 6.2 and Policies 4.1.5 and 5.7.2 have been deleted from Letter
 6. **Goal 4.2 speaks to community engagement. The current response is insufficient.**
Goal 4.2 has been revised to provide more detail on community engagement.
 7. **The response to policy 5.1.2 seems to imply that the DEII is entirely in an Area of Change. Please update.**
The Justification Letter was updated to reflect that the Area of Change pertains to the vacant areas of DEII boundary only.
 8. **Responses to at least Goals 5.6, 7.3, 11.3 and Policy 6.1.3 need more elaboration in regards to how the stated changes specifically apply to the as-built Site Plan. See Point 1 above.**
The revised Justification Letter elaborates where applicable.
- B. Response to 6-6(l)(3)(b)**
1. **This should be an as-built Site Plan. Please update the response accordingly.**
Any reference to the application type in the Justification Letter have been updated to indicate as-built Site Plan.
- C. Response to 6-6(l)(3)(c)**
1. **Most, if not all, DEII Airport facilities were built prior to the IDO under a different zoning code so it may not be up to the current standards.**
 2. **The DFT will review the full Site Plan for the final sign-off if the request is approved by the EPC.**
The Justification Letter was updated to reflect that DEII was developed in 1980's and therefore predated the IDO and existing may not comply to its standards, however future developments must comply with the IDO or the Design Standards, where specified.
- D. Response to 6-6(l)(3)(d)**
1. **Please incorporate text that indicates that the proposed Site Plan documents the existing, as-built facilities.**
The Justification Letter was updated accordingly to indicate the Site Plan documents existing, as-built facilities.
- E. Response to 6-6(l)(3)(e)**
1. **This should be an as-built Site Plan. Please update the response accordingly.**

The Justification Letter was updated accordingly.

F. Response to 6-6(l)(3)(f)

- 1. This should be an as-built Site Plan. Please update the response accordingly.**

The Justification Letter was updated accordingly.

7) SENSITIVE LAND ANALYSIS FORM

- **Pages seem to be missing from the included Sensitive Lands Analysis Form; the last answer in the table of the submitted form seems to be cut off.**
 - *SLA document attached to Resubmittal Documentation.*
- **Attachments indicated in the Other Details column responses were not found within the submitted Application.**
 - *SLA document attached to Resubmittal Documentation, single page document.*

8) SITE PLAN OVERVIEW – PROVIDED DRAWINGS

- A. These plans document as-built facilities.**
- B. All sheets should reference the Site Plan-EPC, not Master Plan.**
- C. The included site plan is incomplete per the Site Plan Checklist/requirements per the NR-SU zone district. We cannot locate any details sheets (lighting, signage, etc.)**

The following details sheets were added to the plan set:

- *AE 3.24 -Refuse Enclosure Details*
- *AE 3.25- Free Standing Signage Details*
- *AE 3.26- Lighting Details*
- *AE 3.27- Fencing, Wall Enclosure Details*

These new sheets were submitted to staff on July 31st, 2024 as part of the supplementary materials that accompany this letter.

- D. In addition to the proposed Design standard sheets, please indicate on the site plan sheet where IDO standards are being followed. Remember, where the design guidelines are silent, the IDO applies.**

- **B.1. Proposed Development – Structural**
 - **Please provide the following:**
 - **Walls, fences, and screening: can you provide the height of the fence and fence type with a list of color and materials**
AE 3.27- Fencing, Wall Enclosure Details sheet includes the height of the fence, the fence type and the materials.
 - **Square footage needs to be included for each structure**
Square footages for each structure where already located on the Facility Index on sheet SDP 1.2.
 - **Dimensions, color, and materials should be included for walls, fences, and screening; e.g., see AE3.23 – this can be in a table format**
Sheet AE 3.27- Fencing, Wall Enclosure Details documents existing fencing.
 - **Site Lighting: Please provide any existing site lighting with dimensions, colors, and materials, light fixture type. We see lighting in the provided elevation photos, but no details for the actual lights.**
AE 3.26- Lighting Details sheet provides details of the light fixture type.
- **B.2. Parking, Loading and Internal Circulation – Vehicular Circulation. Why is this not applicable? Please provide the existing parking, loading and circulation patterns on site (ingress, egress locations).**
SDP 1.2 and SDP 1.3 sheets show Internal Vehicular Circulation. SDP 1.3 sheet shows the unloading and unloading area.

- **B.2. Parking, Loading and Internal Circulation – Pedestrian Circulation. Why is this not applicable? Please add any existing cross walks or sidewalks or verify that there are not any. Are there any existing bike racks?**

There are no known bikes racks on the campus to indicate.

- **B.3. Streets and Circulation. Please provide all ingress and egress locations, drive aisles, etc. per Site Plan Checklist.**

As per Site Plan Checklist- SDP 1.2, SDP 1.3 provides the drive aisles.

- **Sheet #3 - Grading and Drainage**

- **A separate Grading and Drainage Plan should be part of the packet.**

Why was one not included for existing conditions?

- **The plan will be reviewed by the development facilitation team and shall be in compliance with the DPM. The DFT will require it.**

The applicant was requested to provide known as-built conditions for a development that was constructed in the 1980s. After thorough review, the applicant nor the city has not been able to locate any previously approved Grading and Drainage Plans as they appear to not have been properly documented at the time of the airport's construction. Requesting that the applicant attempt to document grading and drainage improvements that have been in place for nearly 40 years, particularly when the site isn't exhibit grading or drainage issues, is severely burdensome. Such a request is also not proportionate to the applicant's initial request to amend to overall airport Master Plan to expanded permissive uses without moving forward new development requests. Any future development, aeronautical or non-aeronautical that would be facilitated by the proposed Master Plan amendment, will be required to provide Grading and Drainage Plans and added to this Site Plan set. It should not the be burden of this application to retroactively provide an as-built grading and drainage plan. Language has been added to the site plan sheets that indicated that the burden of proving compliance with existing regulations will be on future applicants.

- **Sheet #4 - Utility Plan**

- **A separate Utility Plan should be part of the packet. Why were existing utilities not provided?**

- **The plan will be reviewed by the development facilitation team and shall be in compliance with the DPM.**

Similar to the Grading and Drainage Plan, neither the applicant nor the city has not been able to locate any previously approved Utility Plans as they appear to not have been properly documented at the time of the airport's construction 40 plus years ago. Requesting that the applicant attempt to document utility improvements that have been in place for approximately 40 years, particularly when the site isn't exhibit utility issues, is severely burdensome and not proportionate to the applicant's request. Any future development, aeronautical or non-aeronautical that would be facilitated by the proposed Master Plan amendment, will be required to provide Utility Plans that will be added to this Site Plan set. It should not the be burden of this application to retroactively provide an as-built utility plan. Language has been added to the site plan sheets that indicated that the burden of proving compliance with existing regulations will be on future applicants.

- **Sheet #5 - Building and Structure Elevations**

1. General Information

- a. **Dimensions of façade elements, including overall height and width should be consistent through the drawings; e.g., building heights found on AE3.16, 3.19, 3.21 do not seem to represent structure. Please double check building dimensions.**

- b. **Please see measurement definitions (Building Height) in the IDO (pg. 577).**
- c. **Materials and colors of all building elements and structures are missing. A table/schedule of colors and materials for each building shall be provided.**
- d. **Location and dimensions of mechanical equipment is missing (Site Amenities, AE3.22)**

As noted above, the applicant was requested to provide known as-built conditions for a development that was constructed in the 1980s and not properly document at the time of its construction to the best of our ability. Multiple conversations between Planning staff and the applicant prior to submittal indicated that utilizing photographs to document building elevations would suffice. The information depicted in the drawings was document through field measurements utilizing tools lazer levels that have limitations. Requests for documenting as-built conditions for a development should have a rational nexus to the request that triggered this as-built site plan, an amendment to the governing Airport Master Plan that while expanding the lineup of permissive uses, did not move new development forward. Language has been added to the site plan sheets that indicated that the burden of proving compliance with existing regulations will be on future applicants.

2. Building Mounted Signs

- a. **The provided Site Plan Checklist suggest that there is no building mounted signage. See provided elevations and AE3.23: e.g., dimensioned, and all colors and materials should be provided for signage (Sign permits may be provided).**

Similar to our responses above, neither the applicant nor the city has not been able to locate any previously approved plans, elevations or permits for existing signage as they appear to not have been properly documented at the time of the airport's construction 40 plus years ago. Requesting that the applicant attempt to document signage improvements that have been in place for approximately 40 years, particularly when existing signage should be grandfathered in and isn't being expanded, is severely burdensome and not proportionate to the applicant's request. The building elevations have been revised to indicate existing building and site signage. The photos themselves document color and materials. Any future development, aeronautical or non-aeronautical that would be facilitated by the proposed Master Plan amendment, will be required to provide document compliance with current regulations and that document would be added to this Site Plan set. It should not be the burden of this application to retroactively show compliance of signage to current codes that weren't in place at the time of the site's construction. Future developments will be required to comply with current signage regulations. Language has been added to the site plan sheets that indicated that the burden of proving compliance with existing regulations will be on future applicants.

- o Design Standards
 - All language and references to site uses that were part of the recent text amendment should be removed (e.g., SD 3.1, SD 3.2).
 - Please revise language under D. Access and Circulation. This seems incomplete and/or is difficult to understand.

- e.g., “providing access to and from Interstate 40. To provide safe and appropriate transportation infrastructure the following guidelines shall govern.

We believe that the revised justification and supplementary materials have addressed staff concerns as reasonable. Please contact me if you have any questions or concerns and we look forward to further discussion at the EPC.

Sincerely,



Will Gleason, AICP
Principal, Dekker

***Attachments: Revised Justification Letter
Revised Site Plan / Elevation Sheets
Proof of Revised Notifications
Signed Signage Agreement
Sensitive Lands Analysis***

SIGN POSTING AGREEMENT

REQUIREMENTS

POSTING SIGNS ANNOUNCING PUBLIC HEARINGS

All persons making application to the City under the requirements and procedures established by the Integrated Development Ordinance are responsible for the posting and maintaining of one or more signs on the property which is subject to the application, as shown in Table 6-1-1. Vacations of public rights-of-way (if the way has been in use) also require signs. Waterproof signs are provided at the time of application for a \$10 fee per sign. If the application is mailed, you must still stop at the Development Services Front Counter to pick up the sign(s).

The applicant is responsible for ensuring that the signs remain posted throughout the 15-day period prior to any public meeting or hearing. Failure to maintain the signs during this entire period may be cause for deferral or denial of the application. Replacement signs for those lost or damaged are available from the Development Services Front Counter.

1. LOCATION

- A. The sign shall be conspicuously located. It shall be located within twenty feet of the public sidewalk (or edge of public street). Staff may indicate a specific location.
- B. The face of the sign shall be parallel to the street, and the bottom of the sign shall be at least two feet from the ground.
- C. No barrier shall prevent a person from coming within five feet of the sign to read it.

2. NUMBER

- A. One sign shall be posted on each paved street frontage. Signs may be required on unpaved street frontages.
- B. If the land does not abut a public street, then, in addition to a sign placed on the property, a sign shall be placed on and at the edge of the public right-of-way of the nearest paved City street. Such a sign must direct readers toward the subject property by an arrow and an indication of distance.

3. PHYSICAL POSTING

- A. A heavy stake with two crossbars or a full plywood backing works best to keep the sign in place, especially during high winds.
- B. Large headed nails or staples are best for attaching signs to a post or backing; the sign tears out less easily.

4. TIME

Signs must be posted from July 31, 2024 To August 15, 2024

5. REMOVAL

- A. The sign is not to be removed before the initial hearing on the request.
- B. The sign should be removed within five (5) days after the initial hearing.

I have read this sheet and discussed it with the Development Services Front Counter Staff. I understand (A) my obligation to keep the sign(s) posted for (15) days and (B) where the sign(s) are to be located. I am being given a copy of this sheet.

Will Gleason, Dekker (Agent)
(Applicant or Agent)

July 31, 2024
(Date)

I issued 4 signs for this application, July 31, 2024, Catherine Heyne, Planner
(Date) (Staff Member)

PROJECT NUMBER: PR-2018-001577 / SI-2024-00994

D) STAFF INFORMATION

April 25, 2024

TO: Will Gleason, DPS Design
Jessica Lawlis, DPS Design
Rebecca Shank, DPS Design

FROM: Catherine Heyne, Planner
Megan Jones, Principal Planner
City of Albuquerque Planning Department

TEL: (505) 924-3310, cheyne@cabq.gov; mdjones@cabq.gov (505-924-3352)

RE: Double Eagle II (DEII), Site Plan - EPC (PR # 2018-001577, Case # SI-2024-00994)

We've completed a first review of your submission for the DEII consolidated As-Built Site Plan for the airport under NR-SU zoning. We have a few questions and suggestions for the process. Following this review, we will be available to answer any questions. Please provide the following:

⇒ Revised Site Plan revisions discussed in the project memo (one hard copy of the site plan & one electronic copy)

by **9 AM on Tuesday, July 30, 2024.**

⇒ A revised application & justification letter (one electronic copy)

by **9 AM on Wednesday, July 31, 2024.**

Note: If you have difficulty with this deadline, please let us know.

1) Introduction

A. Although we've done our best for this review, additional items may arise as the case progresses. If so, we will inform you immediately.

B. Legal description

- This is what we have for the legal description based on the 2023 Text Amendment:

All or a portion of Tract A-1, Plat of Tract A-1 & Tract L-1, Parcels 1-5, Double Eagle II Airport; Tract C and Parcel-1 Tract M Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; the Southeasterly Portion of Tract D-1-A-1, Tract D-1-A-2, and Tract S-1-A Plat of Tracts D-1-A-1, D-1-A-2, & S-1-A of Amended Bulk Land Plat for Aerospace Technology Park; Tract E, F, and K, Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; Tract N-1 and Tract N-2 AKA Parcel 4 Bulk Land Plat of Tracts N-1, O-1, & N-2 PARCELS 1, 2, 3, & 4 Double Eagle II Airport & Adjacent Lands; and Tract S-2 Bulk Land Plat for Aerospace Technology Park, Tracts D & S of Double Eagle II Airport & Adjacent Lands, located at 7401 Paseo Del Volcan NW, NW of the Atrisco Vista Blvd NW and Shooting Range Access Rd intersection, approximately 4500 acres (the "subject site").

- C. It is our understanding that this request is for a Site Plan – EPC (IDO §14-16-6-6(I)) since there is no Site Plan on file for the DEII. The existing Airport is designated as an NR-SU zone district, as required. Therefore, it is required to have a site plan reviewed by the EPC with site standards negotiated at the EPC.
- For any current and/or future site updates, a Site Plan is required for the subject site to come into compliance with the IDO. All existing development shall be reflected in the proposed, as-built Site Plan drawings. Where the negotiated standards for the Site Plan is silent, IDO standards will apply (IDO § 14-16-2-5(E)(3)(c).
- D. The subject site lies within the Airport Protection Overlay Zone and associated Sub-areas, the Developing DEII Airport Employment Center, and West Mesa Community Planning Area.
- E. The subject site also includes an Area of Change (most undeveloped land) and an Area of Consistency (airport areas) per the Comprehensive Plan.
- F. The subject site is within 330' of the Major Public Open Space (MPOS) and Edges, therefore, standards for IDO § 5-2(J)- Major Public Open Space Edges apply.
- G. The reason the EPC is reviewing this request does not seem to be included in the justification and shall be added.

2) Process

- A. Information regarding the EPC process, including the calendar and current Staff reports, can be found at: <http://www.cabq.gov/planning/boards-commissions/environmental-planning-commission>
- B. Timelines and EPC calendar: the EPC public hearing for August is the 15th. Final staff reports will be available one week prior, on August 8th at: <https://www.cabq.gov/planning/boards-commissions/environmental-planning-commission/epc-agendas-reports-minutes>.
- C. You will be emailed a copy of all received Agency Comments by July 30th and we will forward any late ones to you. Comments from agencies requiring an action will be incorporated into the report as Conditions of Approval.

3) Application

- A. We received the provided Development Review Application Form.
1. In this case, individual zone atlas maps for the entire site are not necessary; the full map will suffice.

4) Justification Letter

The project letter is off to a good start, though we suggest:

- Adding the request to the introduction. Why will this be heard at the EPC?
- This is not a Master Plan Amendment, but an as-built Site Plan. Please amend this throughout the justification, e.g., Section: **Proposed Amendment**, and document as a whole.
- All references to Site Uses that were part of the recent text amendment should be removed.

- The discussed Design Standards should be those currently in place in addition to the proposed design standards. Please make it clear what is existing versus what is proposed as part of the NR-SU Design Standards.
- Please more specifically explain that this is an NR-SU Site Plan, which is why the EPC is hearing this request. See IDO §14-16-2-5(E).
 - Add a table and explanation of where IDO standards apply and do not apply. Please let us know if you would like to see an example.
 - Explain how these fit into the framework of Adjacency to MPOS.
- Please add legends to Figures 3 (Surrounding Areas) and 4 (Major Public Open Space).

5) Notification & Neighborhood Issues

Notification requirements for a Major Amendment are explained in Section 14-16-6-4(K) Public Notice (IDO, p. 409). The required notification consists of: (1) an offer of neighborhood meeting (6-4(B)), (2) An emailed letter to neighborhood representatives indicated by the ONC (6-4(K)(2)), (3) a mailed letter (first-class) to property owners within 100 feet of the subject site (6-4(K)(3)), and (4) a yellow sign posting (6-4(K)(4)).

- According to the ONC, there are no affected registered neighborhood organizations.
- Property owners within a 100 ft buffer were notified as required, however, since this was presented as “The request is for an amendment to the existing Master Plan (i.e., Development Plan) for Double Eagle II Airport.”, which it is not, property owners shall be re-notified with the correct information.
- The West Side Coalition of Neighborhood Association was notified about and invited to a DEII Open House held June 20, 2024. The stated reason was for an “upcoming EPC hearing on a proposed amendment to the airport’s Master Plan”. Please send the WSCONA an updated email with the correct application request, just to inform them on the change since it is different from the original meeting.
- The sign posting period is 15 days prior to and after the August 15, 2024 EPC hearing date, not counting that date: **Wednesday, July 31, 2024 to Friday, August 30, 2024.**
 - The Sign Posting Agreement was included, but needs to be dated and signed.

6) Review & Decision Criteria

The Site Plan – EPC shall be approved if it meets all of the criteria presented in 6-6(I)(3)(a-g). Please amend responses in a revised Justification Letter based upon the points below.

A. Responses to 6-6(I)(3)(a):

1. The task in a justification is to choose applicable Goals and Policies from the Comprehensive Plan that directly relate to the circumstances of the subject site and demonstrate how the request is consistent with (makes a reality) each applicable Goal and Policy.

- Re-phrasing the requirement itself in the response by tailoring the response to match the wording of the Goal or Policy.
 - It is also important not to simply restate the Goal, Policy, or Sub-policy, but make supporting statements as to *how* allowing additional uses would be consistent with the presented Goals/Policies.
 - Please read through your responses to the criteria and be confident that you have provided sound justification for the proposed amendment and tie it back to your request and reasoning. To strengthen all provided responses, it will help to include the language directly from the goals and/ or policies:
2. All mention of or reference to recent Master Plan text amendments shall be removed and updated with the correct request for an NR-SU site plan because there is no site plan on file for the airport (required per the IDO).
 3. Is Aviation still requesting the Design Standards as part of this request? That is the purpose of NR-SU, but all references to new uses that have not been approved by City Council shall be removed.
 4. After review of the provided materials, was the Open House a presentation of the as-built Site Plan or future development ideas and Design Standards? Please clarify throughout.
 5. The request documents an as-built Site Plan. Several included goals and policies don't apply to this request, and therefore, are not applicable and do not need to be included, e.g., Goal 6.2 and Policies 4.1.5 and 5.7.2.
 6. Goal 4.2 speaks to community engagement. The current response is insufficient.
 7. The response to policy 5.1.2 seems to imply that the DEII is entirely in an Area of Change. Please update.
 8. Responses to at least Goals 5.6, 7.3, 11.3 and Policy 6.1.3 need more elaboration in regards to how the stated changes specifically apply to the as-built Site Plan. See Point 1 above.

B. Response to 6-6(l)(3)(b)

- This should be an as-built Site Plan. Please update the response accordingly.

C. Response to 6-6(l)(3)(c)

- Most, if not all, DEII Airport facilities were built prior to the IDO under a different zoning code so it may not be up to the current standards.
- The DFT will review the full Site Plan for the final sign-off if the request is approved by the EPC.

D. Response to 6-6(l)(3)(d)

- Please incorporate text that indicates that the proposed Site Plan documents the existing, as-built facilities.

E. Response to 6-6(l)(3)(e)

- This should be an as-built Site Plan. Please update the response accordingly.

F. Response to 6-6(l)(3)(f)

- This should be an as-built Site Plan. Please update the response accordingly.

7) Sensitive Lands Analysis Form

- Pages seem to be missing from the included Sensitive Lands Analysis Form; the last answer in the table of the submitted form seems to be cut off.
- Attachments indicated in the Other Details column responses were not found within the submitted Application.

8) Site Plan Overview – Provided Drawings

- A. These plans document as-built facilities.
- B. All sheets should reference the Site Plan-EPC, not Master Plan.
- C. The included site plan is incomplete per the **Site Plan Checklist/requirements per the NR-SU zone district**.
 - We cannot locate any details sheets (lighting, signage, etc.)
- D. In addition to the proposed Design standard sheets, please indicate on the site plan sheet where IDO standards are being followed. Remember, where the design guidelines are silent, the IDO applies.

1. B.1. Proposed Development – Structural

- Please provide the following:
 - Walls, fences, and screening: can you provide the height of the fence and fence type with a list of color and materials
 - Square footage needs to be included for each structure
 - Dimensions, color, and materials should be included for walls, fences, and screening; e.g., see AE3.23 – this can be in a table format
 - Site Lighting: Please provide any existing site lighting with dimensions, colors, and materials, light fixture type. We see lighting in the provided elevation photos, but no details for the actual lights.

2. B.2. Parking, Loading and Internal Circulation – Vehicular Circulation

- Why is this not applicable? Please provide the existing parking, loading and circulation patterns on site (ingress, egress locations).

3. B.2. Parking, Loading and Internal Circulation – Pedestrian Circulation

- Why is this not applicable? Please add any existing cross walks or sidewalks or verify that there are not any. Are there any existing bike racks?

4. B.3. Streets and Circulation

- Please provide all ingress and egress locations, drive aisles, etc. per Site Plan Checklist.

5. Sheet #3 - Grading and Drainage

- A separate Grading and Drainage Plan should be part of the packet. Why was one not included for existing conditions?
- The plan will be reviewed by the development facilitation team and shall be in compliance with the DPM. The DFT will require it.

6. Sheet #4 - Utility Plan

- A separate Utility Plan should be part of the packet. Why were existing utilities not provided?
- The plan will be reviewed by the development facilitation team and shall be in compliance with the DPM.

7. Sheet #5 - Building and Structure Elevations

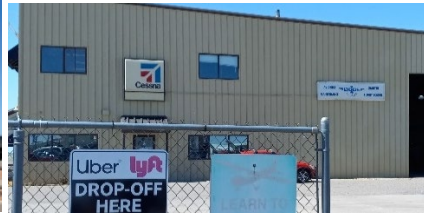
A. General Information

- Dimensions of façade elements, including overall height and width should be consistent through the drawings; e.g., building heights found on AE3.16, 3.19, 3.21 do not seem to represent structure. Please double check building dimensions.
 - Please see measurement definitions (Building Height) in the IDO (pg. 577).
- Materials and colors of all building elements and structures are missing. A table/schedule of colors and materials for each building shall be provided.
- Location and dimensions of mechanical equipment is missing (Site Amenities, AE3.22)

B. Building Mounted Signs

- The provided Site Plan Checklist suggest that there is no building mounted signage. See provided elevations and AE3.23:





etc.

- Although the signage is shown as part of the elevations, they need to be dimensioned, and all colors and materials should be provided for signage (Sign permits may be provided).

8. Design Standards

- All language and references to site uses that were part of the recent text amendment should be removed (e.g., SD 3.1, SD 3.2).
- Please revise language under *D. Access and Circulation*. This seems incomplete and/or is difficult to understand.
 - e.g., “providing access to and from Interstate 40. To provide safe and appropriate transportation infrastructure the following guidelines shall govern.

Additional Questions:

1. Area/Legal description – is the entire property included? It does not match previous text amendment submission
2. Are the mentioned approved Site Plans within the DEII Airport?
 - Aerospace Technology Park
 - Eclipse Site Plan (north of Shooting Range Rd)

August 1, 2024

TO: Will Gleason, Dekker
Jessica Lawlis, Dekker
Rebecca Shank, Dekker

FROM: Catherine Heyne, Planner, City of Albuquerque Planning Department
Megan Jones, Principal Planner, City of Albuquerque Planning Department

TEL: (505) 924-3310, cheyne@cabq.gov; mdjones@cabq.gov (505-924-3352)

RE: Double Eagle II (DEII), Site Plan - EPC (PR # 2018-001577, Case # SI-2024-00994)

Project Memo 2: Site Plan & Elevation Review Notes

As discussed, Site Plan revisions will be uploaded to the OneDrive folder as they are completed. All revisions should be uploaded by end of day Monday or

5 PM Monday August 5, 2024.

GENERAL COMMENT

- We noted that Structure T1 has been constructed since 2022. Could you provide additional case history?
 - Basis of SF estimate?

COVER SHEET

1. Add application # to cover sheet.
2. Right side title bar should include "EPC", i.e., "Site Plan – EPC" for all pages.

SDP1.1

1. The site is included on Zone Atlas page G5, please add.
2. Please add a General Note defining what a Lease Area is.
3. Land zoned NR-PO-C is shown but not labeled. Please update map, symbol, and legend.
4. Some property is defined by shading and others no shading. What is the difference?
5. Marked property lines are confusing. They do not always seem to indicate the actual property boundaries. Please clarify.
6. Add building outline to legend.
7. *Table of Applicable Standards for Aeronautical Uses on NR-SU Zoned Property*

- “None Apply” is too vague. Please further explain what that statement means.

SDP1.2

1. Why is there a T1 and not just a T?
2. The symbols for Improved Landscape Area and Reinforced Concrete Pad almost indistinguishable. Please make clearer.
3. The symbol for the shape under Sheet Keyed Notes 6 and as part of the adjacent parking lots does not seem to be in the legend. Please add.
4. There are light gray rectangle shapes to the NW of T1. Please clarify/add to legend. (This is also found on SDP2.1)
5. Is that a road that wiggles along the NW side of the plan drawing? Please clarify.
6. The fence symbol “14” looks like a taxiway and is not in the legend. Please clarify/add.
7. Please identify the dotted line immediately north of the Double Eagle Road and down the center of the road. Are these the same thing? There is also a solid line down the middle of what appears to be a road to the run way east central to the plan. Please explain/ add to legend.
8. Building Building “I” is surrounded by dots. Are these bollards? The legend and site plan do not seem to match.
9. Please label the bench area to the west of and abutting Building “F”.
10. Please identify the dot symbols to the NNW of Building “F”.
11. Sheet Keyed Notes 27 just south of Building “X” indicates a light fixture. Is this on the side of the building? Please add symbol.
12. The Cell Tower Symbol should be added to the Legend.
13. Building F: propane(?) tank seen in elevation photo is not depicted on sheet.
14. Please label fence(?) to SE of Buildings L and M as this symbol is not in legend (or does this mark a road edge?)

SDP2.1

1. Please identify the small dots to the NE and south of Building “A”.
2. Please add the symbols next to the unmarked building to the SW of Building “B” to the legend, and identify the unmarked structure.

SDP1.3

1. ~~Are the 8 slanted lines next to the Southwest Aeronautics, Mathematics and Science Academy benches? Please clarify.~~ = dimension marks
2. Do all of the features with horizontal lines represent Sheet Keyed Notes 12 (Concrete Curb Ramp)? Please clarify and/or add to Legend.
3. Sheet Keyed Notes 2 is situated next to a Light Fixture (Sheet Keyed Notes 27) on the west side of the Site Plan. Is there an arrow missing? Please clarify.

DESIGN GUIDELINE sheets (SD 4.1, SD 4.2):

1. All “should”s, “shall generally”s, “may” shall be changed to “shall”s. Otherwise, if not made a requirement, these lines of text are unnecessary since these are the regulations.
 - e.g., Section A. Introduction: “...the design standards *are assumed to apply*” change to “standards shall apply”
 - e.g., “A specific Landscape palette **may require** review “ → “...palette shall be reviewed”;
 - e.g., the K.9. line “Electric Vehicle Charging stations are encouraged and...” → “stations shall and”
2. Wireless Telecommunications Facilities (WTFs) are not considered structures and proposed Design Standards conflict with general WTF standards (e.g., MPOS (4-3(E)(12)(I)). The Master Plan specifies the IDO for WTF's and so should the Site Plan as to not undermine the wireless regulations. It is standard practice to request a Waiver to change these specifications. All mention of WTFs shall be removed (Section B, D, G. Building Height, Screening, etc.).
3. References to non-aeronautical development shall be removed at this time, e.g., B. 5.
4. ~~Parking Setback Standards in table for Atrisco Vista Blvd and MPOS seem to conflict. Please explain.~~
5. E. 3: Typo.
6. F.1.a: shall be removed at this time since it references the film studio use.
7. H.1. the indicated 15 feet is not consistent with the tables under Site Design and parking Setback Standards. Please update.
8. K. Landscaping –
 - a) K.1.b. still refers to non-aeronautical land uses, and more specifically, point iv. K.1.b. shall be removed as part of this site plan since non-aeronautical uses are not requested at this time.
 - b) K.7. Remove the extra “Landscape” in the second sentence.
9. M.7. & M.8. Language: will be necessary shall be updated to “is necessary”, “is required”.
10. Add a Process section that contains the instructions about “Subsequent site development plan requirements...” now found under the Design Standards in the background information.

ELEVATIONS

1. Indicate that images are not to scale, when applicable.
2. The site is included on Zone Atlas page G5, please add.
3. Right side title bar should include “EPC”, i.e., “Site Plan – EPC” for all pages.

4. For all page headers, the structure is identified as, e.g., “Building C-T Hanger”. Please update to something like, e.g., “Building C: T-Hanger” to better match Facility Index descriptions and reduce confusion.
5. As mentioned in Memo 1, materials and colors of all building elements and structures are missing. A table/schedule of colors and materials for each building shall be provided.
6. As mentioned in Memo 1, providing signage dimensions, colors, and materials should be provided. Sign permits may be provided.
7. Building F: propane(?) tank seen in photo is not indicated on SDP1.2
8. Building M, North Elevation: are these the fuel tanks shown on SDP1.2?
9. Building O1 and O2: the font looks like zeros and not “O”s. Please update.
10. Building R shows two North elevations. Please update.
 - Are the storage shed and additional room(?) noted in the “North” Elevation permanent structures?
 - G. Building Design point 7. indicates that all temporary buildings shall be removed after 1 year...
11. The outline of Building T1 in the north and south elevations does not seem to be rectangular as shown on SDP1. Please adjust.
12. Building W: Please add dimensions. Are the East and West elevations correctly labeled? Please recheck.
13. Building X: Are the West and South elevations correctly labeled? Please recheck.
14. Building T1 is relatively new (not seen in 2022 aerial photos. Can you provide history details of the process for construction of this building?
15. Building Z:
 - a) A building height measurement seems to be missing; provided dimensions indicate the same height for the one-story and extended building height. Please update.
 - b) Is the existing fencing depicted on the Site Plan?
 - c) What is the extent of the Obstacle Free Zone (Sheet Keyed Notes 32)? Is this what is depicted by the extended lines along the boundary of what looks like a off-road/ un-landscaped area?
16. Site Amenities (page AE3.23)
 - a) The Site Plan shows two Utility Structures. There is only one identified in the Site Amenities. Please show both with dimensions in the elevations.
 - Why are these not identified as facilities or given their own identification?
 - b) There are 3 “Fuel Farms” shown in the Site Amenities elevation. There are 2 identified on the Site Plan (“I” and “J”).
 - Please label accordingly. Also, please identify and show the location of the third on the Site Plan.

- Might one of these be the AVGAS ISLAND (Building K)? Please clarify.

17. Refuse Enclosures

- Is the enclosure labeled “South side of Driveway before DEII Main Gate” the area east of Building I and south of Building J? Please clarify.

18. Freestanding Signs

- a) The Main Gate does not seem to be identified on the Site Plan. Is this located on the “south” side of the Airside parking (a linear feature that indicates the presence of 4 signs)? Please verify and update.
- b) There are a total of 5 signs indicated on SDP1.2, four signs are identified on the Freestanding Sign page (AE3.25). Please update.

19. Walls, Fencing, and Screening

- a) Please provide an estimated height for the “Typical Chain Link Fence with Three Barbed Wires along Building V”

DEII Site Plan-EPC Memo 3

To: Will Gleason & Jessica Lawlis, Dekker

From: Megan Jones, Principal Planner & Catherine Heyne, Planner - CABQ Planning Dept

Date: 8/6/2024

In order to avoid a recommendation of deferral to the EPC, the following changes will be required prior to Thursday 8-8-2024 at 9 AM.

1. Updated Site Plan to Remove Aerospace Technology Park area

- The Aerospace Technology park Site Development Plan for Subdivision is a Valid Pre-IDO Site Plan that governs an approximately 300-acre portion of DEII, which includes the Eclipse technology site (see attached).
- Pursuant to IDO section 6-6(I)(1)(b) A Site Plan – EPC may be approved for property with a prior-approved Site Plan, regardless of whether the prior approved Site Plan is still valid pursuant to Subsection 14-16-6- 4(X), subject to allowable uses and development standards in this IDO. If any portions of the proposed boundary overlap with a prior-approved Site Plan that will remain in place, a Major Amendment shall be required as described in Subsection 14-16-6-6(I)(2)(d).
 - What this means is that the portion of the DEII Airport that has an approved pre-IDO site Development Plan for Subdivision has to be amended prior to approval of a new site plan that replaces it. This can occur after the EPC reviews and approves the As Built Site Plan for the Airport under NR-SU to have the proposed site plan and design guidelines supersede the Aerospace technology park or rescind the prior approval.
 - It seems that this portion of the site was not included in the boundary of the site plan, aside from two tracts.
- In order for a deferral from the August to September EPC hearing not to occur the following option is available:
 - The proposed NR-SU site plan shall be updated to include a note that the following two Tracts are “Not A Part of the request. Please refer to the Aerospace Technology Park Site Development Plan for Subdivision (PR 1003125 03EPC-02054 & 05EPC-00566) ” with the area crossed out. Please also include a
 - D-1-A-2 and Tract S-1A of amended bulk land plat for Aerospace Technology Park;
 - If the entire area is determined to be a part of the DEII Airport boundary, all tracts shall be included as “not a part”: Tract F Bulk Land Plat of Double Eagle II Airport and Adjacent Lands; Tract D-1-A-1, Tract S-1-A, and the SW'LY Portion of Tract D-1-A-1, Plat of Tracts D-1-A-1, D-1-A-2 & S-1-A of amended bulk land plat for Aerospace Technology Park; Tract D-1-A-2 and the Southeasterly portion of Tract D-1-A-1, Plat of Tracts D-1-A-1, D-1-A-2 & S-1-A of amended bulk land plat for Aerospace Technology Park; and LTS 1-15 Bulk Land Plat of lots 1 - 15 Aerospace Technology Park, Unit 1.
 - Please also remove the as built elevations and landscape plan for this site. The “As built” structure can remain on the site plan, crossed out.

- Aviation can determine next steps for the Aerospace Technology Park Major Amendment as part of the amendment to include the non-aeronautical land use on the Site Plan, after approval of the NR-SU Site Plan to be heard on August 15, 2024.
2. Staff still encourages the applicant to request a deferral from August to September to make this change (major amendment to the Aerospace Tech Park SDP for Subd.) and include the non-aeronautical land uses that were approved by City Council on 8-5-2024 for the prior text amendment instead of having to request a major amendment after approval of the NR-SU site plan to include the non-aeronautical land uses. Please let staff know by 9 AM on Thursday how aviation would like to proceed.

E) PUBLIC NOTICE

From: [Bloom, Misa K.](#)
To: [Rebecca Shank](#)
Subject: Re: Mailed Notice to Properties Owners for Double Eagle Airport 7401 Paseo Del Volcan
Date: Monday, July 1, 2024 11:13:13 AM
Attachments: [image001.png](#)
[Outlook-egptya5d.png](#)
[7401 Paseo Del Volcan.pdf](#)
[7401 Paseo Del Volcan 8.5x11 - Scale in Feet.pdf](#)
[7401 Paseo Del Volcan Labels.csv](#)
[7401 Paseo Del Volcan Labels.docx](#)

2024 July 01

Ms. Shank:

Good morning. Attached is your Buffer Map with all associated documents for the subject matter.

If you have any questions, please let me know. Thank you and have a wonderful day.

Respectfully submitted,



MISA K. BLOOM

(she / hers)

planner

urban design & development

o 505.924.3662

e mbloom@cabq.gov

cabq.gov/planning

Confidentiality Notice: This e-mail and any documents attached hereto are intended for the confidential use of the named recipient(s) only and are intended, to the fullest extent permitted by law, to be privileged and confidential as an attorney-client communication and/or work product. If you are not an intended recipient, or an agent responsible for delivering this communication to an intended recipient, you have received this e-mail in error. If you have received this email in error, please (i) immediately notify me by replying to this e-mail; (ii) do not review, copy, save, forward, or print this e-mail or any of its attachments; and (iii) immediately delete and/or destroy this e-mail and its attachments and all copies thereof. Thank you for your cooperation.

From: Rebecca Shank <RebeccaS@dpsdesign.org>

Sent: Monday, July 1, 2024 10:11 AM

To: PLNBufferMaps <plnbuffermaps@cabq.gov>

Subject: Mailed Notice to Properties Owners for Double Eagle Airport 7401 Paseo Del Volcan

[EXTERNAL] Forward to phishing@cabq.gov and delete if an email causes any concern.

Hello,

Please find attached a list of parcels and legal descriptions for a Site Plan -EPC submittal for Double Eagle Airport 7401 Paseo Del Volcan. Can I please get a list of the property owners.

Thank you!

**DEKKER
PERICH
SABATINI**
Architecture
in Progress

Rebecca Shank

Urban Planning Associate

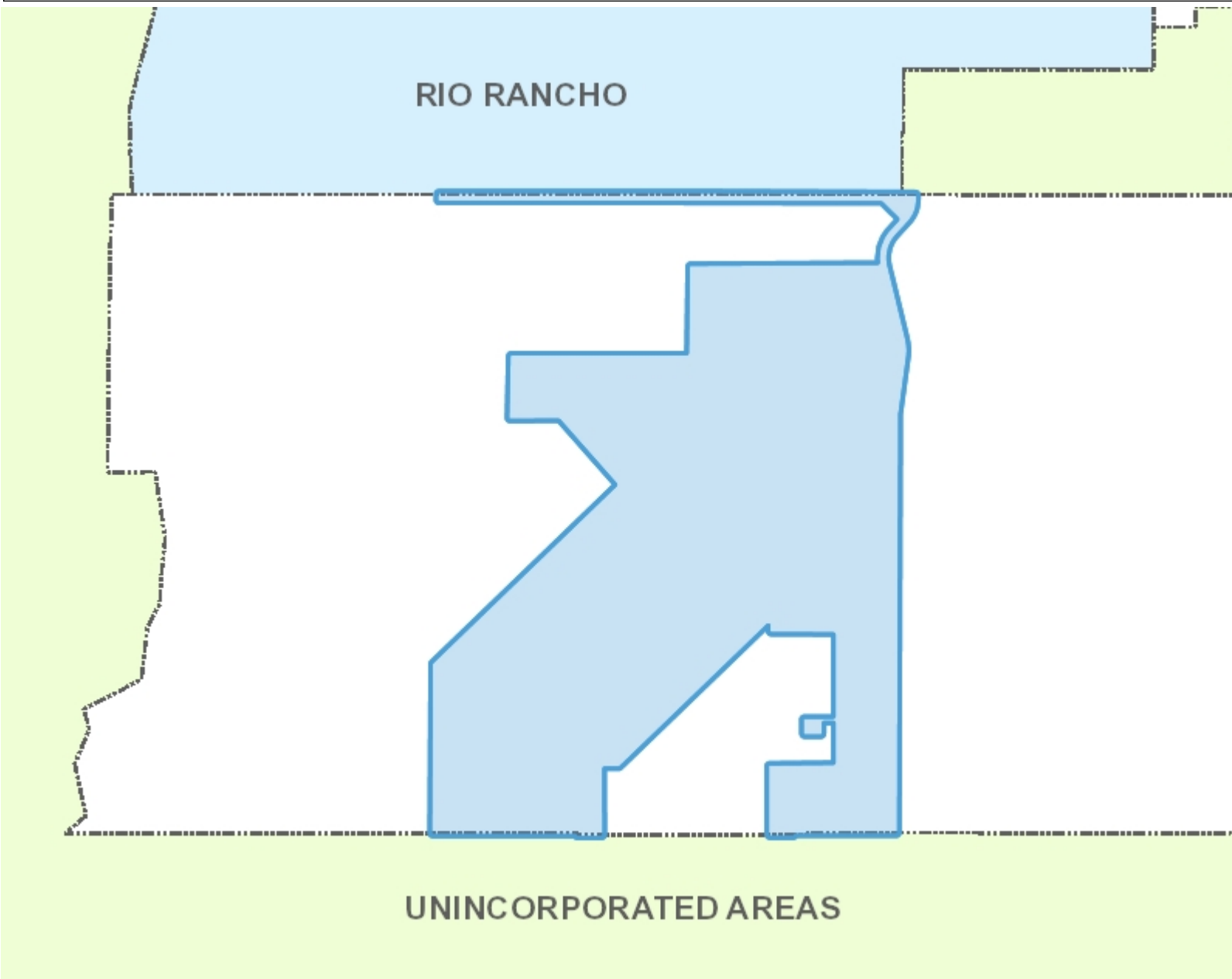
Dekker Perich Sabatini

505.761.9700 / dpsdesign.org

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7401 Paseo Del Volcan



Legend

- Corrales
- Edgewood
- Los Ranchos
- Rio Rancho
- Tijeras
- UNINCORPORATED

Notes

Buffer: 100 Ft.
ROW: Artisco Vista Blvd NW

9,322 0 4,661 9,322 Feet

WGS_1984_Web_Mercator_Auxiliary_Sphere
7/1/2024 © City of Albuquerque

1: 55,930

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THIS MAP IS NOT TO BE USED FOR NAVIGATION

WEST MESA AVIATION
7401 PASEO DEL VOLCAN BLVD NW
ALBUQUERQUE NM 87121-6601

STATE OF NEW MEXICO COMMISSIONER
OF PUBLIC LANDS
PO BOX 1148
SANTA FE NM 87504-1148

SAFE-TEE HANGARAGE I LLC
4916 GLENWOOD HILLS DR NE
ALBUQUERQUE NM 87111-2963

CITY OF ALBUQUERQUE
PO BOX 1293
ALBUQUERQUE NM 87103

BODE AERO SERVICES INC
PO BOX 19006
ALBUQUERQUE NM 87119-0006

ALBUQUERQUE INTERNATIONAL
AIRPORT C/O CITY OF ALB DIRECTOR OF
AVIATION
PO BOX 9948
ALBUQUERQUE NM 87119-1048

3 KI LLC
400 RIO GRANDE BLVD NW
ALBUQUERQUE NM 87104-1451

ALBUQUERQUE AIR CENTER INC C/O T J
DEAN
PO BOX 19006
ALBUQUERQUE NM 87119-0006

ALBUQUERQUE BERNALILLO COUNTY
WATER UTILITY AUTHORITY
PO BOX 1293
ALBUQUERQUE NM 87103-1293

DOUGHTY DANIEL H & ELIZABETH
CHRISTINE CO TRUSTEES DOUGHTY
TRUST
139 BIG HORN RIDGE DR NE
ALBUQUERQUE NM 87122-1903

ALBUQUERQUE 50 LLC
8716 SPANISH RIDGE AVE SUITE 110
LAS VEGAS NV 89148-1396

CITY OF ALBUQUERQUE ATTN: REAL
PROP DIVISION
PO BOX 1293
ALBUQUERQUE NM 87103-1293

KHIMJI HASSAN J TRUSTEE KHIMJI RVT &
KHIMJI ARIF& RALPH TAZMIN & DALE
6400 CAMEL DR NE
ALBUQUERQUE NM 87113-1867

ESTRELLA EAST LLC
6900 E CAMELBACK RD SUITE 607
SCOTTSDALE AZ 85251-8044

PARADISE WEST INC
7423 LANTERN RD NE
ALBUQUERQUE NM 87109

RANCH JOINT VENTURE ATTN: W L
ROGERS
PO BOX 101149
FORT WORTH TX 76185-1149

MECHENBIER CONSTRUCTION
8500 WASHINGTON ST NE SUITE A-5
ALBUQUERQUE NM 87113-1861

PNM
2401 AZTEC RD NW MS 2140
ALBUQUERQUE NM 87107

United States of American C/O
Petroglyph Naitional Monument
6001 Unser Blvd NW
Albuquerque, NM 87120-2069

AERO WEST C/O BODE AERO SERVICES
PO BOX 19006
ALBUQUERQUE NM 87119-0006

STATE OF NEW MEXICO
PO BOX 1148
SANTA FE NM 87504

UNITED STATES OF AMERICA
901 ATRISCO VISTA BLVD NW
ALBUQUERQUE NM 87120-0000

UP II LLC
6900 E CAMELBACK RD SUITE 607
SCOTTSDALE AZ 85251-8044

July 3, 2024

Re: Public Notice of Site Plan - EPC Submittal and Hearing
Double Eagle II Master Plan

Dear Property Owner,

Dekker Perich Sabatini, as the agent for the City of Albuquerque Aviation Department, is seeking a Site Plan approval to establish Design Standards for the Double Eagle II Master Plan.

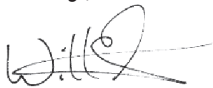
Per notification received from the Office of Neighborhood Coordination (ONC), no impacted Neighborhood Associations are within the notification boundary. Therefore, a neighborhood meeting was not conducted.

The request is for an amendment to the existing Master Plan (i.e. Development Plan) for Double Eagle II Airport. The approximately 3,906.3013 acres are zoned Non-Residential Sensitive Use (NR-SU) and accommodates aviation related uses and non-aviation uses of commercial, lodging, light industrial, educational, film studio uses, and renewable energy generation. The proposed Design Standards established with the Double Eagle II Master Plan Site Plan would provide standards for future development that take into consideration the context of the abutting uses and neighbors and FAA regulations for a cohesive, safe, and visually appealing area.

As required by IDO Table 6-1-1, the EPC Public Hearing date will be August 15, 2024, beginning at 8:30 AM via ZOOM at which time the application will be reviewed and decided on by the City of Albuquerque. Members of the public may attend via the web at this address: <https://cabq.zoom.us/j/2269592859> or by calling the following number: 1-301 715-8592 and entering Meeting ID: 226 959 2859.

Enclosed is the Site Plan and Design Guidelines proposed for Double Eagle II Master Plan. If you have any additional questions or concerns about any of the materials contained therein, please feel free to contact me at willg@dpsdesign.org or 505(761-9700).

Sincerely,



Will Gleason, AICP
Principal, Dekker Perich Sabatini
Agent for City of Albuquerque Aviation Department

Attachments: Required EPC Notice of Hearing Form, Site Plan, Design Guidelines



PNM
2401 AZTEC RD NW MS 2140
ALBUQUERQUE NM 87107

RODE AERO SERVICES
INC PO BOX 13006
ALBUQUERQUE NM 87119-0006

3 M LLC
400 RIO GRANDE BLVD NW
ALBUQUERQUE NM 87104-1451

AERO WEST C/O BOB AERO
SERVICES PO BOX 13006
ALBUQUERQUE NM 87119-0006

MECHENBER CONSTRUCTION
8500 WASHINGTON ST NE
SUITE A-5
ALBUQUERQUE NM 87133-1861

RANCH JOINT VENTURE ATTN:
W L ROGERS
PO BOX 101149
FORT WORTH TX 76185-1149

ALBUQUERQUE AIR CENTER INC C/O T
DEAN
PO BOX 13006
ALBUQUERQUE NM 87119-0006

ALBUQUERQUE 50 LLC
8718 SPANISH RIDGE AVE SUITE
130 LAS VEGAS NV 89148-1396

CITY OF ALBUQUERQUE PO
BOX 1293
ALBUQUERQUE NM 87103

ESTRELLA EAST LLC
6900 E CAMELBACK RD SUITE 667
SCOTTSDALE AZ 85251-8044

ALBUQUERQUE BERNALILLO COUNTY
WATER UTILITY AUTHORITY
PO BOX 1293
ALBUQUERQUE NM 87103-1293

ALBUQUERQUE INTERNATIONAL
AIRPORT C/O CITY OF ALBUQ
DIRECTOR OF AVIATION
PO BOX 9948
ALBUQUERQUE NM 87119-1048



**OFFICIAL PUBLIC NOTIFICATION FORM
FOR MAILED OR ELECTRONIC MAIL NOTICE
CITY OF ALBUQUERQUE PLANNING DEPARTMENT**



PART I - PROCESS

Use [Table 6-1-1](#) in the Integrated Development Ordinance (IDO) to answer the following:

Application Type:

Decision-making Body:

Pre-Application meeting required: ☒ Yes ☐ No

Neighborhood meeting required: ☒ Yes ☐ No

Mailed Notice required: ☒ Yes ☐ No

Electronic Mail required: ☒ Yes ☐ No

Is this a Site Plan Application: ☒ Yes ☐ No **Note: if yes, see second page**

PART II – DETAILS OF REQUEST

Address of property listed in application: 7401 Paseo Del Volcan NW, Albuquerque, NM 87120

Name of property owner: City of Albuquerque Aviation Department - Double Eagle II Airport

Name of applicant: Jessica Lawlis, Dekker Perich Sabatini

Date, time, and place of public meeting or hearing, if applicable: August 15 at 8:30 AM

Address, phone number, or website for additional information: Members of the public may attend via the web at: <https://cabq.zoom.us/j/2269592859> or calling :1-301-715-8592 & entering Meeting ID: 226 959 2859

PART III - ATTACHMENTS REQUIRED WITH THIS NOTICE

☒ Zone Atlas page indicating subject property.

☒ Drawings, elevations, or other illustrations of this request.

☒ Summary of pre-submittal neighborhood meeting, if applicable.

☒ Summary of request, including explanations of deviations, variances, or waivers.

IMPORTANT: PUBLIC NOTICE MUST BE MADE IN A TIMELY MANNER PURSUANT TO [SUBSECTION 14-16-6-4\(K\)](#) OF THE INTEGRATED DEVELOPMENT ORDINANCE (IDO). PROOF OF NOTICE WITH ALL REQUIRED ATTACHMENTS MUST BE PRESENTED UPON APPLICATION.

I certify that the information I have included here and sent in the required notice was complete, true, and accurate to the extent of my knowledge.

(Applicant signature) July 3, 2024 (Date)

Note: Providing incomplete information may require re-sending public notice. Providing false or misleading information is a violation of the IDO pursuant to IDO Subsection 14-16-6-9(B)(3) and may lead to a denial of your application.

**OFFICIAL PUBLIC NOTIFICATION FORM
FOR MAILED OR ELECTRONIC MAIL NOTICE
CITY OF ALBUQUERQUE PLANNING DEPARTMENT**



PART IV – ATTACHMENTS REQUIRED FOR SITE PLAN APPLICATIONS ONLY

Provide a site plan that shows, at a minimum, the following:

☒ a. Location of proposed buildings and landscape areas.

☒ b. Access and circulation for vehicles and pedestrians.

~~NA~~c. Maximum height of any proposed structures, with building elevations.

~~NA~~d. For residential development: Maximum number of proposed dwelling units.

~~NA~~e. For non-residential development:

☐ Total gross floor area of proposed project.

☐ Gross floor area for each proposed use.

[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: WEST MESA AVIATION

Mailing Address*: 7401 PASEO DEL VOLCAN BLVD NW

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for the Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
 - ☐ Zoning Hearing Examiner (ZHE)
 - ☐ Development Review Board (DRB)
 - ☐ Landmarks Commission (LC)
 - ☒ Environmental Planning Commission (EPC)

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

<https://ido.abc-zone.com/>

IDO Interactive Map

<https://tinyurl.com/IDOzoningmap>

⁵ Available here: <https://tinyurl.com/idozoningmap>

[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: STATE OF NEW MEXICO COMMISSIONER OF PUBLIC LANDS

Mailing Address*: PO BOX 1148

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
 - ☐ Zoning Hearing Examiner (ZHE)
 - ☐ Development Review Board (DRB)
 - ☐ Landmarks Commission (LC)
 - ☒ Environmental Planning Commission (EPC)

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:

By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.
The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation, tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

- ☐ d. **For residential development***: Maximum number of proposed dwelling units.
- ☐ e. **For non-residential development***:
 - ☐ Total gross floor area of proposed project.
 - ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center & Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

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IDO Interactive Map

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**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: SAFE-TEE HANGARAGE I LLC

Mailing Address*: 4916 GLENWOOD HILLS DR NE

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
 - ☐ Zoning Hearing Examiner (ZHE)
 - ☐ Development Review Board (DRB)
 - ☐ Landmarks Commission (LC)
 - ☒ Environmental Planning Commission (EPC)

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

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5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

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³ Address (mailing or email), phone number, or website to be provided by the applicant

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[Note: Items with an asterisk (*) are required.]

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- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
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Date of Notice*: July 3, 2024

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Property Owner within 100 feet*: CITY OF ALBUQUERQUE

Mailing Address*: PO BOX 1293

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
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 - ☐ Conditional Use Approval
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 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☒ Other: Site Plan - EPC submittal for Double Eagle II Airport Master Plan

Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

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Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

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Explanation*:

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tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

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³ Address (mailing or email), phone number, or website to be provided by the applicant

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Additional Information:

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1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

<https://ido.abc-zone.com/>

IDO Interactive Map

<https://tinyurl.com/IDOzoningmap>

⁵ Available here: <https://tinurl.com/idozoningmap>

[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: BODE AERO SERVICES INC

Mailing Address*: PO BOX 19006

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☐ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☒ Other: Site Plan - EPC submittal for Double Eagle II Airport Master Plan

Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

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<https://ido.abc-zone.com/>

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: ALBUQUERQUE AIR CENTER INC C/O T J DEAN

Mailing Address*: PO BOX 19006

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☐ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☒ Other: Site Plan - EPC submittal for Double Eagle II Airport Master Plan

Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:

By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
- ☒ b. Access and circulation for vehicles and pedestrians.*
- ☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: AERO WEST C/O BODE AERO SERVICES

Mailing Address*: PO BOX 19006

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☐ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☒ Other: Site Plan - EPC submittal for Double Eagle II Airport Master Plan

Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: 3 KI LLC

Mailing Address*: 400 RIO GRANDE BLVD NW

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☐ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☒ Other: Site Plan - EPC submittal for Double Eagle II Airport Master Plan

Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
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☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

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[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
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NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: ALBUQUERQUE 50 LLC

Mailing Address*: 8716 SPANISH RIDGE AVE SUITE 110

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
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Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
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☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

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[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
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NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: Albuquerque Bernalillo County Water Utility Authority

Mailing Address*: PO Box 1293, Albuquerque, NM 87103

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☐ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☒ Other: Site Plan - EPC submittal for Double Eagle II Airport Master Plan

Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

<https://ido.abc-zone.com/>

IDO Interactive Map

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: City of Albuquerque Attn: Real Property Division

Mailing Address*: PO Box 1293, Albuquerque, NM 87120

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☐ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
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 - ☐ Waiver
 - ☒ Other: Site Plan - EPC submittal for Double Eagle II Airport Master Plan

Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

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☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

DOUGHTY DANIEL H & ELIZABETH CHRISTINE CO
Property Owner within 100 feet*: TRUSTEES DOUGHTY TRUST

Mailing Address*: 139 BIG HORN RIDGE DR NE, ALBUQUERQUE NM 87122-1903

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☐ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☒ Other: Site Plan - EPC submittal for Double Eagle II Airport Master Plan

Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
 - ☐ Zoning Hearing Examiner (ZHE)
 - ☐ Development Review Board (DRB)
 - ☐ Landmarks Commission (LC)
 - ☒ Environmental Planning Commission (EPC)

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: ESTRELLA EAST LLC

Mailing Address*: 6900 E CAMELBACK RD SUITE 607, SCOTTSDALE AZ 85251-8044

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☐ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☒ Other: Site Plan - EPC submittal for Double Eagle II Airport Master Plan

Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

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[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
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Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
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NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: PARADISE WEST INC

Mailing Address*: 7423 LANTERN RD NE, ALBUQUERQUE NM 87109

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☐ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
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Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
- | | |
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| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
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Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

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To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

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Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

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Additional Information:

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**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

KHIMJI HASSAN J TRUSTEE KHIMJI RVT & KHIMJI
Property Owner within 100 feet*: ARIF & RALPH TAZMIN & DALE

Mailing Address*: 6400 CAMEL DR NE, ALBUQUERQUE NM 87113-1867

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* *[if applicable]* Dekker Perich Sabatini, Jessica Lawlis
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 - ☐ Permit _____ (Carport or Wall/Fence – Major)
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Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

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| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
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¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

<https://ido.abc-zone.com/>

IDO Interactive Map

<https://tinyurl.com/IDOzoningmap>

⁵ Available here: <https://tinyurl.com/idozoningmap>

[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: MECHENBIER CONSTRUCTION

Mailing Address*: 8500 WASHINGTON ST NE SUITE A-5, ALBUQUERQUE NM 87113-1861

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☐ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☒ Other: Site Plan - EPC submittal for Double Eagle II Airport Master Plan

Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:

By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

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² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: PNM

Mailing Address*: 2401 AZTEC RD NW MS 2140, ALBUQUERQUE NM 87107

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☐ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☒ Other: Site Plan - EPC submittal for Double Eagle II Airport Master Plan

Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

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tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

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[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
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NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

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**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: RANCH JOINT VENTURE ATTN: W L ROGERS

Mailing Address*: PO BOX 101149, FORT WORTH TX 76185-1149

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☐ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☒ Other: Site Plan - EPC submittal for Double Eagle II Airport Master Plan

Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
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¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

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NA d. **For residential development***: Maximum number of proposed dwelling units.

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- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
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NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

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**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: United States of American C/O Petroglyph National Monument

Mailing Address*: 6001 Unser Blvd NW, Albuquerque, NM 87120-2069

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☐ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
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 - ☐ Waiver
 - ☒ Other: Site Plan - EPC submittal for Double Eagle II Airport Master Plan

Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
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To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

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Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

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**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: STATE OF NEW MEXICO

Mailing Address*: PO BOX 1148, SANTA FE NM 87504

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
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| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
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Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

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Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

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As of May 9, 2024 there are no Neighborhood Associations to notify.

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representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

<https://ido.abc-zone.com/>

IDO Interactive Map

<https://tinyurl.com/IDOzoningmap>

⁵ Available here: <https://tinurl.com/idozoningmap>

[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: UP II LLC

Mailing Address*: 6900 E CAMELBACK RD SUITE 607, SCOTTSDALE AZ 85251-8044

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☐ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☒ Other: Site Plan - EPC submittal for Double Eagle II Airport Master Plan

Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

<https://ido.abc-zone.com/>

IDO Interactive Map

<https://tinyurl.com/IDOzoningmap>

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: UNITED STATES OF AMERICA

Mailing Address*: 901 ATRISCO VISTA BLVD NW, ALBUQUERQUE NM 87120-0000

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☐ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☒ Other: Site Plan - EPC submittal for Double Eagle II Airport Master Plan

Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

<https://ido.abc-zone.com/>

IDO Interactive Map

<https://tinyurl.com/IDOzoningmap>

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 3, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Albuquerque International Airport C/O
Property Owner within 100 feet*: City of Albuquerque Director of Aviation

Mailing Address*: PO Box 9948, ALBUQUERQUE NM 87119-1048

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☐ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☒ Other: Site Plan - EPC submittal for Double Eagle II Airport Master Plan

Summary of project/request¹*:

Submittal of a Site Plan with Design Standards for th Double Eagle II Master Plan to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:

By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
- ☒ b. Access and circulation for vehicles and pedestrians.*
- ☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

<https://ido.abc-zone.com/>

IDO Interactive Map

<https://tinyurl.com/IDOzoningmap>

⁵ Available here: <https://tinyurl.com/idozoningmap>

July 29, 2024

Re: Public Notice of Site Plan - EPC Submittal and Hearing
Double Eagle II Master Plan

Dear Property Owner,

As you have already been made aware, Dekker, as the agent for the City of Albuquerque Aviation Department, has been working with the City Aviation Department on updates to Double Eagle Airport II. We are making you aware that our prior notice, dated July 3, 2024, mentioned a July submittal to the EPC to update the Double Eagle II Airport Master Plan. The City has informed us that the proposed request is classified as a Site Plan- EPC application rather than a Master Plan amendment previously notified. The specifics of the amendments have not changed and seek to establish a site plan that documents existing conditions. You are being renotified that the request considered will be referred to as a Site Plan – EPC going forward.

As indicated in pervious notifications, the airport is approximately 3,906.3013 acres, all zoned Non-Residential Sensitive Use (NR-SU). The primary goal of the Site Plan request is to document existing site conditions and establish Design Guidelines for Double Eagle II Airport that would provide standards for future development that take into consideration the context of the abutting uses, neighbors, and FAA regulations for a cohesive, safe, and visually appealing area.


Per notification received from the Office of Neighborhood Coordination (ONC), no impacted Neighborhood Associations are within the notification boundary. Therefore, a pre-submittal facilitated neighborhood meeting per the requirements of the IDO was not conducted. The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation, tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

As required by IDO Table 6-1-1, the Site Plan will be reviewed by the Environmental Planning Commission (EPC) at a Public Hearing on August 15, 2024, beginning at 8:30 AM via ZOOM.

Members of the public may attend via the web at this address: <https://cabq.zoom.us/j/2269592859> or by calling the following number: 1-301 715-8592 and entering Meeting ID: 226 959 2859.

Enclosed are updated primary Site Plan sheets proposed for Double Eagle II Airport. Interested parties can access the full site plan set utilizing the following OneDrive link: https://dpsdesign-my.sharepoint.com/:f/g/personal/rebeccas_dpsdesign_org/EhMOlgHvuihBnDeVVnyd_uUByXOdb8lmMTrc5lZxhIW-jA?e=3QuVat. If you have any additional questions or concerns about any of the materials contained therein, please feel free to contact me at willg@dpsdesign.org or 505(761-9700).

Sincerely,



Will Gleason, AICP
Principal, Dekker Perich Sabatini
Agent for City of Albuquerque Aviation Department

Attachments: Required Zone Atlas Map, EPC Notice of Hearing Form, Site Plan

**OFFICIAL PUBLIC NOTIFICATION FORM
FOR MAILED OR ELECTRONIC MAIL NOTICE
CITY OF ALBUQUERQUE PLANNING DEPARTMENT**



PART I - PROCESS

Use [Table 6-1-1](#) in the Integrated Development Ordinance (IDO) to answer the following:

Application Type:

Decision-making Body:

Pre-Application meeting required: ☒ Yes ☐ No

Neighborhood meeting required: ☒ Yes ☐ No

Mailed Notice required: ☒ Yes ☐ No

Electronic Mail required: ☒ Yes ☐ No

Is this a Site Plan Application: ☒ Yes ☐ No **Note: if yes, see second page**

PART II – DETAILS OF REQUEST

Address of property listed in application: 7401 Paseo Del Volcan NW, Albuquerque, NM 87120

Name of property owner: City of Albuquerque Aviation Department - Double Eagle II Airport

Name of applicant: Jessica Lawlis, Dekker Perich Sabatini

Date, time, and place of public meeting or hearing, if applicable: August 15 at 8:30 AM

Address, phone number, or website for additional information: Members of the public may attend via the web at: <https://cabq.zoom.us/j/2269592859> or calling :1-301-715-8592 & entering Meeting ID: 226 959 2859

PART III - ATTACHMENTS REQUIRED WITH THIS NOTICE

☒ Zone Atlas page indicating subject property.

☒ Drawings, elevations, or other illustrations of this request.

☒ Summary of pre-submittal neighborhood meeting, if applicable.

☒ Summary of request, including explanations of deviations, variances, or waivers.

IMPORTANT: PUBLIC NOTICE MUST BE MADE IN A TIMELY MANNER PURSUANT TO [SUBSECTION 14-16-6-4\(K\)](#) OF THE INTEGRATED DEVELOPMENT ORDINANCE (IDO). PROOF OF NOTICE WITH ALL REQUIRED ATTACHMENTS MUST BE PRESENTED UPON APPLICATION.

I certify that the information I have included here and sent in the required notice was complete, true, and accurate to the extent of my knowledge.

(Applicant signature) July 25, 2024 (Date)

Note: Providing incomplete information may require re-sending public notice. Providing false or misleading information is a violation of the IDO pursuant to IDO Subsection 14-16-6-9(B)(3) and may lead to a denial of your application.

**OFFICIAL PUBLIC NOTIFICATION FORM
FOR MAILED OR ELECTRONIC MAIL NOTICE
CITY OF ALBUQUERQUE PLANNING DEPARTMENT**



PART IV – ATTACHMENTS REQUIRED FOR SITE PLAN APPLICATIONS ONLY

Provide a site plan that shows, at a minimum, the following:

☒ a. Location of proposed buildings and landscape areas.

☒ b. Access and circulation for vehicles and pedestrians.

~~NA~~c. Maximum height of any proposed structures, with building elevations.

~~NA~~d. For residential development: Maximum number of proposed dwelling units.

~~NA~~e. For non-residential development:

☐ Total gross floor area of proposed project.

☐ Gross floor area for each proposed use.

[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: WEST MESA AVIATION

Mailing Address*: 7401 PASEO DEL VOLCAN BLVD NW

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
 - ☐ Zoning Hearing Examiner (ZHE)
 - ☐ Development Review Board (DRB)
 - ☐ Landmarks Commission (LC)
 - ☒ Environmental Planning Commission (EPC)

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
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² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

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[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
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Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
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- Current Land Use(s) [vacant, if none] Aviation related uses

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

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IDO Interactive Map

<https://tinyurl.com/IDOzoningmap>

⁵ Available here: <https://tinyurl.com/idozoningmap>

[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: STATE OF NEW MEXICO COMMISSIONER OF PUBLIC LANDS

Mailing Address*: PO BOX 1148

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan - EPC with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
 - ☐ Zoning Hearing Examiner (ZHE)
 - ☐ Development Review Board (DRB)
 - ☐ Landmarks Commission (LC)
 - ☒ Environmental Planning Commission (EPC)

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.
The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation, tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

- ☐ d. **For residential development***: Maximum number of proposed dwelling units.
- ☐ e. **For non-residential development***:
 - ☐ Total gross floor area of proposed project.
 - ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center & Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

<https://ido.abc-zone.com/>

IDO Interactive Map

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: SAFE-TEE HANGARAGE I LLC

Mailing Address*: 4916 GLENWOOD HILLS DR NE

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
 - ☐ Zoning Hearing Examiner (ZHE)
 - ☐ Development Review Board (DRB)
 - ☐ Landmarks Commission (LC)
 - ☒ Environmental Planning Commission (EPC)

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

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**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: CITY OF ALBUQUERQUE

Mailing Address*: PO BOX 1293

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:

- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

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**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: BODE AERO SERVICES INC

Mailing Address*: PO BOX 19006

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
 - ☐ Zoning Hearing Examiner (ZHE)
 - ☐ Development Review Board (DRB)
 - ☐ Landmarks Commission (LC)
 - ☒ Environmental Planning Commission (EPC)

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

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[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: ALBUQUERQUE AIR CENTER INC C/O T J DEAN

Mailing Address*: PO BOX 19006

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
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 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
 - ☐ Zoning Hearing Examiner (ZHE)
 - ☐ Development Review Board (DRB)
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¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

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Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

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To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

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2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
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Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

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representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

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Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: AERO WEST C/O BODE AERO SERVICES

Mailing Address*: PO BOX 19006

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
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 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
 - ☐ Zoning Hearing Examiner (ZHE)
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Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

<https://ido.abc-zone.com/>

IDO Interactive Map

<https://tinyurl.com/IDOzoningmap>

⁵ Available here: <https://tinurl.com/idozoningmap>

[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: 3 KI LLC

Mailing Address*: 400 RIO GRANDE BLVD NW

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
 - ☐ Zoning Hearing Examiner (ZHE)
 - ☐ Development Review Board (DRB)
 - ☐ Landmarks Commission (LC)
 - ☒ Environmental Planning Commission (EPC)

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.
The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included
representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,
tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

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[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: ALBUQUERQUE 50 LLC

Mailing Address*: 8716 SPANISH RIDGE AVE SUITE 110

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
 - ☐ Zoning Hearing Examiner (ZHE)
 - ☐ Development Review Board (DRB)
 - ☐ Landmarks Commission (LC)
 - ☒ Environmental Planning Commission (EPC)

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.
The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included
representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,
tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

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IDO Interactive Map

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: Albuquerque Bernalillo County Water Utility Authority

Mailing Address*: PO Box 1293, Albuquerque, NM 87103

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
 - ☐ Zoning Hearing Examiner (ZHE)
 - ☐ Development Review Board (DRB)
 - ☐ Landmarks Commission (LC)
 - ☒ Environmental Planning Commission (EPC)

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

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⁵ Available here: <https://tinyurl.com/idozoningmap>

[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: City of Albuquerque Attn: Real Property Division

Mailing Address*: PO Box 1293, Albuquerque, NM 87120

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:

- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
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☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

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☒ c. Maximum height of any proposed structures, with building elevations.*

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NA d. **For residential development***: Maximum number of proposed dwelling units.

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- ☐ Total gross floor area of proposed project.
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Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
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 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
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- Current Land Use(s) [vacant, if none] Aviation related uses
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NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

DOUGHTY DANIEL H & ELIZABETH CHRISTINE CO
Property Owner within 100 feet*: TRUSTEES DOUGHTY TRUST

Mailing Address*: 139 BIG HORN RIDGE DR NE, ALBUQUERQUE NM 87122-1903

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
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Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:

- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

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Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

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² Physical address or Zoom link

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⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

<https://ido.abc-zone.com/>

IDO Interactive Map

<https://tinyurl.com/IDOzoningmap>

⁵ Available here: <https://tinyurl.com/idozoningmap>

[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: ESTRELLA EAST LLC

Mailing Address*: 6900 E CAMELBACK RD SUITE 607, SCOTTSDALE AZ 85251-8044

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

<https://ido.abc-zone.com/>

IDO Interactive Map

<https://tinyurl.com/IDOzoningmap>

⁵ Available here: <https://tinyurl.com/idozoningmap>

[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: PARADISE WEST INC

Mailing Address*: 7423 LANTERN RD NE, ALBUQUERQUE NM 87109

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
 - ☐ Zoning Hearing Examiner (ZHE)
 - ☐ Development Review Board (DRB)
 - ☐ Landmarks Commission (LC)
 - ☒ Environmental Planning Commission (EPC)

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

<https://ido.abc-zone.com/>

IDO Interactive Map

<https://tinyurl.com/IDOzoningmap>

⁵ Available here: <https://tinyurl.com/idozoningmap>

[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

KHIMJI HASSAN J TRUSTEE KHIMJI RVT & KHIMJI
Property Owner within 100 feet*: ARIF & RALPH TAZMIN & DALE

Mailing Address*: 6400 CAMEL DR NE, ALBUQUERQUE NM 87113-1867

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
 - ☐ Zoning Hearing Examiner (ZHE)
 - ☐ Development Review Board (DRB)
 - ☐ Landmarks Commission (LC)
 - ☒ Environmental Planning Commission (EPC)

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

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IDO Interactive Map

<https://tinyurl.com/IDOzoningmap>

⁵ Available here: <https://tinyurl.com/idozoningmap>

[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: MECHENBIER CONSTRUCTION

Mailing Address*: 8500 WASHINGTON ST NE SUITE A-5, ALBUQUERQUE NM 87113-1861

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submission of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

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IDO Interactive Map

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⁵ Available here: <https://tinyurl.com/idozoningmap>

[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: PNM

Mailing Address*: 2401 AZTEC RD NW MS 2140, ALBUQUERQUE NM 87107

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☐ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☒ Other: Site Plan - EPC submittal for Double Eagle II Airport Master Plan

Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

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[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: RANCH JOINT VENTURE ATTN: W L ROGERS

Mailing Address*: PO BOX 101149, FORT WORTH TX 76185-1149

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
 - ☐ Zoning Hearing Examiner (ZHE)
 - ☐ Development Review Board (DRB)
 - ☐ Landmarks Commission (LC)
 - ☒ Environmental Planning Commission (EPC)

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:

By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

<https://ido.abc-zone.com/>

IDO Interactive Map

<https://tinyurl.com/IDOzoningmap>

⁵ Available here: <https://tinyurl.com/idozoningmap>

[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: United States of American C/O Petroglyph National Monument

Mailing Address*: 6001 Unser Blvd NW, Albuquerque, NM 87120-2069

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
 - ☐ Zoning Hearing Examiner (ZHE)
 - ☐ Development Review Board (DRB)
 - ☐ Landmarks Commission (LC)
 - ☒ Environmental Planning Commission (EPC)

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

<https://ido.abc-zone.com/>

IDO Interactive Map

<https://tinyurl.com/IDOzoningmap>

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: STATE OF NEW MEXICO

Mailing Address*: PO BOX 1148, SANTA FE NM 87504

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
 - ☐ Zoning Hearing Examiner (ZHE)
 - ☐ Development Review Board (DRB)
 - ☐ Landmarks Commission (LC)
 - ☒ Environmental Planning Commission (EPC)

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:

By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: UP II LLC

Mailing Address*: 6900 E CAMELBACK RD SUITE 607, SCOTTSDALE AZ 85251-8044

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☒ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☐ Other: _____

Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:
 - ☐ Zoning Hearing Examiner (ZHE)
 - ☐ Development Review Board (DRB)
 - ☐ Landmarks Commission (LC)
 - ☒ Environmental Planning Commission (EPC)

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

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<https://tinyurl.com/IDOzoningmap>

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Property Owner within 100 feet*: UNITED STATES OF AMERICA

Mailing Address*: 901 ATRISCO VISTA BLVD NW, ALBUQUERQUE NM 87120-0000

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☐ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☒ Other: Site Plan - EPC submittal for Double Eagle II Airport Master Plan

Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:

- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:
By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

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2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
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³ Address (mailing or email), phone number, or website to be provided by the applicant

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[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
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-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

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[Note: Items with an asterisk (*) are required.]

**Public Notice of a Proposed Project in the City of Albuquerque
for Decisions Requiring a Meeting or Hearing
Mailed to a Property Owner**

Date of Notice*: July 25, 2024

This notice of an application for a proposed project is provided as required by Integrated Development Ordinance (IDO) [Subsection 14-16-6-4\(K\) Public Notice](#) to:

Albuquerque International Airport C/O
Property Owner within 100 feet*: City of Albuquerque Director of Aviation

Mailing Address*: PO Box 9948, ALBUQUERQUE NM 87119-1048

Project Information Required by [IDO Subsection 14-16-6-4\(K\)\(1\)\(a\)](#)

1. Subject Property Address* 7401 Paseo Del Volcan NW, Albuquerque, NM 87120
Location Description Double Eagle II Airport, see attached zone atlas maps
2. Property Owner* City of Albuquerque
3. Agent/Applicant* [if applicable] Dekker Perich Sabatini, Jessica Lawlis
4. Application(s) Type* per IDO [Table 6-1-1](#) [mark all that apply]
 - ☐ Conditional Use Approval
 - ☐ Permit _____ (Carport or Wall/Fence – Major)
 - ☐ Site Plan
 - ☐ Subdivision _____ (Minor or Major)
 - ☐ Vacation _____ (Easement/Private Way or Public Right-of-way)
 - ☐ Variance
 - ☐ Waiver
 - ☒ Other: Site Plan - EPC submittal for Double Eagle II Airport Master Plan

Summary of project/request¹*:

Submittal of a Site Plan - EPC, with Design Standards for the Double Eagle II Airport to document existing development and standards for future developments.

5. This application will be decided at a public meeting or hearing by*:

- | | |
|--|---|
| <input type="checkbox"/> Zoning Hearing Examiner (ZHE) | <input type="checkbox"/> Development Review Board (DRB) |
| <input type="checkbox"/> Landmarks Commission (LC) | <input checked="" type="checkbox"/> Environmental Planning Commission (EPC) |

¹ Attach additional information, as needed to explain the project/request.

[Note: Items with an asterisk (*) are required.]

Date/Time*: August 15, 2024 at 8:30 AM

Location*²: https://cabq.zoom.us/j/2269592859 or by calling: 1-301 715-8592 & entering Meeting ID: 226 959 2859

Agenda/meeting materials: <http://www.cabq.gov/planning/boards-commissions>

To contact staff, email devhelp@cabq.gov or call the Planning Department at 505-924-3860.

6. Where more information about the project can be found*³:

By contacting agent, Jessica Lawlis at jessical@dpsdesign.org or at (505)761-9700

Project Information Required for Mail/Email Notice by [IDO Subsection 6-4\(K\)\(1\)\(b\)](#):

1. Zone Atlas Page(s)*⁴ C4/5/6, D4/5/6, E4/5/6, F4/5/6, G4/5/6
2. Architectural drawings, elevations of the proposed building(s) or other illustrations of the proposed application, as relevant*: Attached to notice or provided via website noted above
3. The following exceptions to IDO standards have been requested for this project*:

☐ Deviation(s) ☐ Variance(s) ☐ Waiver(s)

Explanation*:

4. A Pre-submittal Neighborhood Meeting was required by [Table 6-1-1](#): ☒ Yes ☐ No

Summary of the Pre-submittal Neighborhood Meeting, if one occurred:

As of May 9, 2024 there are no Neighborhood Associations to notify.

The City of Aviation Department did conduct a Public Open House on June 28, 2024 to provide project context and to solicit feedback. In attendance included

representatives of the West Side Neighborhood Association Coalition, Petroglyph National Monument, New Mexico Department of Transportation,

tenants of DEII, Pueblo of Laguna, Bernalillo County, and New Mexico Legislature.

5. **For Site Plan Applications only***, attach site plan showing, at a minimum:

- ☒ a. Location of proposed buildings and landscape areas.*
☒ b. Access and circulation for vehicles and pedestrians.*
☒ c. Maximum height of any proposed structures, with building elevations.*

² Physical address or Zoom link

³ Address (mailing or email), phone number, or website to be provided by the applicant

⁴ Available online here: <http://data.cabq.gov/business/zoneatlas/>

[Note: Items with an asterisk (*) are required.]

NA d. **For residential development***: Maximum number of proposed dwelling units.

NA e. **For non-residential development***:

- ☐ Total gross floor area of proposed project.
- ☐ Gross floor area for each proposed use.

Additional Information:

From the IDO Zoning Map⁵:

1. Area of Property [typically in acres] approximately 3,906.3013 Acres
 2. IDO Zone District NR-SU
 3. Overlay Zone(s) [if applicable] (APO)-Airport Protection Overlay Zone (3-4)
 4. Center or Corridor Area [if applicable] Developing Employment Center and Commuter Corridor
- Current Land Use(s) [vacant, if none] Aviation related uses
-

NOTE: Pursuant to [IDO Subsection 14-16-6-4\(L\)](#), property owners within 330 feet and Neighborhood Associations within 660 feet may request a post-submittal facilitated meeting. If requested at least 15 calendar days before the public meeting/hearing date noted above, the facilitated meeting will be required. To request a facilitated meeting regarding this project, contact the Planning Department at devhelp@cabq.gov or 505-924-3955.

Useful Links

Integrated Development Ordinance (IDO):

<https://ido.abc-zone.com/>

IDO Interactive Map

<https://tinyurl.com/IDOzoningmap>

⁵ Available here: <https://tinyurl.com/idozoningmap>





DEKKER PERICH SABATINI

7601

MECHENBER CONSTRUCTION
8001 WASHINGTON ST NE
SUITE 4-5
ALBUQUERQUE NM 87113-1863



DEKKER PERICH SABATINI

7601

ESPINOSA EAST LLC
6900 E CAMELBACK RD SUITE 607
SCOTTSDALE AZ 85251-8044



DEKKER PERICH SABATINI

7601

RANCH JOINT VENTURE ATTN:
W L ROGERS
PO BOX 101149
FORT WORTH TX 76185-1149



DEKKER PERICH SABATINI

7601

STATE OF NEW MEXICO
COMMISSIONER OF PUBLIC LANDS
PO BOX 1148
SANTA FE NM 87504-1148



DEKKER PERICH SABATINI

7601

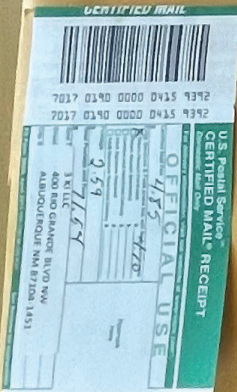
UP B LLC
6900 E CAMELBACK RD SUITE 607
SCOTTSDALE AZ 85251-8044



DEKKER PERICH SABATINI

7601

ALBUQUERQUE AIR CENTER INC C/O T
J DEAN
PO BOX 18006
ALBUQUERQUE NM 87119-0006



DEKKER PERICH SABATINI

7601

3 KILLC
400 RIO GRANDE BLVD NW
ALBUQUERQUE NM 87104-1451



DEKKER PERICH SABATINI

7601

CITY OF ALBUQUERQUE PO
BOX 1293
ALBUQUERQUE NM 87103

U.S. Postal Service
CERTIFIED MAIL RECEIPT
OFFICIAL USE

7017 0190 0000 0415 9224
7017 0190 0000 0415 9224

2.59 4.75 4.75

ALBUQUERQUE NM 87133-0000

DEKKER PERICH SABATINI

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ALBUQUERQUE NM 87133-0000

U.S. Postal Service
CERTIFIED MAIL RECEIPT
OFFICIAL USE

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7017 0190 0000 0415 9224

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ALBUQUERQUE NM 87133-0000

DEKKER PERICH SABATINI

7601

ALBUQUERQUE INTERNATIONAL AIRPORT C/O DEKKER PERICH SABATINI

U.S. Postal Service
CERTIFIED MAIL RECEIPT
OFFICIAL USE

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7017 0190 0000 0415 9224

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ALBUQUERQUE NM 87133-0000

DEKKER PERICH SABATINI

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ALBUQUERQUE NM 87133-0000

CERTIFIED MAIL

U.S. Postal Service
CERTIFIED MAIL RECEIPT
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7017 0190 0000 0415 9224

2.59 4.75 4.75

ALBUQUERQUE NM 87133-0000

DEKKER PERICH SABATINI

7601

CITY OF ALBUQUERQUE
ATTN: REAL PROPERTY DIVISION
PO BOX 1293
ALBUQUERQUE NM 87103-1293

CERTIFIED MAIL

U.S. Postal Service
CERTIFIED MAIL RECEIPT
OFFICIAL USE

7017 0190 0000 0415 9231
7017 0190 0000 0415 9231

2.59 4.75 4.75

ALBUQUERQUE NM 87133-0000

DEKKER PERICH SABATINI

7601

SAFE TEE HANGARAGE LLC 4916
GLENWOOD MILLS DR NE
ALBUQUERQUE NM 87111-2963

CERTIFIED MAIL

U.S. Postal Service
CERTIFIED MAIL RECEIPT
OFFICIAL USE

7017 0190 0000 0415 9217
7017 0190 0000 0415 9217

2.59 4.75 4.75

ALBUQUERQUE NM 87120-0000

DEKKER PERICH SABATINI

7601

WEST MESA AVIATION
7401 ATRISCO VISTA BLVD NW
ALBUQUERQUE NM 87120-6601

CERTIFIED MAIL

U.S. Postal Service
CERTIFIED MAIL RECEIPT
OFFICIAL USE

7017 0190 0000 0415 9248
7017 0190 0000 0415 9248

2.59 4.75 4.75

ALBUQUERQUE NM 87120-0000

DEKKER PERICH SABATINI

7601

UNITED STATES OF AMERICA 901
ATRISCO VISTA BLVD NW
ALBUQUERQUE NM 87120-0000

From: [Rebecca Shank](#)
To: [aboard111@gmail.com](#); [elizabethkayhaley@gmail.com](#)
Cc: [Sandoval, Christina M.](#); [Manriquez, Manny](#); [lrabeyta@cabq.gov](#); [bboyd@cabq.gov](#); [Will Gleason](#); [Jessica Lawlis](#); [Vikki Everett](#); [Athulya Usha Menon](#)
Subject: Double Eagle Airport Public Open House Invite
Date: Tuesday, June 11, 2024 4:54:00 PM
Attachments: [DEII Public Open House Invitation Flyer06_11_24.pdf](#)
[image001.png](#)

Hello Rene and Elizabeth,

We are reaching out to formally invite West Side Coalition of Neighborhood Associations to Double Eagle Airport (DEII), June 20th from 4-6pm, to review the materials related to an upcoming EPC hearing on a proposed amendment to the airport's Master Plan. The amendment is a secondary request which would add a governing site plan and design guidelines to facilitate the non-aeronautical uses recommended for approval by the EPC April 2024. While DEII is not technically within the range of a Neighborhood Association for public notice as required by the IDO/ONC, the Department of Aviation would like to involve the community and receive feedback on the Master Plan Design Guidelines, prior to the EPC Hearing. Please join us to review the proposed materials and provide your feedback.

If you have any questions, please do not hesitate to reach out.

Thank you,

**DEKKER
PERICH
SABATINI**
**Architecture
in Progress**

Rebecca Shank
Urban Planning Associate
Dekker Perich Sabatini
505.761.9700 / dpsdesign.org
[Keep up with DPS news and updates on LinkedIn](#)

From: [Rebecca Shank](#)
To: aboard111@gmail.com; elizabethkayhaley@gmail.com
Cc: [Sandoval, Christina M.](#); [Manriquez, Manny](#); lrabeyta@cabq.gov; bboyd@cabq.gov; [Will Gleason](#); [Jessica Lawlis](#); [Vikki Everett](#); [Athulya Usha Menon](#)
Subject: RE: Double Eagle Airport Public Open House Invite
Date: Wednesday, July 31, 2024 9:31:00 AM
Attachments: [image001.png](#)
[image002.png](#)

Good Morning Rene and Elizabeth,

As a follow up from our June 20th Open House, we have some clarifications to advise.

Our EPC application was submitted on July 3rd and is currently under review. Planning Staff has asked we clarify with the West Site Coalition of Neighborhood Associations that the Double Eagle Airport (DEII) application was for the EPC review of an as-built Site Plan and proposed Design Standards as neither are currently in place which is a requirement of the NR-SU Zoning District. Furthermore, since our Open House, “Permissive Uses” has been removed from the proposed Design Standards.

The as-built Site Plan and Design Standards have been updated and documents can be found in this folder:

 [7.3.24 DEII Submittal](#)

If you have anyone has questions, please do not hesitate to reach out of forward my contact information.

Thank you,

Rebecca Shank

Urban Planning Associate | **Dekker**

From: Rebecca Shank
Sent: Tuesday, June 11, 2024 4:55 PM
To: aboard111@gmail.com; elizabethkayhaley@gmail.com
Cc: Sandoval, Christina M. <cmsandoval@cabq.gov>; Manriquez, Manny <mmanriquez@cabq.gov>; lrabeyta@cabq.gov; bboyd@cabq.gov; Will Gleason <WillG@dpsdesign.org>; Jessica Lawlis <JessicaL@dpsdesign.org>; Vikki Everett <VikkiE@dpsdesign.org>; Athulya Usha Menon <AthulyaM@dpsdesign.org>
Subject: Double Eagle Airport Public Open House Invite

Hello Rene and Elizabeth,

We are reaching out to formally invite West Side Coalition of Neighborhood Associations to

Double Eagle Airport (DEII), June 20th from 4-6pm, to review the materials related to an upcoming EPC hearing on a proposed amendment to the airport's Master Plan. The amendment is a secondary request which would add a governing site plan and design guidelines to facilitate the non-aeronautical uses recommended for approval by the EPC April 2024. While DEII is not technically within the range of a Neighborhood Association for public notice as required by the IDO/ONC, the Department of Aviation would like to involve the community and receive feedback on the Master Plan Design Guidelines, prior to the EPC Hearing. Please join us to review the proposed materials and provide your feedback.

If you have any questions, please do not hesitate to reach out.

Thank you,



Rebecca Shank

Urban Planning Associate

Dekker Perich Sabatini

505.761.9700 / dpsdesign.org

[Keep up with DPS news and updates on LinkedIn](#)

DEII as-built Site Plan - EPC Sign Posting locations



DEII as-built Site Plan - EPC Sign Posting locations

Sign Posting 1



Sign Posting 2



Sign Posting 3



Sign Posting 4



F) NEIGHBORHOOD MEETING REPORT



DOUBLE EAGLE AIRPORT MASTER PLAN AMENDMENT

You're Invited to a Public Open House!

The City of Albuquerque Aviation Department is in the process of amending the Double Eagle II Airport Master Plan to facilitate a wider range of non-aeronautical uses that will generate revenue for the airport. You are invited to join the City and its consultant, Dekker Perich Sabatini, to discuss the associated Site Development Plan and Design Guidelines to create a prosperous, cohesive, and safe airport.

REVIEW THE PROPOSED DESIGN GUIDELINES AND
PROVIDE YOUR FEEDBACK!

THURSDAY, JUNE 20TH 4:00PM-6:00PM

DOUBLE EAGLE AIRPORT, CITY OF ALBUQUERQUE BUILDING
7401-A PASEO DEL VOLCAN NW, ALBUQUERQUE, NM 87120

- Review the new Master Plan Site Plan reflecting present day and proposed land uses.
- Review Design Guidelines including: site design, height restrictions, architectural standards, parking and loading, landscaping, buffering and screening standards.
- Provide comments and feedback.

QUESTIONS? CONTACT US

Christina Sandoval
City Aviation Department
(505) 224-7819
cmsandoval@cabq.gov

Rebecca Shank
Dekker Perich Sabatini
(505) 761-9700
RebeccaS@dpsdesign.org

DIRECTIONS:

From Double Eagle Road:

- Enter through the DEII Mid Field Gate.
- The City of Albuquerque Building is the first building on your right.

<https://maps.app.goo.gl/RYP6PGTXQks6rsqGA>





DOUBLE EAGLE AIRPORT MASTER PLAN AMENDMENT

PROJECT OVERVIEW



WHAT IS THE AIRPORT MASTER PLAN?

An airport master plan is a comprehensive study that is used to determine the development plans for an airport. DEII Airport Master Plan was enacted in 2003, with subsequent amendments in 2018 and 2024.

WHAT IS THIS AMENDMENT FOR?

The City of Albuquerque (the City), through the Integrated Development Ordinance (IDO) which was adopted in 2018, requires a site plan and design guidelines for DEII. At the time of the 2003 Master Plan, these documents had not been developed. A condition of adoption for the 2024 DEII Master Plan amendment, which includes non-aeronautical uses, will require approval of a site plan and design guidelines.

WHAT IS THE AMENDMENT PROCESS?

DEII is zoned NR-SU: Non-Residential Sensitive Use. The City of Albuquerque Economic Planning Commission (EPC) must review the Site Plan and provide their recommendation to Planning and Zoning for implementation. The City of Albuquerque Aviation Department, contracted with Dekker Perich Sabatini, will be submitting an EPC application to establish a Site Plan documenting existing uses conditions which will be the guide for future development.

WHAT IS THE FOCUS OF THE SITE PLAN AMENDMENT?



ECONOMIC DEVELOPMENT

DEII is not funded by tax payer dollars. Revenue is generated from sources such as land leasing, aviation fees or goods and services. Economic development ventures that provide revenue are vital to the longevity and success of the facility.



LAND USE

Permissive airport uses are changing nationally to allow for a variety of aeronautical and non-aeronautical uses on site, similar to other aviation facilities within the city.



CIRCULATION

Site design standards for clear site circulation patterns, which are vital safety measures for interaction of aeronautical and non-aeronautical uses.



SITE DESIGN

Ensure a variety of aeronautical and non-aeronautical uses permitted on site, similar to other aviation facilities within the city.



OPEN SPACE PROTECTIONS

DEII is adjacent to the Petroglyph National Monument to the east. It is important to protect the views of sensitive area's, limit environmental impacts, and protect natural habitats and vegetation.





DOUBLE EAGLE AIRPORT MASTER PLAN AMENDMENT

DRAFT SITE PLAN

OVERALL FACILITY SITE PLAN

A Site Development plan is an architectural document that acts as a blueprint for a property, depicting its general uses, layout, and configuration. The Amendment consists of two (2) site development plans. The first depicts the entire extent of the airport's approximate 4,000 acres, while the second focuses on the existing aviation faculties. The parcels currently available for lease are shown on the overall site plan. This plan also highlights the site's relationship to adjacent properties and open space.



a View looking east toward Petroglyph National Monument



b Primary entrance to the Double Eagle -II Airport



c View from East Side of Atrisco Vista of North Side DEII



d Entrance to the Petroglyph National Monument



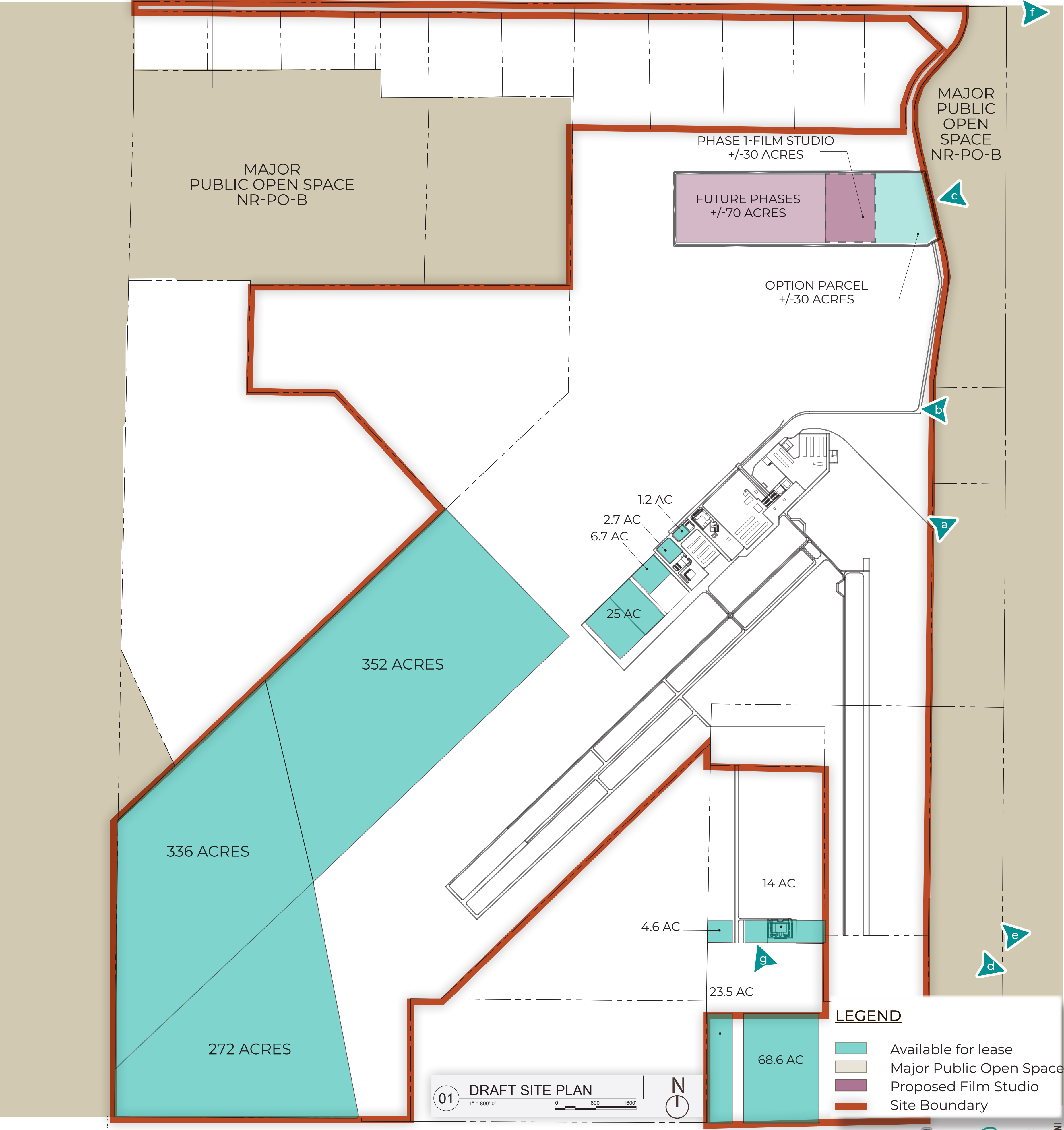
e The Petroglyph National Monument Signage



f Paseo de la Mesa trail entrance



g View looking West from the S.A.M.S. Building





DOUBLE EAGLE AIRPORT MASTER PLAN AMENDMENT

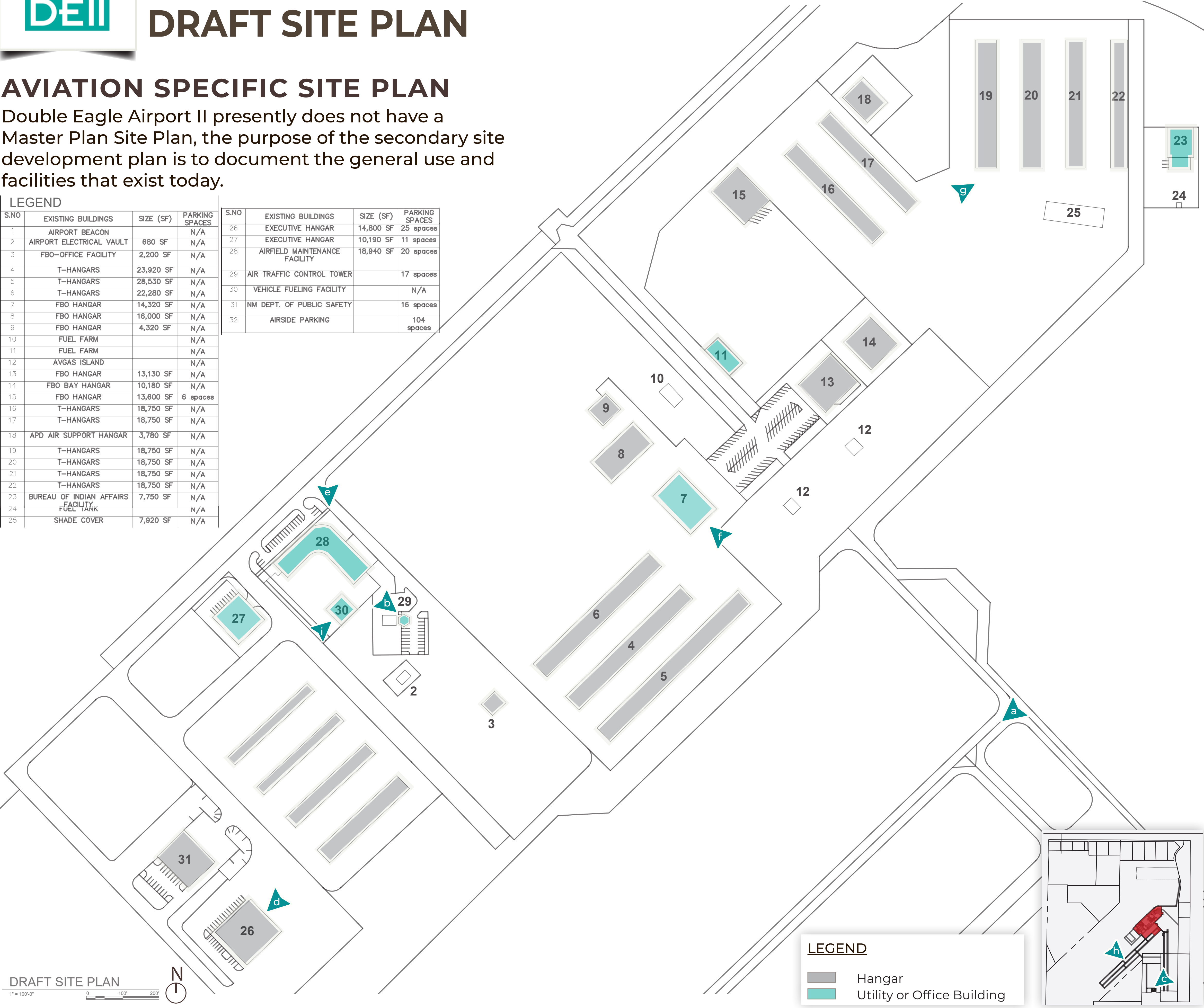
DRAFT SITE PLAN

AVIATION SPECIFIC SITE PLAN

Double Eagle Airport II presently does not have a Master Plan Site Plan, the purpose of the secondary site development plan is to document the general use and facilities that exist today.

LEGEND			
S.NO	EXISTING BUILDINGS	SIZE (SF)	PARKING SPACES
1	AIRPORT BEACON	N/A	N/A
2	AIRPORT ELECTRICAL VAULT	680 SF	N/A
3	FBO-OFFICE FACILITY	2,200 SF	N/A
4	T-HANGARS	23,920 SF	N/A
5	T-HANGARS	28,530 SF	N/A
6	T-HANGARS	22,280 SF	N/A
7	FBO HANGAR	14,320 SF	N/A
8	FBO HANGAR	16,000 SF	N/A
9	FBO HANGAR	4,320 SF	N/A
10	FUEL FARM	N/A	N/A
11	FUEL FARM	N/A	N/A
12	AVGAS ISLAND	N/A	N/A
13	FBO HANGAR	13,130 SF	N/A
14	FBO BAY HANGAR	10,180 SF	N/A
15	FBO HANGAR	13,600 SF	6 spaces
16	T-HANGARS	18,750 SF	N/A
17	T-HANGARS	18,750 SF	N/A
18	APD AIR SUPPORT HANGAR	3,780 SF	N/A
19	T-HANGARS	18,750 SF	N/A
20	T-HANGARS	18,750 SF	N/A
21	T-HANGARS	18,750 SF	N/A
22	T-HANGARS	18,750 SF	N/A
23	BUREAU OF INDIAN AFFAIRS FACILITY	7,750 SF	N/A
24	FUEL TANK	N/A	N/A
25	SHADE COVER	7,920 SF	N/A

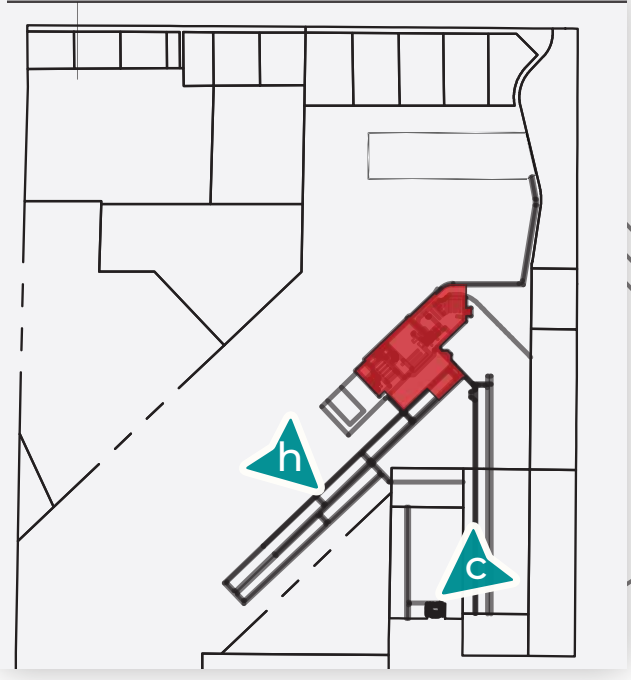
S.NO	EXISTING BUILDINGS	SIZE (SF)	PARKING SPACES
26	EXECUTIVE HANGAR	14,800 SF	25 spaces
27	EXECUTIVE HANGAR	10,190 SF	11 spaces
28	AIRFIELD MAINTENANCE FACILITY	18,940 SF	20 spaces
29	AIR TRAFFIC CONTROL TOWER		17 spaces
30	VEHICLE FUELING FACILITY		N/A
31	NM DEPT. OF PUBLIC SAFETY		16 spaces
32	AIRSIDE PARKING		104 spaces



LEGEND

Hangar

Utility or Office Building



View of Northside Taxiway looking South



Air Traffic Control Tower



S.A.M.S. Building



Executive Hangar



City of Albuquerque Office Building



FBO



T-Hangars



View of Southside Taxiway looking East



Vehicle Fueling Facility



NM Department of Public Safety

DRAFT SITE PLAN

1" = 100'-0"

0 100' 200'

N



DOUBLE EAGLE AIRPORT MASTER PLAN AMENDMENT

DRAFT DESIGN GUIDELINES

Give Us Feedback!

These guidelines are in **DRAFT** form. The City of Aviation Department is presenting these drafts to the public prior to public hearing to respond to public feedback. Your input is critical!

Grab a post-it and provide your input!

INTRODUCTION

These design standards provide a framework to achieve the following goals. Unless stated otherwise, the standards are assumed to apply to all areas within the DEII boundary.

1. Create an attractive built environment that provides a unique sense of place and identity of DEII, and supports opportunities for non-aeronautical, revenue generating development.
2. Define a common aesthetic for site, architectural and landscape design that results in a consistent identity across the DEII property.
3. Encourage developments that further the success of DEII and are compatible with the surrounding context.

ALLOWED USES

DEII is zoned NR-SU, Non-Residential Sensitive Use, within the Airport Protection Overlay (APO). The following uses are permitted within this zone, and in accordance with the Double Eagle Master Plan, provided such uses are conducted in a compatible and harmonious matter to airport use.

1. Aeronautical uses
2. Non-aeronautical uses including
 - Commercial
 - Educational
 - Film studio
 - Hotel
 - Light industrial
 - Renewable energy generation

3. Wireless Telecommunications Facilities pursuant to approval by the FAA and the City Aviation Department and provided they comply with concealment requirements of the Integrated Development Ordinance (IDO) Section 14-16-4-3(E)(12)(a).
4. Accessory uses include fuel storage, including petroleum-based fuel.

SITE DESIGN

Site design should be utilized to create a functional environment for employees and visitors. Buildings on site should be designed in a manner that reinforces the identity of DEII. Future buildings shall be laid out to work with overall airport circulation corridors, with flight pathways and approaches, and be respectful of adjacent/proximate open spaces.

- All development shall comply with the City of Albuquerque's IDO and the Development Process Manual (DPM), except where FAA regulations supersede or is stated otherwise within these design standards
2. Developments shall not be located within 150 feet of any property lines that abut Major Public Open Space. See more detail about buffering in the screening section.
 3. Cross access easements between adjoining tracts are encouraged.
 4. Loading areas shall follow the general building and site design regulations for Non-Residential Uses of the City's IDO.
 5. Non-aeronautical developments shall provide at least one (1) minimum 250 square foot outdoor patio area for buildings over 30,000 square feet gross floor area. Development tracts with multiple buildings can aggregate requirements into one large gathering space. Patio areas should provide shade that covers a minimum 25% of the area, to the maximum extent feasible.
 6. Perimeter walls, if provided, shall comply with Section 14-16-5-7 of the City's IDO, except:

Walls/fences similar to IDO in regards to adjacent fence CS.

- Chain-link fencing is allowed for airport security fencing purposes and for the use of view fencing within the aviation area.
- The use of razor or barbed wire, wood fencing, or plastic vinyl fencing is not permitted unless required by the FAA

Dimensional standards should limit the height of "commercial" structures to 35'!

Minimum dimensional standards are summarized below:

Key Standards Summary Table	
Standards	Requirement
Setbacks	
Setback from Major Public Open Space	150 feet
Setback from Atrisco Vista Blvd	150 feet
Setback from Double Eagle Rd	50 feet
Setback from Adjoining Properties	25 feet
Property Line Setback	25 feet
Taxilane Setback	35 feet
Building Height	
All Building Height	85 feet maximum, provided they comply with FAA FAR Part 77
Parking Setbacks	
Setback from Major Public Open Space	100 feet*
Setback from Atrisco Vista Blvd and Double Eagle Rd	25 feet*
Setback from Property Line or Lease Line	10 feet, except when a shared parking lot is used
Lighting	
Streetlights, not including public ROW	
Parking Area Lights	
Pedestrian Lighting	
Any lighting on tracts adjacent to Major Public Open Space	20 feet

*Any lighting on tracts adjacent to Major Public Open Space

Orient windows to enjoy the views.

Keep Building Heights low to protect the skyline view from the volcanoes.

Keep building heights low enough to not protrude above the natural volcanic landscape as viewed from the east side of the river. Don't repeat Amazon.

maintain large setbacks so Buildings Don't set close to the Roadway in your face, blocking the view

Acoustic pollution from Data Centers would be extremely detrimental to Petroglyph National Monument. Add Regs to measure sound from the NPS Boundary





DOUBLE EAGLE AIRPORT MASTER PLAN AMENDMENT

DRAFT DESIGN GUIDELINES

Give Us Feedback!

These guidelines are in **DRAFT** form. The City of Aviation Department is presenting these drafts to the public prior to public hearing to respond to public feedback. Your input is critical!



SETBACKS

The use of building and parking area setbacks is required to provide space for the creation of a visually attractive entrance streetscape, provide a safe and compatible environment for aircraft, service vehicles, and pedestrians throughout DEII, and provide buffers to adjacent public open space. Elements required within these setbacks will be pedestrian walkways, screening devices, and landscape improvements.

1. Minimum building setbacks shall be as follows:

Setback Standards	Requirement
Setback from Major Public Open Space	150 feet
Setback from Atrisco Vista Blvd	150 feet
Setback from Double Eagle Rd	50 feet
Setback from Adjoining Properties	25 feet
Property Line Setback	25 feet
Taxilane Setback	35 feet

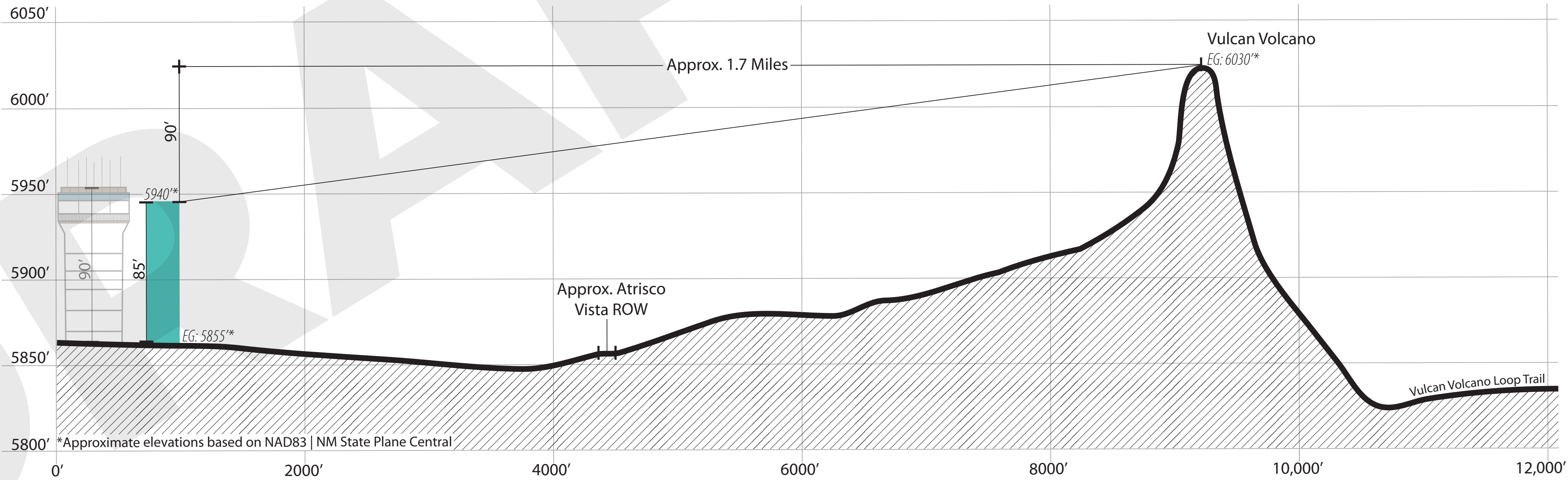
2. Buildings shall not be located closer to the airfield than the Building Restriction Line as shown on the “Airport Layout of the Plan” of the DEII Master Plan.
3. Buildings shall maintain a clear line of sight from the air traffic control tower controller positions to airfield pavements on the airfield.

BUILDING HEIGHT

- Maximum allowable building height **shall not exceed 85’**.
- Building heights and structures shall comply with FAA regulations. The FAA limits building and signage heights to below Federal Aviation Regulation (FAR) Part 77, Objects Affecting Navigable Airspace, surfaces including a 7:1 (horizontal to vertical from a distance 250 ft. from runway centerline) Transitional Surface and the Horizontal Surface Have the regulations in the reference

folder. located 150 ft. above airport elevation. Additionally, FAA FAR Part 77 states that any structure penetrating the plane created by a 100:1 sloped surface beginning at the nearest point, or the nearest runway must be airspaced by FAA in accordance with Part 77. Notification is required by completing and submitting SF 7460, Notice of Proposed Construction of Alteration, to the FAA for review and airspacing.

VIEWSHED FROM TOP OF VULCAN VOLCANO



Conceptual Cross Section of Vulcan Volcano at Petroglyph National Monument



Panoramic view of Petroglyph National Monument Spatter Cones East of Atrisco Vista Blvd



DOUBLE EAGLE AIRPORT MASTER PLAN AMENDMENT

DRAFT DESIGN GUIDELINES

BUILDING DESIGN

1. The scale, proportion, and composition of fenestration of facades of non-utilitarian buildings shall be designed to create visual interest.
2. Primary facades of non-aeronautical buildings greater than 100 feet in length should comply with the building design standards of Section 14-16-5-11 of the City's IDO where feasible.
3. Primary and Accessory buildings must comply with Major Public Open Space setback requirements.
4. The major entry to each non-aeronautical building should be clearly defined, face the primary circulation corridors, and connect to pedestrian pathways to the maximum extent feasible.
5. Columns, corner articulation, overhangs, awnings, gutters and scuppers, breezeways, and soffits shall be carefully dimensioned and detailed to provide a human scale and visual interest.
6. Special attention shall be given to the articulation of aircraft hangar facilities through use of color, materials, or building offsets. Metal structures are acceptable, provided they visually tie to other building elements, such as the front office, storage areas, etc.
7. Any glass or metal elements must comply with FAA standards addressing glint and glare.
8. Temporary buildings are subject to the temporary structure standards of Section 14-16-5-4-3 of the City's IDO and shall be allowed for a maximum period of one (1) year. After one-year, temporary structures must be removed or improved to meet DEII standards.

MATERIALS AND COLORS

1. Acceptable colors of exterior building materials for all development on DEII should take their cues from the native New Mexican landscape palette, including but not limited to: Sand, Terracotta, Blues and Teals, and Sage Green. Due to the proximity to the Major Public Open Space, subdued colors are preferred.
2. Buildings sited within 250' of the property line of Major Public Open Space shall:
 - a. Utilize colors that blend with the surrounding natural

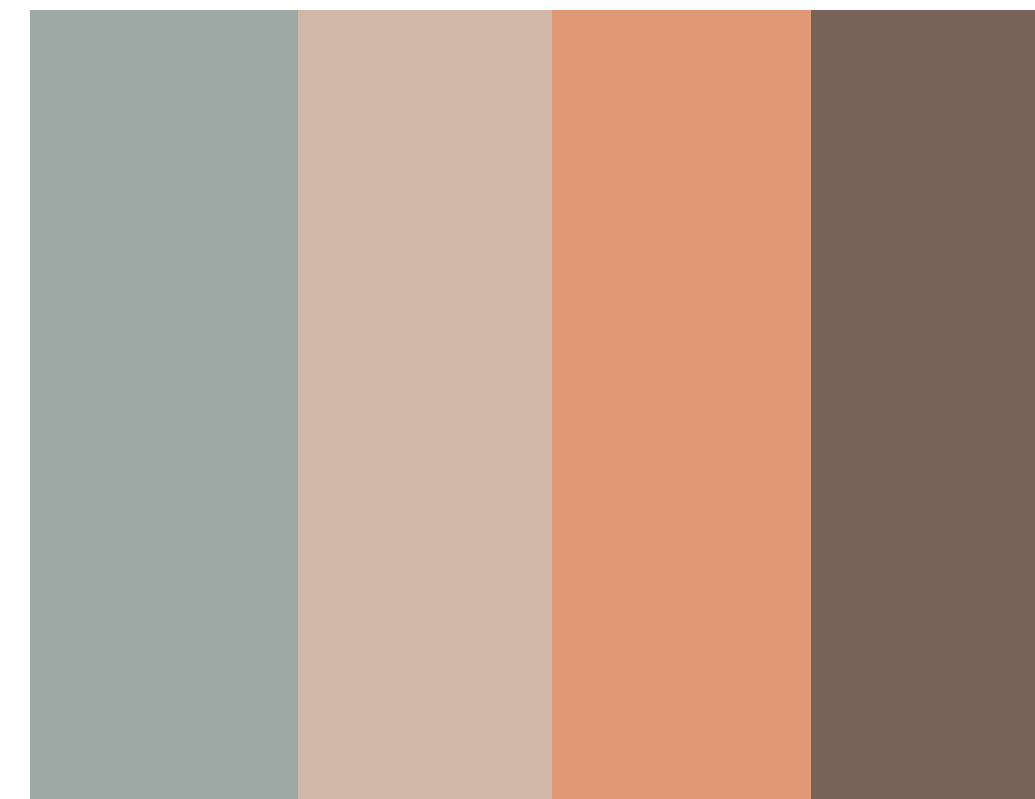
environment and generally include yellow ochres, browns, dull reds, and grey greens.

- b. Limit the colors of exterior surfaces of structures such as mechanical devices, roof vents, and screening materials to those with light reflective value (LRV) rating between 20 percent and 50 percent.
3. The use of accent colors and materials is encouraged to bring out detailing to better articulate or give scale to a building, including glazed tile, wood trim, tile roofing, paint, metal, etc.
 4. Special consideration shall be given to roof structures. The use of contrasting colors between roofs and walls is encouraged to help differentiate the planes of building masses.
 5. Prohibited building materials include the following:
 - a. Plastic or vinyl building panels, awnings, or canopies.
 - b. Exposed, untreated precision block or wood.
 - c. Highly reflective surfaces
 - d. Materials with high maintenance requirements
 - e. Buildings that are predominately white in color

General Colors adjacent to Petroglyph National Monument



General Colors of New Mexico Natural Landscape



BUILDING PRECEDENTS



Flying High Hangars



One Presidential Plaza



Altura Office Building



Pathway Office Park



Red Rock Office Building



Sennheiser Office Warehouse



Amazon Warehouse



DOUBLE EAGLE AIRPORT MASTER PLAN AMENDMENT

DRAFT DESIGN GUIDELINES

Give Us Feedback!

These guidelines are in draft form. The City of Aviation Department is presenting these draft to the public prior to public hearing to be able to respond to public feedback. Your input is critical!



ACCESS AND CIRCULATION

DEII is uniquely situated adjacent to a designated Major Public Open Space (Petroglyph National Monument) while operating as an airport servicing many types of vehicular traffic. Atrisco Vista along the east side of the airport is planned as a regional transportation corridor, providing access to and from Interstate 40. To provide safe and appropriate transportation infrastructure the following guidelines shall govern.

1. Access off Atrisco Vista Blvd shall be coordinated with NMDOT and other applicable agencies.
2. Sites should be designed to minimize conflicts between automobiles, trucks, bikes, and pedestrians to create an organized system of entrances, driveways, parking lots, and delivery areas.
3. DEII shall designate specific areas for the pickup and drop-off of passengers and guests utilizing rideshares, taxi, and guest transportation within the aviation areas.
4. Passenger and guest transport vehicles, including rideshare, taxis, shuttles, etc., shall proceed to the designated drop-off and pickup location at DEII.
5. Passenger and guest transport vehicles should not enter the airfield gates or travel elsewhere without explicit permission and/or escort by DEII personnel.
6. DEII shall designate delivery routes for the aviation areas of the property. All tenants must submit commercial delivery routes to DEII for approval.
7. A minimum six (6) foot clear pedestrian pathway shall be provided from each building to the internal circulation system and to adjacent roadways.

8. Commercial delivery trucks must enter DEII within the secured air operations area, follow defined DEII delivery routes, and must be escorted at all times; all unescorted commercial delivery traffic is prohibited.

TAXILANES

Aircraft circulation throughout DEII shall be developed with minimum interaction with vehicular traffic. Taxilane standards are based on an average tailing speed of at least 20 mph and wingspan and wingtip clearance. Taxilane access must have a minimum right-of-way width of 150 feet with a minimum paved surface width of 50 feet.

1. Taxilane shoulders must be either stabilized or paved to reduce the possibility of blast erosion and engine ingestion problems.
2. Minimum separation between centerline of taxilane to structure (parked aircraft, structure, etc.) is 0.70 times the wingspan of the largest airplane, plus 10 feet. Setback standards have been designed to accommodate all Type I and II aircraft.

Taxilane Standards	Requirement
Airplane Design Group	II
Taxilane Width	35 feet
Taxilane Object Free Area Width	124 feet
Taxilane Shoulder Width	15 feet
Wingspan (w) for Airplane Design Group	49 feet ≤ w 79 feet

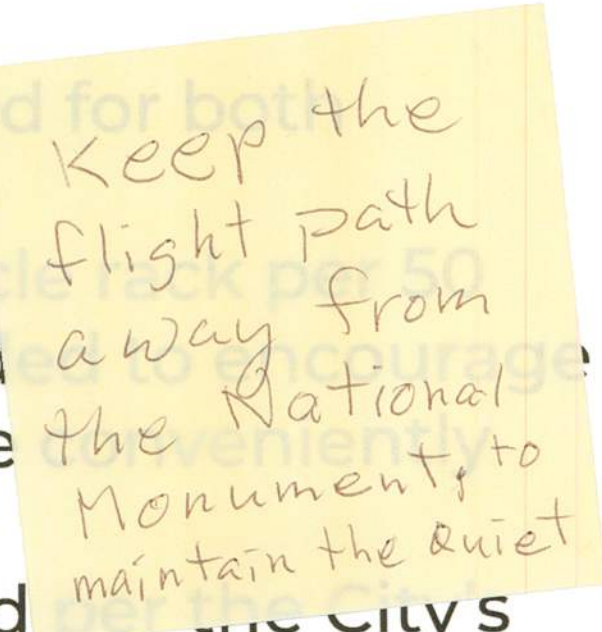
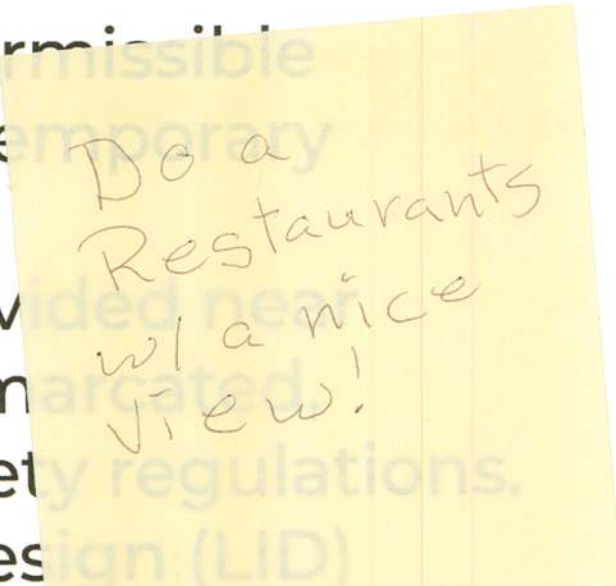
4. DEII facilities located adjacent to the airport taxilane system or service corridors are assumed to support aviation related activities, which may require outside storage of aircraft as well as airfield maintenance equipment.

PARKING

Parking areas should be designed to mitigate heat/glare through the provision of landscaping, minimize the visual impact of parking areas, and provide accessible and safe circulation within and adjacent to the parking areas. Off-

street parking areas, including minimum off-street parking requirements, space size, aisle widths, pedestrian pathways, and screening, shall comply with Section 14-16-5-5 of the City's IDO and the DPM.

1. Off-street parking areas, including minimum off-street parking requirements, space size, aisle widths, pedestrian pathways, and screening, shall comply with Section 14-16-5-5 of the City's IDO and the DPM.
 - a. Film Studio uses will utilize requirements for Light Industrial uses of the City's IDO.
2. The City Aviation Department shall approve the design of all parking areas on DEII property.
3. Unstriped or pervious parking areas are permissible around buildings of aeronautical uses for temporary airplane or vehicle parking.
4. Car/vanpool preferred parking shall be provided near building entrances and shall be clearly demarcated, provided they do not conflict with FAA safety regulations.
5. Parking lots should employ Low Impact Design (LID) techniques and direct surface runoff to landscaped water harvesting areas.
6. Alternate paving materials are permitted for both the vehicular and pedestrian zones.
7. Bicycle parking at a ratio of one (1) bicycle parking space shall be provided for non-vehicle use. Bicycle racks should be located near building entrances.
8. Bicycle racks or lockers shall be installed in accordance with the City's DPM.

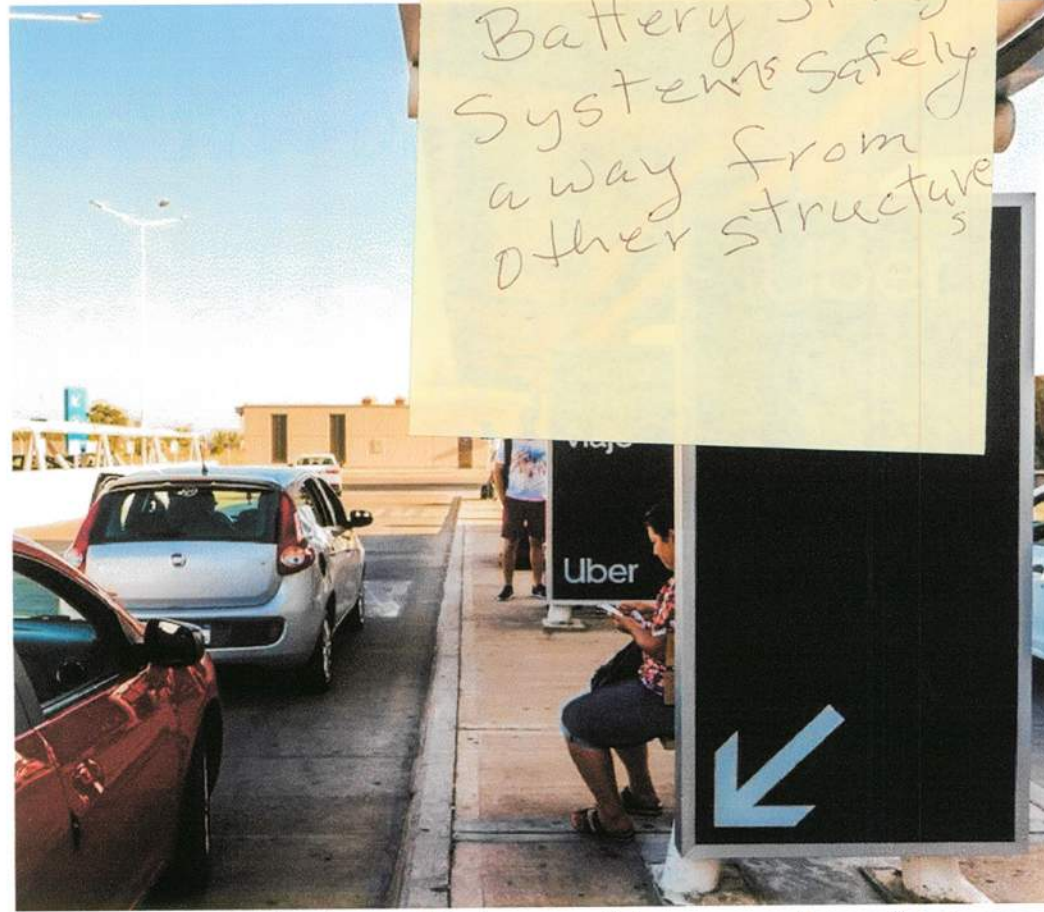


PERVIOUS SURFACE PRECEDENT



Pervious Surface Materials on Parking Lot

RIDESHARE PRECEDENT



Rideshare Pick-up with covered seating area



DOUBLE EAGLE AIRPORT MASTER PLAN AMENDMENT

DRAFT DESIGN GUIDELINES

Give Us Feedback!

These guidelines are in draft form. The City of Aviation Department is presenting these draft to the public prior to public hearing to be able to respond to public feedback. Your input is critical!



LIGHTING

Lighting should enhance the safety, security, and visual aesthetics of DEII. The primary objective of site lighting shall be to maximize public safety without impacting the FAA requirements of the site or generating unnecessary glare or reflection to adjacent properties or Major Public Open Spaces.

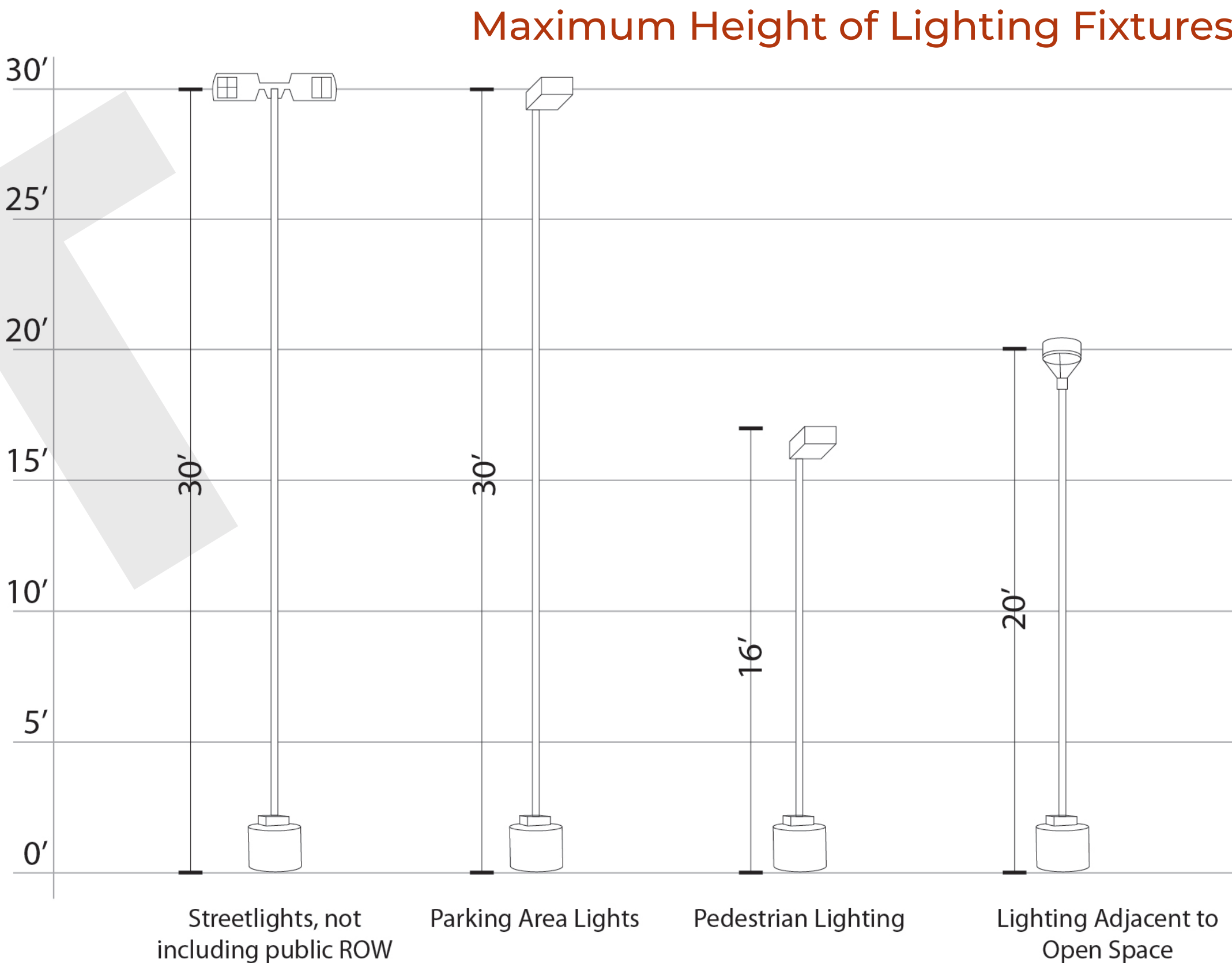
1. All lighting shall generally comply with the outdoor lighting standards of Section 14-16-5-8 of the City's IDO
2. Specific lighting fixtures styles shall be established by / reviewed by DEII and shall be consistent with other projects within DEII property. Lighting fixtures shall be fully shielded horizontal lamps to eliminate light trespass beyond the property line and no light source shall be visible from the site perimeter except where FAA regulations and standards may vary.
3. The maximum height standards of light fixtures shall not exceed the following:

Lighting Standards Table	Requirement
Streetlights, not including public ROW	30 feet
Parking Area Lights	30 feet
Pedestrian Lighting	16 feet
Any lighting on tracts adjacent to Major Public Open Space	20 feet

SIGNAGE

Signage should create a sense of arrival to DEII and ensure proper wayfinding for visitors, employees, and deliveries.

1. All signage shall follow signage regulations of Section 14-16-5-12 of the City's IDO unless otherwise noted herein.
2. Signage design and locations shall be approved at the discretion of DEII. Monument signage shall be coordinated to have the same appearance as established precedents on DEII property.
3. Future development of tracts is limited to two (2) monument signs per the City's IDO size requirements.
4. Directional signs to direct visitors and delivery services may be provided at the discretion of DEII.
5. Prohibited signs include the following:
 - a. Banners, pennants, ribbons, streamers.
 - b. Brashly colored signs with moving or flashing lights.
 - c. Animated, illuminated, flashing, or electronic signs.
 - d. Portable signs.
 - e. Off-premises signs referring to a business or merchant not conducting business on the premise where the sign is displayed.
6. Temporary banners, pennants, or ribbons may be permitted for special, time-limited events at the discretion of DEII. Temporary signage must be removed by the agreed-upon timeline and must comply with FAA requirements.
7. No sign may overhang a right-of-way or property line, intrude upon any architectural or decorative features, or interfere with clear sight requirements of the City's DPM.
8. Refer to the PNM Electric Service Guide at www.pnm.com for specifications.
9. When an above-ground backflow prevention device is required by the City of Albuquerque, the heated enclosure shall be constructed of materials compatible with the architectural materials used as the main elements of the building. If prefabricated fiberglass enclosures are used, they shall be appropriately screened from view by walls and/or landscaping.



SIGNAGE PRECEDENT



Signage from Aerospace Technology Park Entrance



Signage from Double Eagle Road Entrance



Signage from Tohono Chul Park Entrance



Tenant Signage Example



DOUBLE EAGLE AIRPORT MASTER PLAN AMENDMENT

DRAFT DESIGN GUIDELINES

LANDSCAPING

Site landscape shall serve to enhance the visual aesthetic of the campus and aid in reinforcing the street presence and pedestrian experience without interfering with the site's primary aviation function. The primary focus for site landscape should emphasize visitor and employee centric spaces of the campus including the main entry, primary buildings entrances, pedestrian pathways, and patios or gathering spaces. The intent is to provide year-round color and interest and reflect the natural environment of New Mexico landscape.

1. Landscaping plans should generally comply with the Section 14-16-5-6 of the City's IDO. On-site uses may utilize Alternative Landscape Plans to ensure site compliance with FAA regulations or optimize operations.
 - a. Aeronautical uses shall not be required to comply with the City's IDO landscaping to ensure compliance with FAA requirements.
 - b. Non-aeronautical uses may request relief from standards that interfere with or compromise their functionality, for example parking lot landscape requirements interfering with the functionality of parking lots within film studios.
 - c. Total net site and coverage requirements should be calculated for the entire DEII site, provided that only the developed portions of the site are used for calculations.
2. Future development tracts within DEII property shall be maintained in their natural condition until developed. If these areas are disturbed, they shall be re-vegetated with native seed mix appropriate for this region.
3. Development tracts adjacent to the Major Public Open Space shall provide a minimum six (6) foot landscape strip that varies in width to avoid the appearance of a hard, straight line. Plant coverages and selections within this buffer shall provide a visual barrier, with the exception of areas within the Airport Runway Protection Zone of the City's IDO.
4. All plant material shall be maintained in a living, attractive condition. All areas shall be maintained and be free of weeds.

6" requirement is confusing since the setback is 150-100'

5. Due to the aviation nature of the site, trees should only be provided along the streetscape of Double Eagle Rd, within patio or gathering spaces, and in landscape buffers adjacent to Major Public Open Space. Tree selections and locations shall be coordinated with DEII to ensure they do not conflict with aviation operations.
6. A specific landscape palette may require review by the City Aviation Department's USDA Wildlife Biologist and conform to current FAA regulations, including those restrictions of specific plants and trees that attract animals that conflict with aviation operations.
 - a. The use of native or regionally adapted plant materials that will thrive in local conditions with less fertilizer, water, and maintenance is encouraged.
 - b. Fruit and nut trees or shrubs, which may attract wildlife, shall not be permitted on DEII.
 - c. High water use turf is restricted to patio or public gathering areas within DEII.
7. Landscape design shall incorporate active and passive water harvesting techniques to reduce potable water use for irrigation where possible.
8. All landscape areas shall be stabilized with mulches or plant material to keep soils from washing or blowing away. Gravel mulch, bark mulch, and similar materials are acceptable as a top dressing for landscape areas.
9. An automatic underground irrigation system is required to support all site landscaping. The system shall be designed to avoid over-spraying onto pedestrian paths, buildings, fences, etc.
10. Landscaping shall not interfere with clear sight requirements of the City's DPM. Trees and shrubs between 3 and 8 feet in height (as measured from the gutter pan) shall not be acceptable in this area.

SCREENING

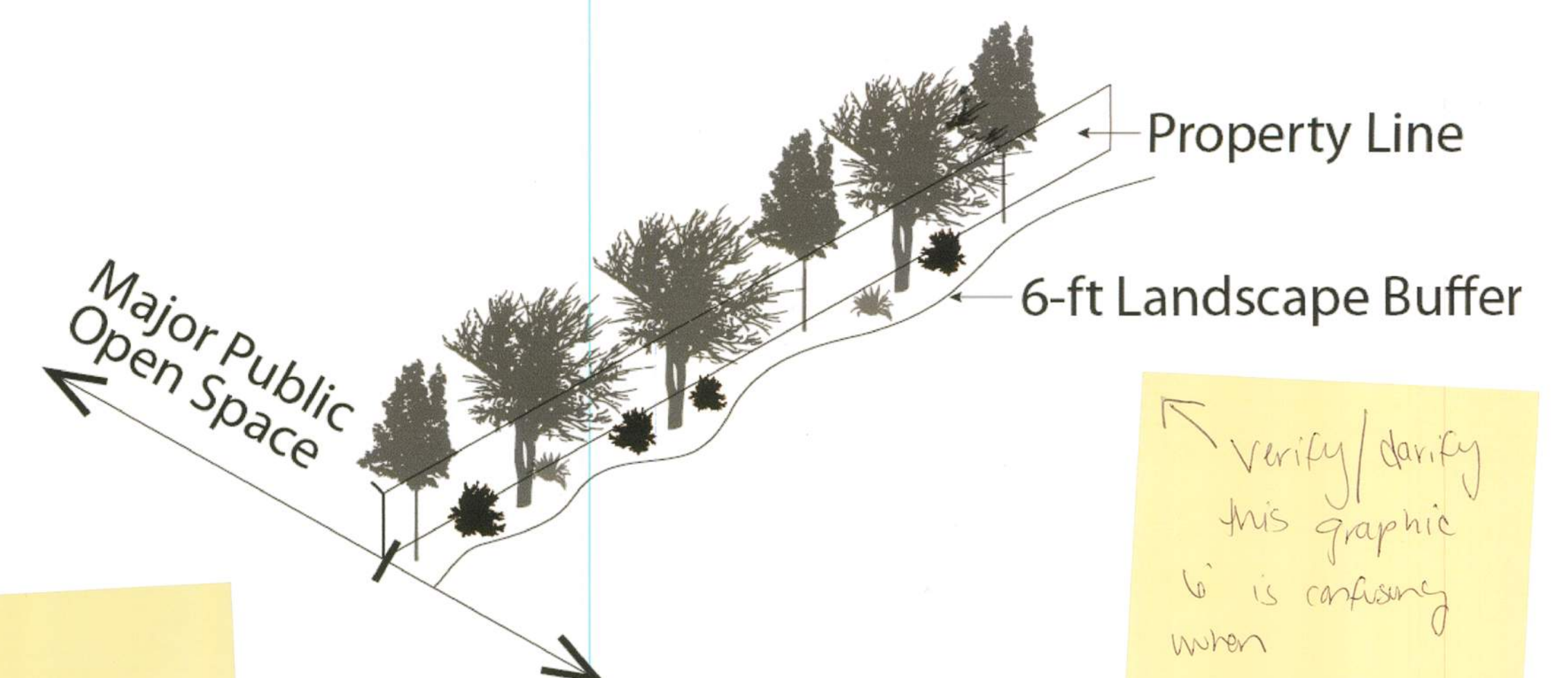
Screening of less attractive areas, including loading, service, and/or mechanical area, including refuse facilities, mechanical equipment storage yards, and loading docks, should be accomplished using walls and fences or landscape buffers that are designed to be an integral part of DEII's overall aesthetic. All such functions except for airplanes viewed from taxilanes, should be

designed in an aesthetically pleasing manner, or screened from Atrisco Vista Blvd, and from any designated Major Public Open Spaces.

1. Parking areas adjacent to Atrisco Vista Blvd, other public rights-of-ways, or within 100 feet of Major Public Open Space shall be screened through measures including not limited to perimeter walls, landscaping, or the use of buildings.
2. Loading, service, and refuse areas shall be integrated into the building design if possible or be located so as to be visible from Atrisco Vista Blvd, or the Major Public Open Space. If not, they must be screened from view by an opaque wall or fence at least six (6) feet tall that is compatible with materials and design of the building with which it is associated.
3. Mechanical equipment shall be screened from Atrisco Vista Blvd, other public right-of-way, and the Major Public Open Space pursuant to Section 14-16-5-5-6 of the City's IDO and be compatible with materials and design of the building with which it is associated.
4. Passenger loading areas do not require screening.

Loading, service & refuse areas must be located so as not to be visible from mpos. Screening above is inadequate

Landscape Buffer to Major Open Space



Verify/clarify this graphic as it is confusing when



DOUBLE EAGLE AIRPORT MASTER PLAN AMENDMENT

DRAFT DESIGN GUIDELINES

SUSTAINABILITY

Green architectural design is encouraged, with a particular emphasis on creating facilities that do not rely on carbon-producing sources of energy. DEII promotes the use of sustainable design principles, environmentally responsible building concepts, and lower embodied carbon building practices. DEII supports the use of solar photovoltaic, wind turbines (if compliant with glint and glare studies), ground-source heat pumps, and other alternative energy production facilities on site.

1. Stormwater control measures shall be designed to manage the first flush and control runoff generated by contributing impervious surfaces.
2. Buildings shall be oriented to take advantage of heat gain in the winter where possible while coordinating with shading strategies to inhibit solar gain in the summer.
3. Roofs shall drain water to areas which are landscaped appropriately for such run-off.
4. Permeable paving may be used on site within parking areas, or pedestrian areas provided it does not compromise the ability to carry the applicable loads required for parking, vehicular activity including freight trucks, or trash hauling vehicles.
5. Parking islands shall allow flow of water as required to maintain proper drainage of the site.
6. Grasses and other ground vegetation should be placed near project edges to help filter and slow runoff as it exits and enters the site.

UTILITIES

ility systems should ensure the minimum expectations for the design of new or renovated service connections at DEII. The systems shall enhance the safety measures of FAA and the aesthetic of DEII.

1. Developers shall contact PNM's New Service Delivery Department to coordinate electric service and options for the location of electric service connection. Any existing or proposed public utility distribution easements are to be indicated on subsequent Site Plan for Building Permit utility sheets. PNM's standard for public utility distribution easements is 10 feet in width to ensure adequate, safe clearances.
2. All electric distribution lines shall be placed underground.
3. Transformers, utility pads, and telephone boxes shall be appropriately screened with walls and/or vegetation when viewed from the public right-of-way. Screening of transformers, utility boxes and other utility structures shall not impede access to the equipment and shall provide for safe maintenance and repair by utility workers.
4. Transformers, utility pads, and ground-mounted equipment screening shall allow for access to utility facilities. All screening and vegetation surrounding ground mounted transformers and utility pads are to allow 10 feet of clearance in front of the equipment door and 5-6 feet of clearance on the remaining three sides for safe operation, maintenance, and repair purposes.

SUSTAINABILITY PRECEDENTS



Bicycle Technologies International Warehouse



Existing Stormwater Control at NM State Police Hangar



Existing Stormwater Control from an Executive Hangar



Landscape Improvements within Drainage Areas



Landscape Improvements within Drainage Areas



Ground Vegetation within Stormwater Control Area



Parking Island Stormwater Control Area

DOUBLE EAGLE AIRPORT MASTER PLAN

PUBLIC OPEN HOUSE

DATE: 06.20.2024 TIME: 4:00-6:00 PM



Name	Affiliation	Email	Phone
Darren Sosebee	Resident	mb422@comcast.net	
Artie Davidson	Resident	NMUTVOL@AOL.COM	
Harry Relkin	Mesa Film Studios	hurrelkin@nalkalifornia.com	
Deb Salvato	Resident	debsalvato@comcast.net	
Libby Engdahl Whiteley	Resident	libbyabg88@gmail.com	
Tim Gorman	Tenant	tim.gorman@flyback	505-884-4530
Jane Baechle	Santa Fe Village N.A	Jane.Baechle@gmail.com	505-400-6516
Alan Schwartz	RSNA	aschwartz74@comcast.net	505 890 3542
René Horvath	WSCONA	aboard111@gmail.com	505 898-2114
Desirae Shorter	Vis. Senator Ben R. Lujan	desirae_shorter@lujan.senate.gov	(505) 337-7023
Mike Voorhees	WSCONA	mike@cyonic.com	505-453-3573
Leila Momenzadeh	Bernalillo County	lmomenzadeh@bernco.gov	505-252-2837
Rick Tietzen	AECOM	rick.tietzen@aecom.com	
Elizabeth Haley	WSCONA	elizabeth.haley@gmail.com	505 908 5576
Yanira Guroda	NM legis	Yanira.guroda@NM.legis.gov	

PUBLIC OPEN HOUSE

DATE: 06.20.2024 TIME: 4:00-6:00 PM

[illegible]

PUBLIC OPEN HOUSE

[illegible]

G) AGENCY COMMENT



United States Department of the Interior

NATIONAL PARK SERVICE
Petroglyph National Monument
6001 Unser Blvd NW
Albuquerque, New Mexico 87120



PETR 1.A.1.

August 5, 2024

Re: Double Eagle II Site Plan – EPC application comments; Project # PR-2018-001577
SI-2024-00994 – Site Plan-EPC

Dear Environmental Planning Commission,

Thank you for the opportunity to provide feedback on the site plan and design guidelines for the Double Eagle II airport located west of Petroglyph National Monument. I am writing on behalf of the National Park Service (NPS) Petroglyph National Monument.

The NPS appreciates receiving a project overview provided by the City of Albuquerque Aviation Department to Petroglyph National Monument staff on April 11, 2024, and the design criteria provided to us for our review in June. We understand that the City of Albuquerque Aviation Department is no longer proposing to update the Double Eagle II Airport Master Plan and instead they are requesting Environmental Planning Commission (EPC) approval of a Site Plan and design guidelines.

As this property is very close to Petroglyph National Monument, located across Atrisco Vista Blvd, any development at the site would affect the views of the area from the volcanoes west to Mount Taylor. This cultural landscape and viewshed is considered by some area Pueblos and Tribes to be sacred. As the National Park Service and the City of Albuquerque are legislatively mandated to protect the Monument, we are very interested in the proposed non-aviation development on the Double Eagle II property.

Many of our comments related to setbacks, lighting and the protection of dark skies, building materials and colors, and screening have been addressed and are included in the design guidelines.

We are concerned, however, about the building height and lack of dimensional standards proposed to be authorized on the site. Buildings as tall as 85 feet would be allowed 250 feet from Atrisco Vista Blvd, and buildings as tall as 60 feet would be allowed 150 feet from Atrisco Vista. In our prior meeting, the City of Albuquerque Aviation Department provided us with information that the maximum building height would be around three stories (30-36 feet) with the exception of the control tower. While the Master Plan must allow for the control tower, as currently written, the plan would allow for future structures of any use, any square footage, and heights up to 85 feet tall.

As we've seen from recent development south of the airport, tall buildings detract from the viewshed and cultural landscape of the area and mar the landscape. For example, the Amazon buildings can be seen not only from Petroglyph National Monument, but from as far away as I-25. Double Eagle Airport II is even closer to the Monument and structures of this scale would intrude on the landscape and the experience of visitors to Petroglyph National Monument, including the Tribal people who hold this landscape sacred.

We cannot support building heights as currently included in the design guidelines and request the City of Albuquerque Aviation Department reconsider and institute a maximum building height of 36 feet in order to protect the values of the West Mesa and Petroglyph National Monument.

Petroglyph National Monument is one of the region's most significant, and large protected areas in Albuquerque. In 1989, community members, Pueblos and Tribes, the City of Albuquerque, and the New Mexico Congressional delegation pushed for the protection of a portion of the West Mesa to forever protect thousands of petroglyphs and the cultural heritage of the area. Shortly thereafter, on June 27, 1990, Petroglyph National Monument was designated by Congress. The community leaders and elected officials at that time committed to protecting this special place as a unit of the NPS "in light of the national significance of the West Mesa Escarpment and the petroglyphs and the urgent need to protect the cultural and natural resources of the area from urbanization and vandalism" Pub. Law 101-313, § 101 (1990).

The stated purpose of Section 14-16-5-2 of the IDO is to "minimize the impacts of development on natural and cultural resources." Petroglyph National Monument is a cultural landscape. The volcanoes are a visual reference point for the city, and contain significant and numerous archaeological sites. The entire Monument area is considered sacred to all 19 Pueblos and 10 additional tribes across the Southwest.

Help us protect Petroglyph National Monument following the ABC Comp Plan Goals and Policies and the IDO by limiting building heights on the Double Eagle II.

For more information, please contact Superintendent Nancy Hendricks (nancy_hendricks@nps.gov).

Sincerely,

A handwritten signature in cursive script that reads "Nancy Hendricks".

Nancy Hendricks
Superintendent

H) PUBLIC COMMENT

Jane Baechle
7021 Lamar Avenue NW
Albuquerque, NM 87120
Jane.Baechle@gmail.com

Date: August 5, 2024

To: Jonathan Hollinger
Chair, EPC

From: Jane Baechle
Member, SFVNA

Re: Project # PR-2018-001577
SI-2024-00994 – Site Plan-EPC

Dear Chair Hollinger and Commissioners,

I am submitting individual comments in *opposition* to the Master Plan for DEII as outlined in the application for this project. I agree that the City has engaged in a process of requesting public comment. I have participated in those including the Open House on June 20, 2024 and a subsequent meeting with City Aviation Department officials and City Planning staff. The proposed project, however, fails to address a central issue raised by me in both of those meetings.

That issue is the allowed height of structures within the DEII Master Plan area. This proposal clearly states that the allowed maximum height is 85'. Given that this area is zoned NR-SU, a zoning designation which includes no dimensional standards except those outlined by the Master Plan itself, this document, if approved, would permit future structures of any use and square footage to be 85' tall. There is no scenario in which such a structure could fail to intrude on the viewscape including of the western horizon, natural and cultural landscape or experience of visiting the Volcanoes area of the Petroglyph National Monument.

In the June 20, 2024 meeting, Planning Staff defended the 85' maximum height citing the Control Tower for Double Eagle Two airport. Clearly, the airport requires a control tower and the Master Plan must allow for that. Any other structure, however, should be limited in height to three stories. The existing hangars and other structures represent a height which currently functions for airport operations; there should be no option for taller structures than those. If Commissioners read all of the notes from the Open House attached to the proposal, you will find

my proposal to limit heights for commercial structures to 35' and multiple other comments opposing any taller structures and calling for protection of views.

Requirements for setbacks, appropriate colors of structures and natural landscaping are critically important and appreciated. Reassurances that additional projects beside the film studio are years in the future and subject to specific approvals before development are unpersuasive. As long as the allowed heights are 85', someone will build a structure of that height. This application fails to ensure context sensitive design and adherence to ABC Comp Plan Goals and Policies calling for the protection of natural and cultural landscapes.

I respectfully request that the Master Plan be denied until it establishes height limits and ensures the protections of the landscape described in the ABC Comp Plan. Policy 11.3.1 and specific sub-policies call for the protection of views and includes limits on building heights. Integration into the landscape requires more than simply using natural colors and native plants.

Thank you for your time and consideration.

Sincerely,

Jane Baechle

I) PROPOSED AS-BUILT SITE PLAN



SHEET INDEX

SDP 1.1	EXISTING SITE PLAN	AE 3.14	BUILDING ELEVATIONS
SDP 1.2	EXISTING SITE PLAN	AE 3.15	BUILDING ELEVATIONS
SDP 2.1	LANDSCAPE PLAN	AE 3.16	BUILDING ELEVATIONS
AE 3.1	BUILDING ELEVATIONS	AE 3.17	BUILDING ELEVATIONS
AE 3.2	BUILDING ELEVATIONS	AE 3.18	BUILDING ELEVATIONS
AE 3.3	BUILDING ELEVATIONS	AE 3.19	BUILDING ELEVATIONS
AE 3.4	BUILDING ELEVATIONS	AE 3.20	BUILDING ELEVATIONS
AE 3.5	BUILDING ELEVATIONS	AE 3.21	BUILDING ELEVATIONS
AE 3.6	BUILDING ELEVATIONS	AE 3.22	BUILDING ELEVATIONS
AE 3.7	BUILDING ELEVATIONS	AE 3.23	REFUSE ENCLOSURE
AE 3.8	BUILDING ELEVATIONS		DETAILS
AE 3.9	BUILDING ELEVATIONS	AE 3.24	FREE STANDING
AE 3.10	BUILDING ELEVATIONS		SIGNAGE DETAILS
AE 3.11	BUILDING ELEVATIONS	AE 3.25	LIGHTING DETAILS
AE 3.12	BUILDING ELEVATIONS	AE 3.26	FENCING, WALL
AE 3.13	BUILDING ELEVATIONS		ENCLOSURE DETAILS
		SDP 4.1	DESIGN GUIDELINES
		SDP 4.2	DESIGN GUIDELINES

PROJECT NUMBER: xxx

Application Number: xxx

This plan is consistent with the specific Site Development Plan approved by the Environmental Planning Commission (EPC), dated xxx, and the Findings and Conditions in the Official Notification of Decision are satisfied.

Is an Infrastructure List required? () Yes (X) No If yes, then a set of approved DRC plans with a work order is required for any construction within Public Right-of-Way for construction of public improvements.

DFT SITE DEVELOPMENT PLAN APPROVAL:

Traffic Engineering, Transportation Division	Date
ABCWUA	Date
Parks and Recreation Department	Date
Hydrology	Date
Code Enforcement	Date
*Environmental Heath Department (conditional)	Date
Solid Waste Management	Date
Planning Department	Date

*Environmental Health, if necessary
07/05/2024

DOUBLE EAGLE-II AIRPORT SITE PLAN

PROJECT TEAM

OWNER

CITY OF ALBUQUERQUE, AVIATION
ADMINISTRATION OFFICE, 3RD LEVEL
2200 SUNPORT BLVD. SE
ALBUQUERQUE, NM 87106

ARCHITECT

DEKKER
7601 JEFFERSON ST. NE, SUITE 100
ALBUQUERQUE, NM 87109
PHONE: 505.761.9700
FAX: 505.761.4222

SEAL

EXISTING CONDITIONS
NOT FOR
CONSTRUCTION

PROJECT

PR# 2018-001577
CASE# SI-2024-00994

DOUBLE-II EAGLE AIRPORT
SITE PLAN-EPC
7401 PASEO DEL VOLCAN NW
ALBUQUERQUE, NM 87121

REVISIONS

△
△
△
△
△
△

DRAWN BY

REVIEWED BY

DATE 08/8/2024
PROJECT NO. 21-0082.014

DRAWING NAME

EPC
SUBMITTAL

SHEET NO.

GENERAL SHEET NOTES

A. THIS SITE PLAN DEPICTS ALL KNOWN CONDITIONS AS DETERMINED FROM FIELD OBSERVATIONS AND DIGITAL RECORDS. IN THE EVENT THAT REVISIONS TO THIS SITE PLAN AND RELATED SHEETS IS REQUIRED, THE BURDEN OF DOCUMENTING COMPLIANCE WITH IDO STANDARDS FOR ELEMENTS LIKE PARKING, LANDSCAPING, SIGNAGE, OF A SPECIFIC BUILDING OR DEVELOPMENT WILL BE ON THE APPLICANT OF THAT REQUEST.

B. ALL DIMENSIONS ARE APPROXIMATE, AND BASED ON FIELD OBSERVATION AND DIGITAL RECORDS.

C. PARKING COUNTS WERE DETERMINED FROM FIELD OBSERVATIONS AND DIGITAL RECORDS.

D. LEASE AREA DENOTES EXISTING OR AVAILABLE GROUND LEASE THROUGHOUT DEII AIRPORT.

FACILITY INDEX			
S.NO	EXISTING BUILDINGS	SIZE (SF)	PROVIDED PARKING
A	AIRPORT ELECTRICAL VAULT	680 SF	N/A
B	FBO-OFFICE FACILITY	2,200 SF	N/A
C	T-HANGARS	23,920 SF	N/A
D	T-HANGARS	28,530 SF	N/A
E	T-HANGARS	22,280 SF	N/A
F	FBO*	14,320 SF	6 spaces
G	FBO HANGAR	14,833 SF	N/A
H	FBO HANGAR	4,320 SF	N/A
I	FUEL FARM		N/A
J	FUEL FARM		N/A
K	AVGAS ISLAND		N/A
L	FBO HANGAR	13,130 SF	N/A
M	FBO BAY HANGAR	10,180 SF	N/A
N	FBO HANGAR	13,600 SF	6 spaces
O1	T-HANGARS	18,750 SF	N/A
O2	T-HANGARS	18,750 SF	N/A
P	APD AIR SUPPORT HANGAR	3,870 SF	N/A
Q1	T-HANGARS	18,750 SF	N/A
Q2	T-HANGARS	18,750 SF	N/A
Q3	T-HANGARS	18,750 SF	N/A
Q4	T-HANGARS	18,750 SF	N/A
R	BUREAU OF INDIAN AFFAIRS FACILITY	7,750 SF	N/A
S	SHADE COVER	7,920 SF	N/A
T1	FLYING HIGH HANGAR	27,600 SF	N/A
T2	FLYING HIGH HANGAR-Under Construction	12,000 SF	N/A
T3	FLYING HIGH HANGAR-Under Construction	8,984 SF	N/A
T4	FLYING HIGH HANGAR-Under Construction	8,984 SF	N/A
U	EXECUTIVE HANGAR	14,800 SF	25 spaces
V	EXECUTIVE HANGAR	10,190 SF	11 spaces
W	AIRFIELD MAINTENANCE FACILITY	18,940 SF	20 spaces
X	AIR TRAFFIC CONTROL TOWER	12,000 SF	17 spaces
Y	VEHICLE FUELING FACILITY	3,960 SF	N/A
Z	NM DEPT. OF PUBLIC SAFETY	12,600 SF	17 spaces
AIRSIDE PARKING			104 spaces
* FBO - FIXED BASED OPERATIONS			

PROJECT DATA

1. **ZONING:** NR-SU

2. **LEGAL DESCRIPTION:** A PORTION OF TR A-1 PLAT OF TRACT A-1 & TRACT L-1 PARCELS 1-5 DOUBLE EAGLE II AIRPORT CONT 4134.8132 AC

3. **ZONE ATLAS:** C4, C5, C6, D4, D5, D6, E4, E5, E6, F4, F5, F6, G4, G6

4. **PARKING:** PER TABLE S-5-1: MINIMUM OFF STREET PARKING REQUIREMENTS OF THE IDO REQUIRED FOR AIRPORTS SHALL BE "DETERMINED BY AIRPORT MANAGEMENT". DEII CURRENTLY CONTAINS 288 OFF-STREET PARKING SPACES

KEY PLAN

SHEET KEYED NOTES

CODE DESCRIPTION

1 PROPERTY BOUNDARY

2 LANDSCAPE BUFFER AREA

3 FIRE HYDRANT

4 ASPHALT

5 ROCK MULCH

6 PONDING/DRAINAGE AREA

7 CURB AND GUTTER

8 CURB CUT

9 6" DIA. BOLLARD

10 CONCRETE SIDEWALK

11 NO PARKING AREA

12 CONCRETE CURB RAMP

13 FIRE LANE STRIPING AND SIGNAGE

14 METAL FENCE

15 ACCESS GATES

16 STOP SIGN

17 ACCESSIBLE PARKING

18 ACCESSIBLE PARKING SIGNAGE

19 MOTORCYCLE PARKING AND SIGNAGE

20 OUTDOOR SEATING AND GATHERING AREA

21 AIRSIDE PARKING

22 UTILITY STRUCTURE

23 CELL TOWER

24 FUEL TANK

25 PERIMETER WALL

26 SYNTHETIC TURF

27 LIGHT FIXTURE

28 COVERED PATIO AND PET RELIEF AREA

29 PROPANE TANK

30 RUNWAY

31 WATER TANK AND UTILITY AREA

32 OBSTACLE FREE ZONE

33 FRENCH DRAIN

34 MAILBOX

35 FREE STANDING SIGNAGE

36 TRASH ENCLOSURE

37 BIKE RACK

38 LOADING/UNLOADING ZONE

39 MAIN GATE

40 MID FIELD GATE

41 PET RELIEF AREA

LEGEND

IMPROVED LANDSCAPE AREA

REINFORCED CONCRETE PAD

UNIMPROVED LANDSCAPE AREA

DIRT ROAD

PROPERTY LINE

TAXIWAY LINES

EXISTING BUILDINGS

FUTURE BUILDINGS

FENCE

CONCRETE WALL SCREENING

ROAD CENTERLINE

BENCH

FIRE HYDRANT

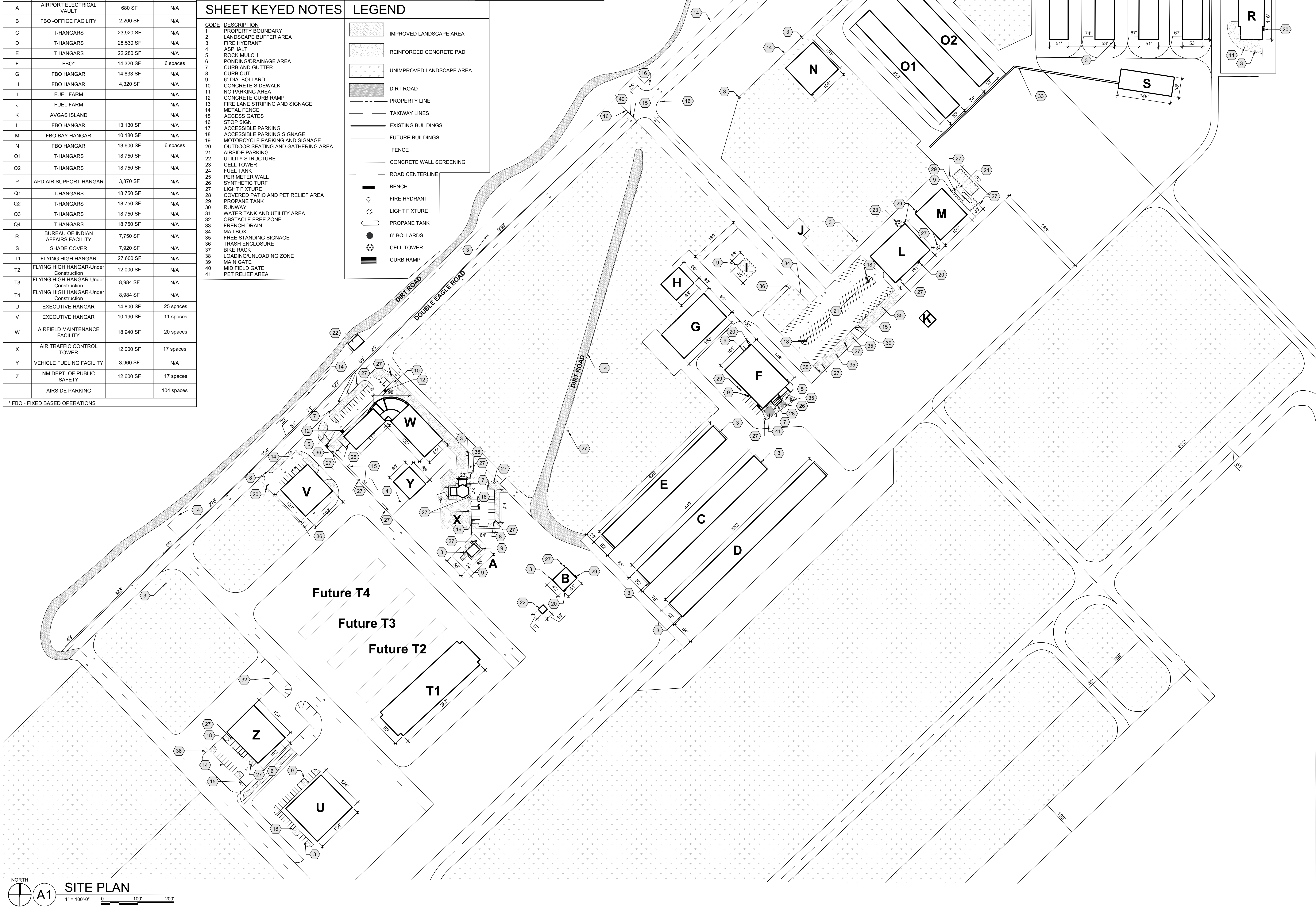
LIGHT FIXTURE

PROPANE TANK

6" BOLLARDS

CELL TOWER

CURB RAMP



NORTH

A1

SITE PLAN

1" = 100'-0"

0 100' 200'

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SEAL

EXISTING CONDITIONS
NOT FOR
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PROJECT

PR# 2018-001577
CASE# SI-2024-00994

DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPC
7401 PASEO DEL VOLCAN NW
ALBUQUERQUE, NM 87121

REVISIONS

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DRAWN BY AM

REVIEWED BY VE

DATE 08/8/2024

PROJECT NO. 21-0082.014

DRAWING NAME

SITE PLAN

SHEET NO.

SDP1.2

A. THIS SITE PLAN DEPICTS ALL KNOWN CONDITIONS AS DETERMINED FROM FIELD OBSERVATIONS AND DIGITAL RECORDS. IN THE EVENT THAT REVISIONS TO THIS SITE PLAN AND RELATED SHEETS IS REQUIRED, THE BURDEN OF DOCUMENTING COMPLIANCE WITH IDO STANDARDS FOR ELEMENTS LIKE PARKING, LANDSCAPING, SIGNAGE, OR A SPECIFIC TYPE OF DEVELOPMENT WILL BE ON THE APPLICANT OF THAT REQUEST. ALL DIMENSIONS ARE APPROXIMATE, AND BASED ON FIELD OBSERVATIONS AND DIGITAL RECORDS.

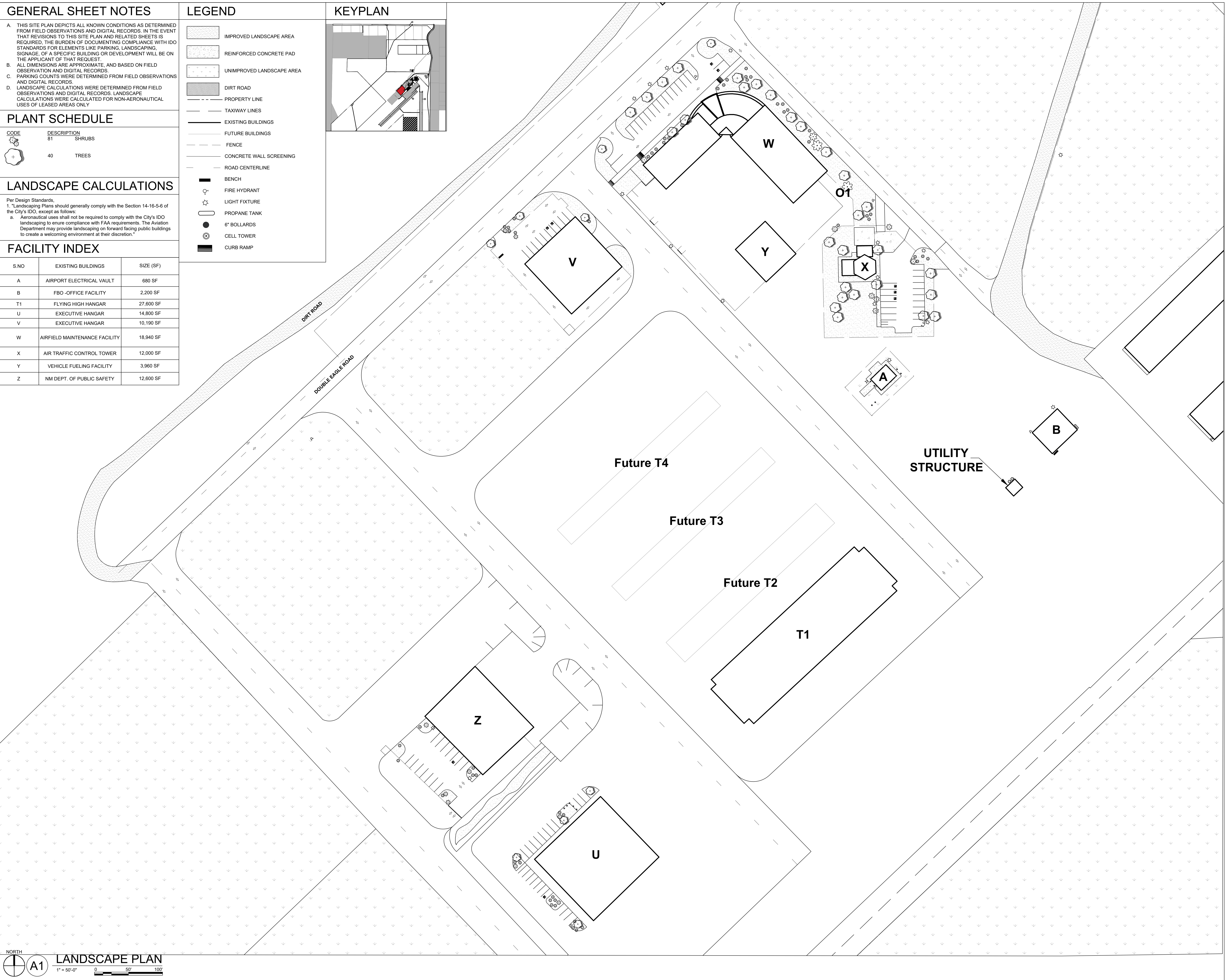
C. PARKING COUNTS WERE DETERMINED FROM FIELD OBSERVATIONS AND DIGITAL RECORDS. LANDSCAPE CALCULATIONS WERE DETERMINED FROM FIELD OBSERVATIONS AND DIGITAL RECORDS. LANDSCAPE CALCULATIONS WERE CALCULATED FOR NON-AERONAUTICAL USES OF LEASED AREAS ONLY.

CODE	DESCRIPTION
81	SHRUBS
40	TREES

Per Design Standards,
1. "Landscape Plans should generally comply with the Section 14-16-5-6 of the City's IDO, except as follows:
a. Aeronautical uses shall not be required to comply with the City's IDO landscaping to ensure compliance with FAA requirements. The Aviation Department may provide landscaping on forward facing public buildings to create a welcoming environment at their discretion."

S.NO	EXISTING BUILDINGS	SIZE (SF)
A	AIRPORT ELECTRICAL VAULT	680 SF
B	FBO -OFFICE FACILITY	2,200 SF
T1	FLYING HIGH HANGAR	27,600 SF
U	EXECUTIVE HANGAR	14,800 SF
V	EXECUTIVE HANGAR	10,190 SF
W	AIRFIELD MAINTENANCE FACILITY	18,940 SF
X	AIR TRAFFIC CONTROL TOWER	12,000 SF
Y	VEHICLE FUELING FACILITY	3,960 SF
Z	NM DEPT. OF PUBLIC SAFETY	12,600 SF

	IMPROVED LANDSCAPE AREA
	REINFORCED CONCRETE PAD
	UNIMPROVED LANDSCAPE AREA
	DIRT ROAD
	PROPERTY LINE
	TAXIWAY LINES
	EXISTING BUILDINGS
	FUTURE BUILDINGS
	FENCE
	CONCRETE AREA SCREENING
	ROAD CENTERLINE
	BENCH
	FIRE HYDRANT
	LIGHT FIXTURE
	PROPANK TANK
	6" BOLLARDS
	CELL TOWER
	CURB RAMP



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L

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2018-001577
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**DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPIC**
7401 PASEO DEL VOLCAN NW
ALBUQUERQUE, NM 87121

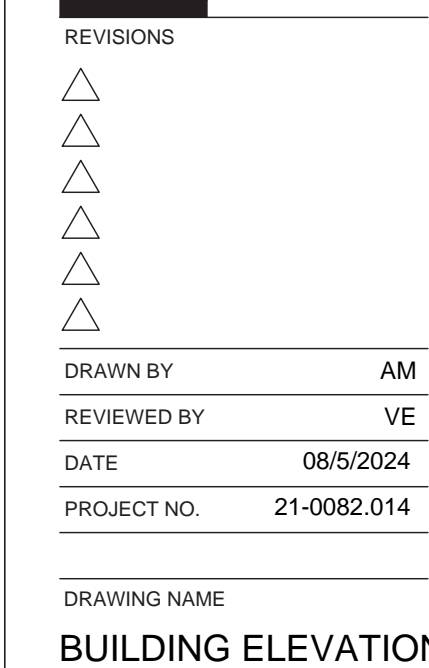
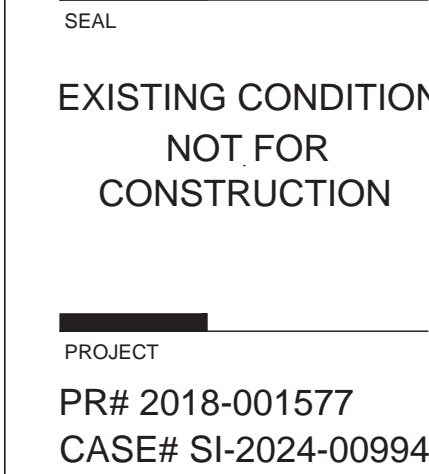
OWN BY	AM
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E	08/8/2024
SUBJECT NO.	21-0082.014

LANDSCAPE PLAN

ET NO.
SDP2.1

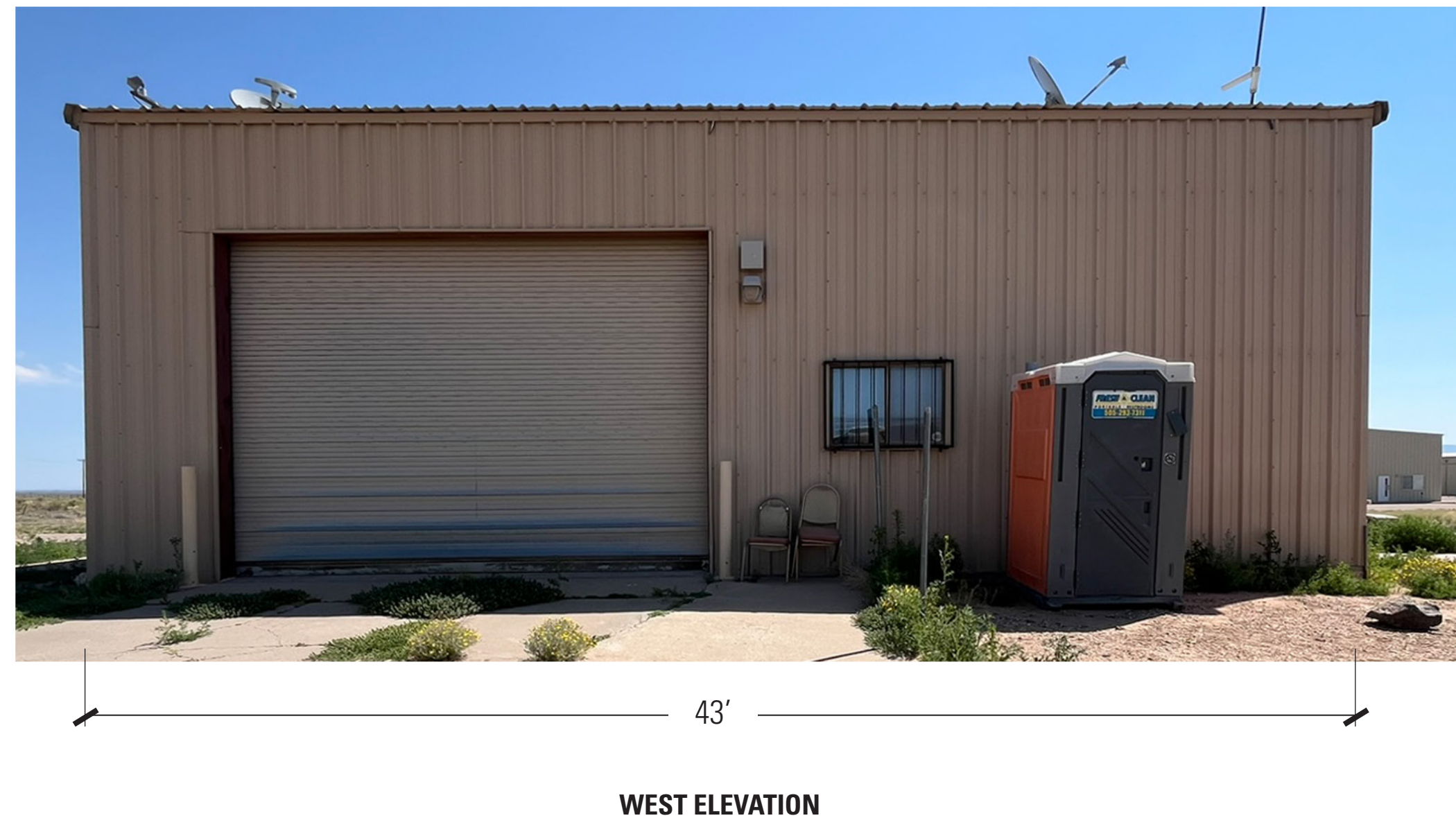
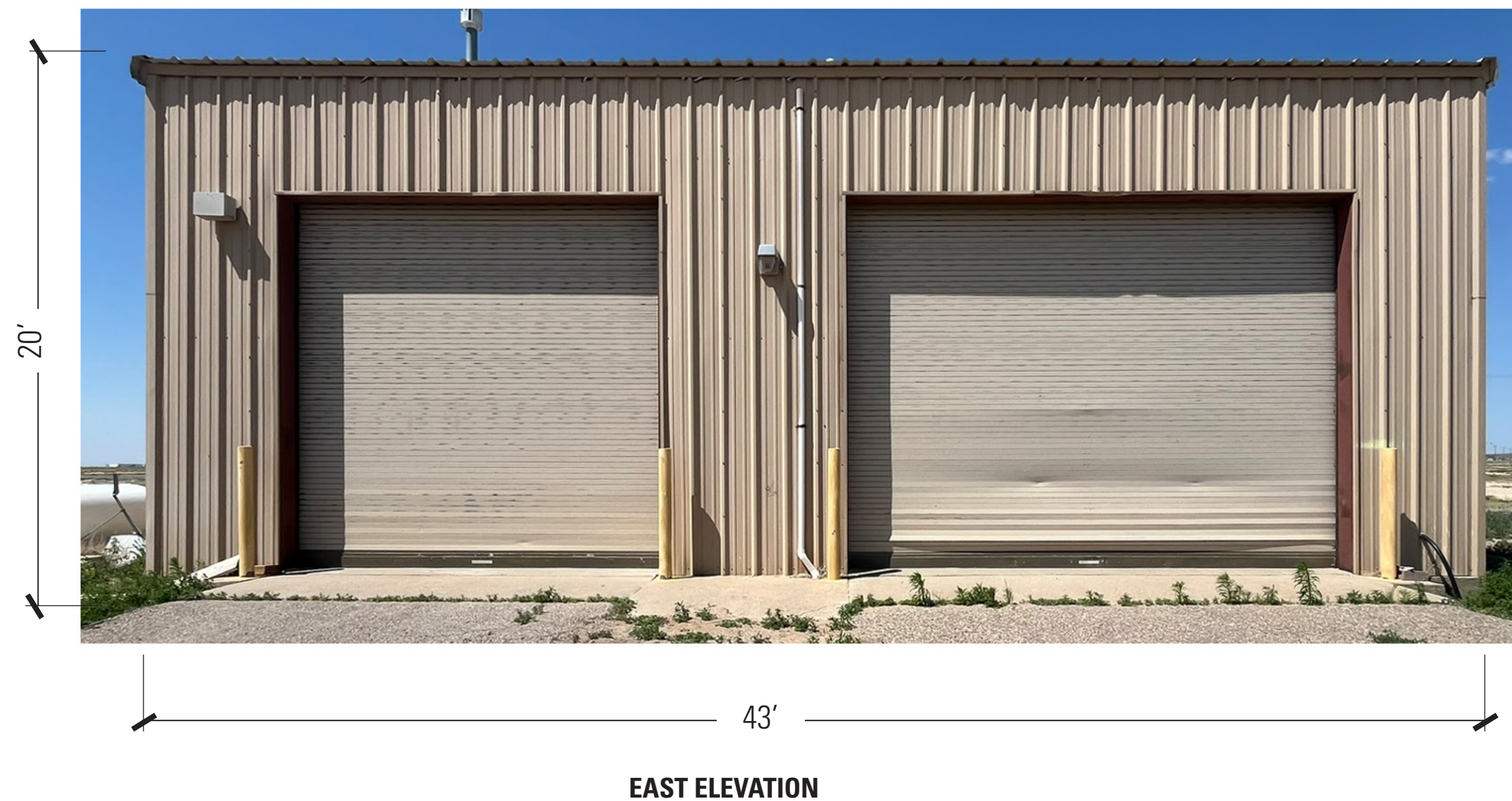
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[illegible]

BUILDING B: FBO OFFICE FACILITY



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**DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPC**
7401 PASEO DEL VOLCAN NW
ALBUQUERQUE, NM 87121

REVISIONS



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DATE	08/5/2024
PROJECT NO.	21-0082.014

DRAWING NAME

BUILDING ELEVATION

SHEET NO.

AE 3.2

[illegible]

BUILDING C: T HANGAR ELEVATION



TYPICAL ELEVATION - NORTH SOUTH



EAST ELEVATION



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21'6"

52'

EAST ELEVATION



A long, single-story industrial building with a dark roof and light-colored corrugated metal siding. The building features several large roll-up doors and smaller windows. A dimension line at the bottom indicates a length of 425 feet.

21'6"

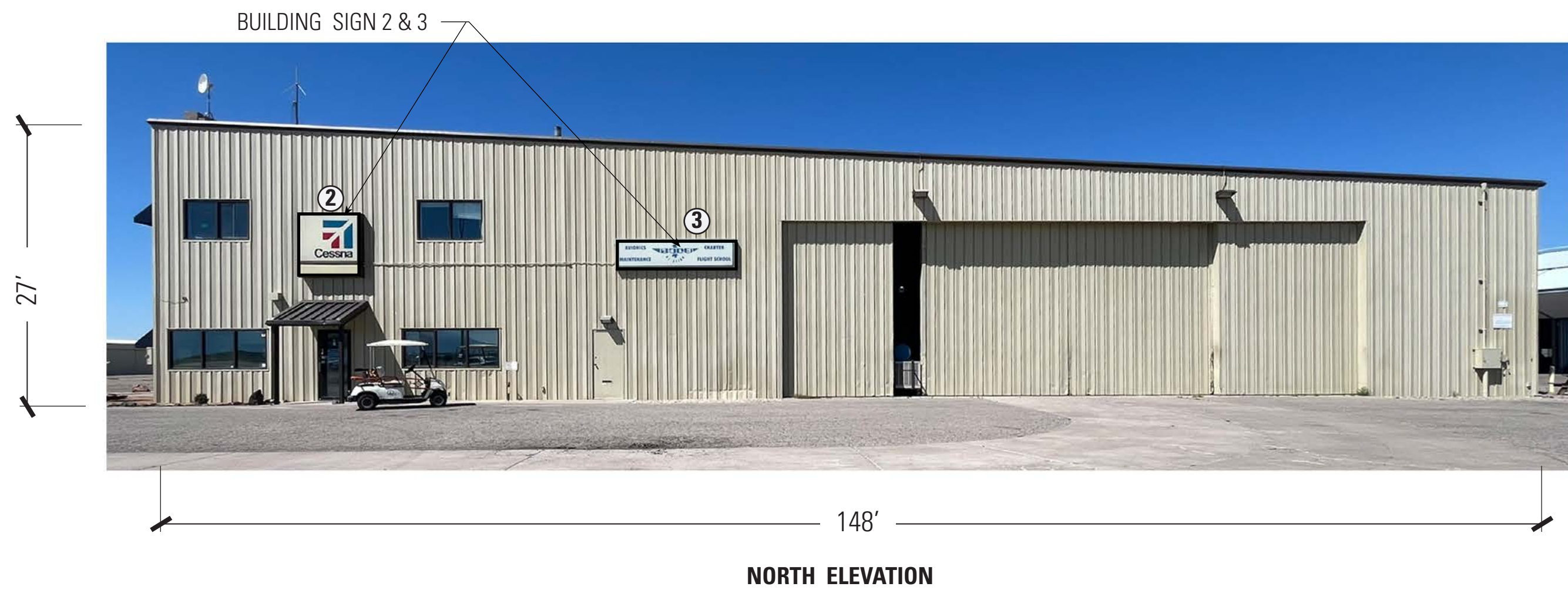
52'

EAST ELEVATION



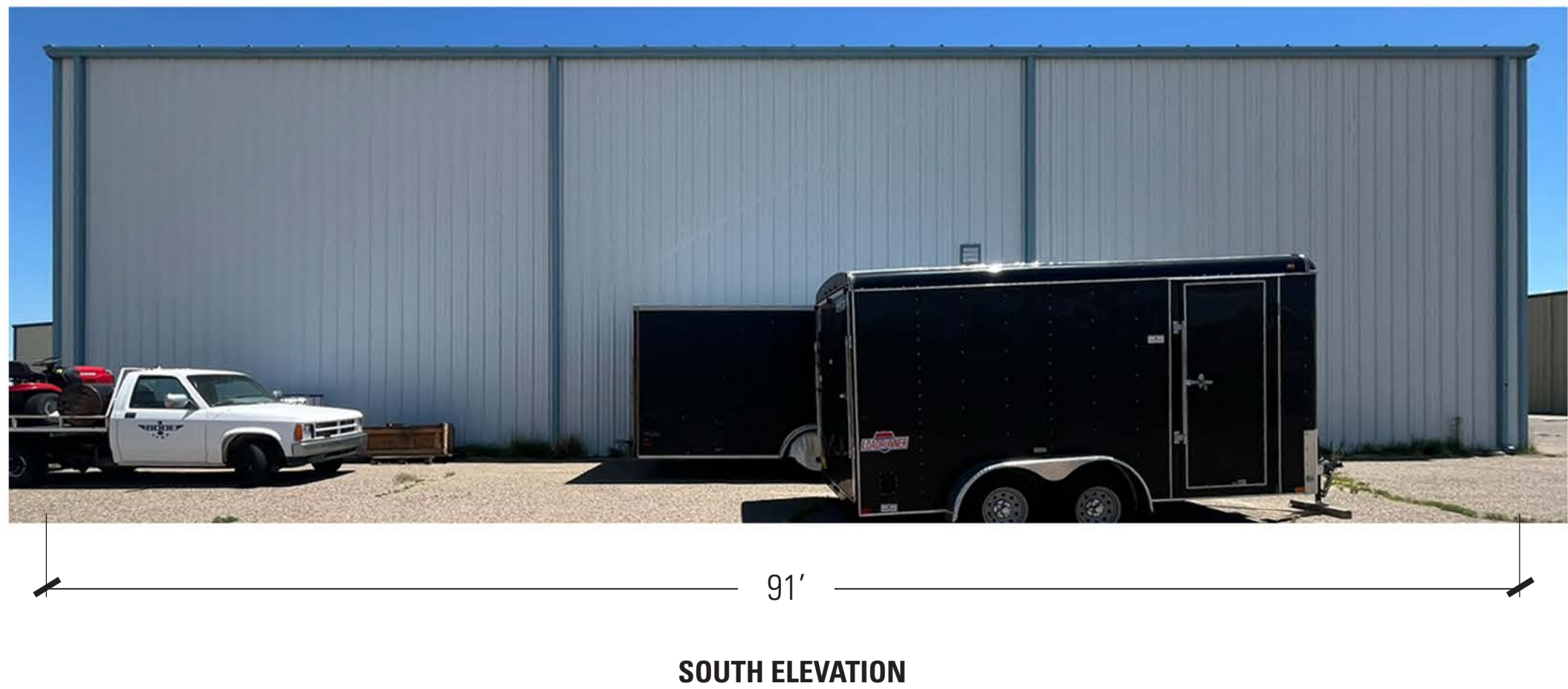
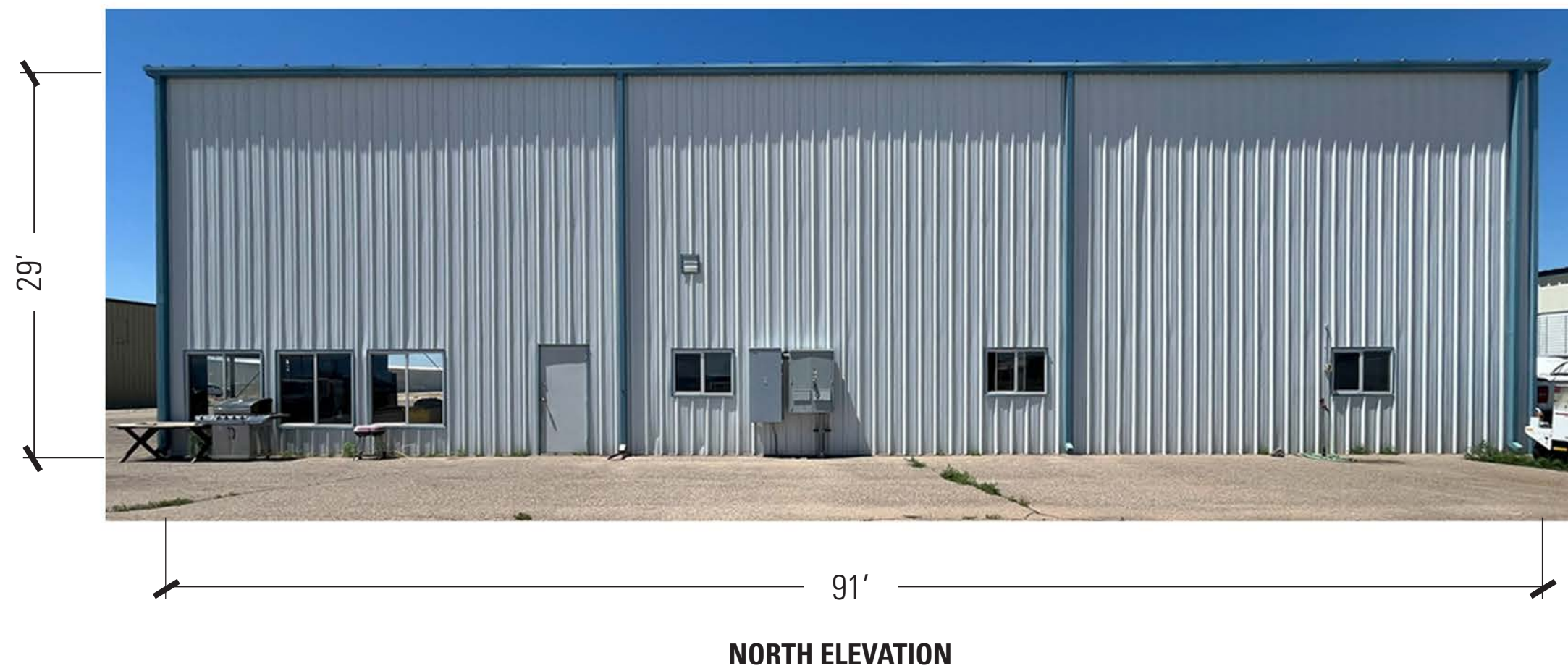
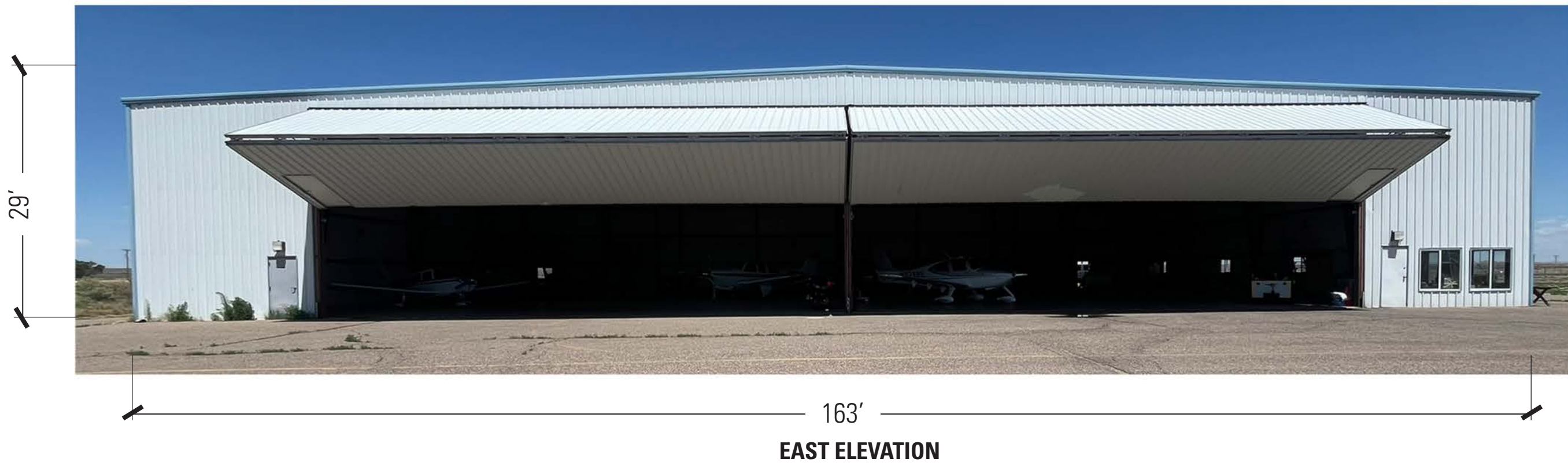
[illegible]

BUILDING F: FBO BUILDING



[illegible]

BUILDING G: FBO HANGAR



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SEAL

EXISTING CONDITIONS
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**DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPC**
7401 PASEO DEL VOLCAN NW
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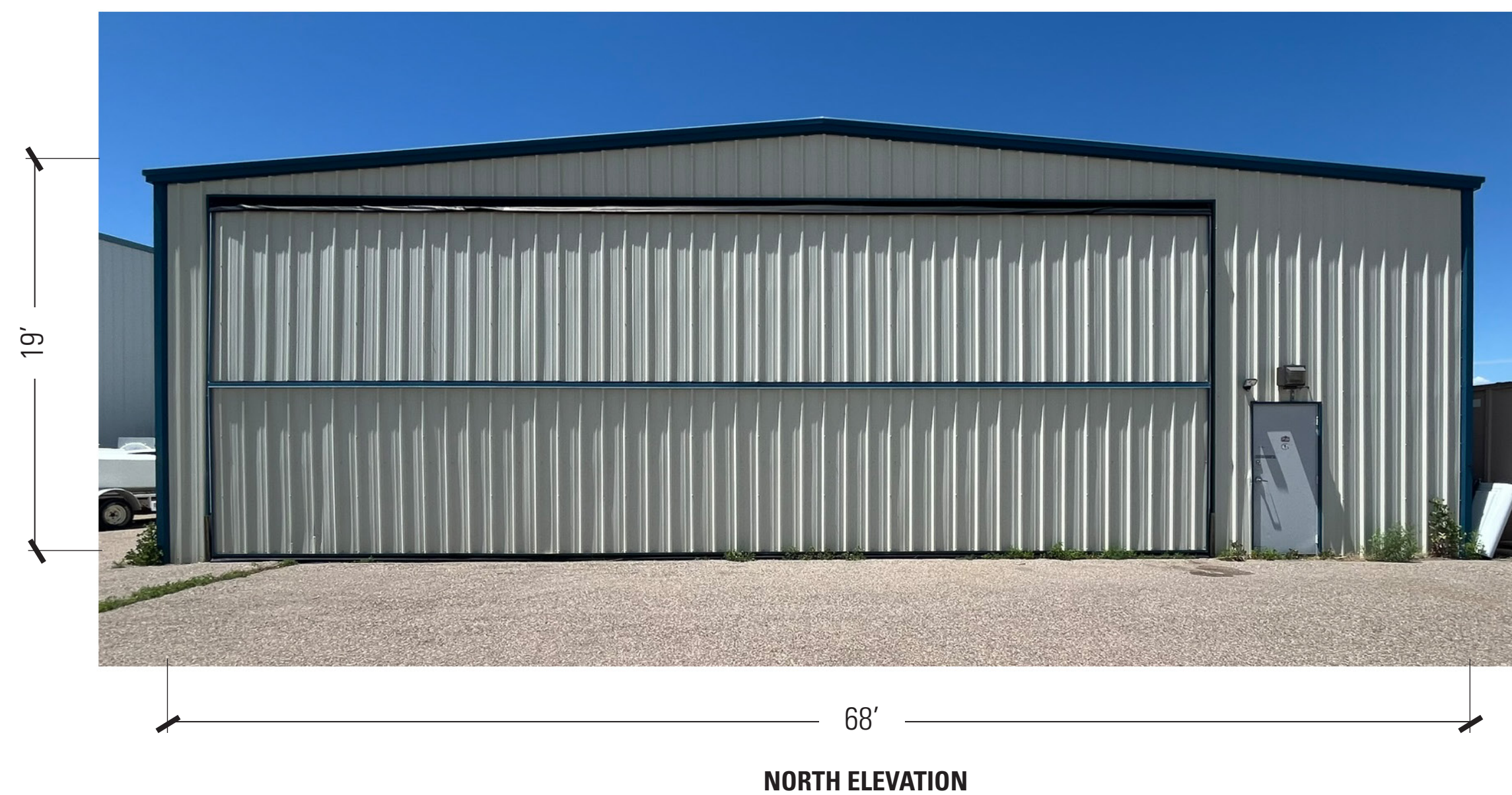
BUILDING ELEVATION

SHEET NO.

AE 3.6

[illegible]

BUILDING H: FBO HANGAR



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DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPC
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BUILDING ELEVATION

SHEET NO. AE3.7

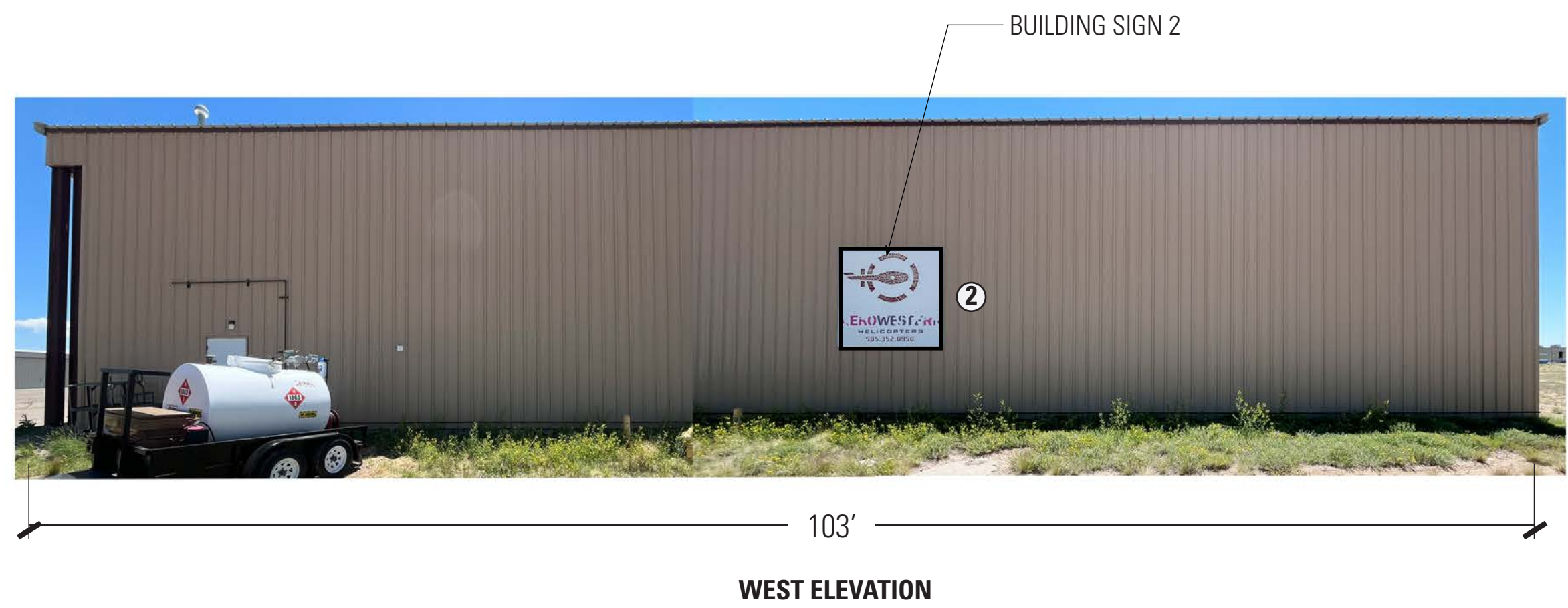
BUILDING M: FBO BAY HANGAR



SHEET NO. AE3.9

[illegible]

BUILDING N: FBO HANGAR



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SEAL

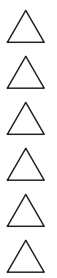
EXISTING CONDITIONS
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**DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPC**
7401 PASEO DEL VOLCAN NW
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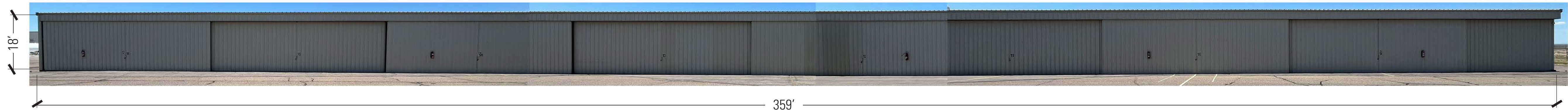
BUILDING ELEVATION

SHEET NO.

AE3.10

[illegible]

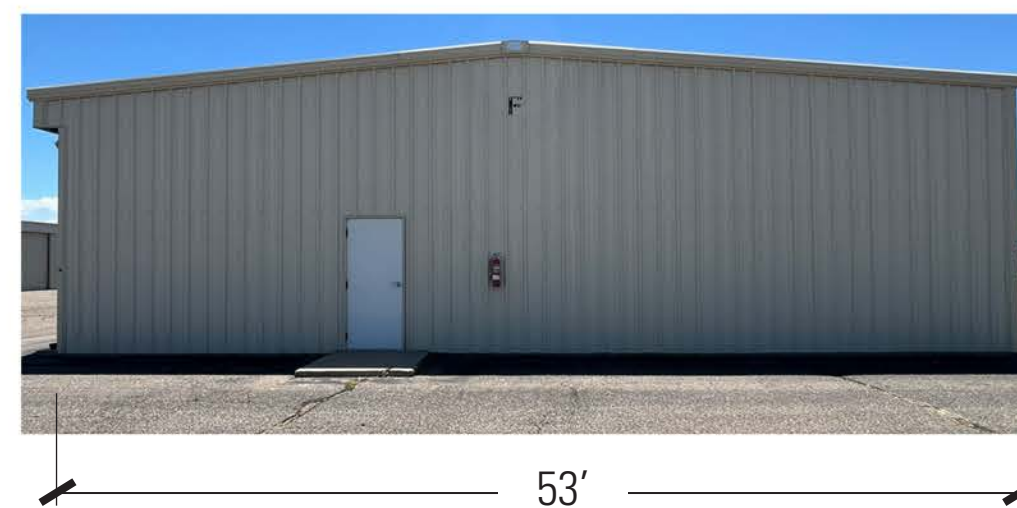
BUILDING 01: T HANGAR ELEVATION



TYPICAL ELEVATION - NORTH SOUTH SOUTH



EAST ELEVATION



WEST ELEVATION

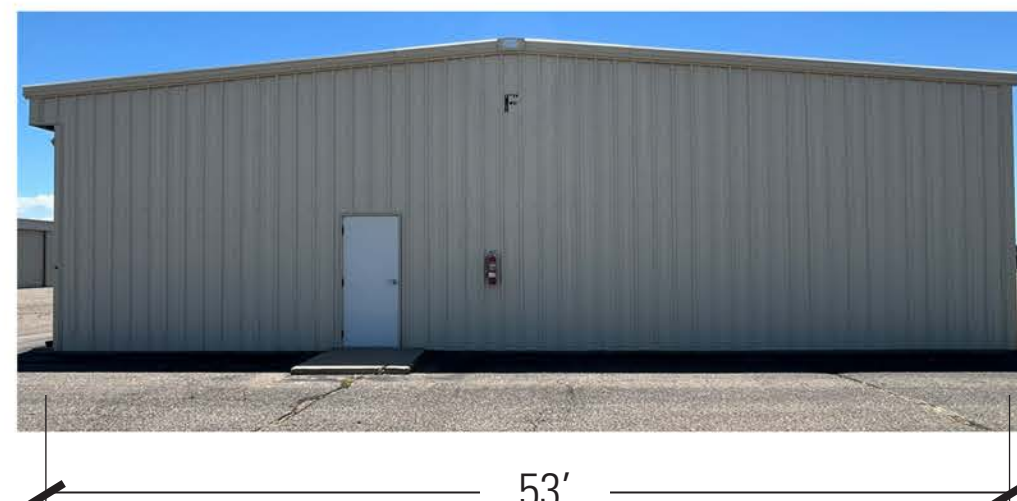
BUILDING 02: T HANGAR ELEVATION



TYPICAL ELEVATION - NORTH SOUTH



EAST ELEVATION



WEST ELEVATION

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**DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPC**
7401 PASEO DEL VOLCAN NW
ALBUQUERQUE, NM 87121

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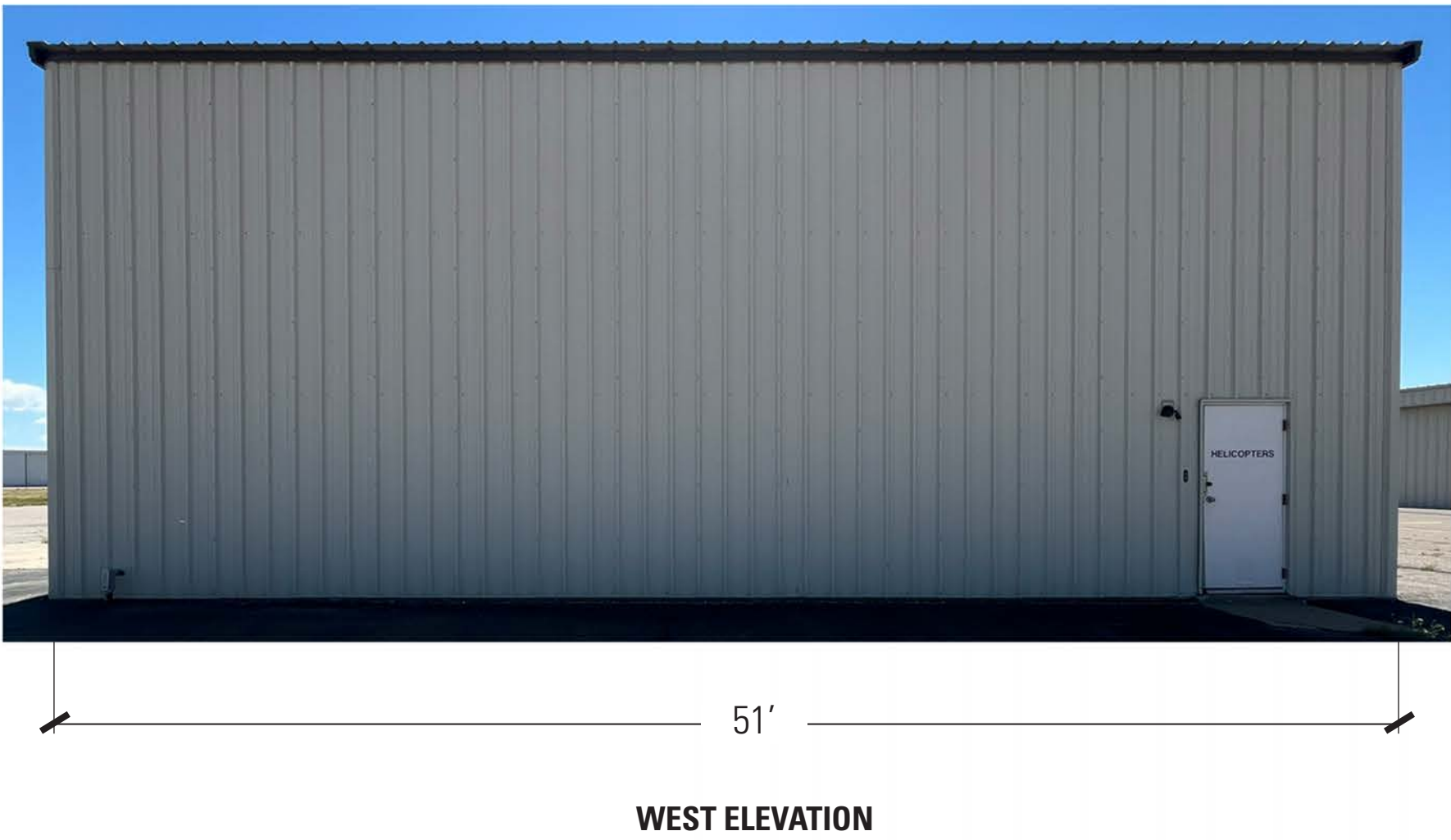
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BUILDING ELEVATION

HEET NO.

AE3.11

BUILDING P: APD AIR SUPPORT HANGAR



SEAL

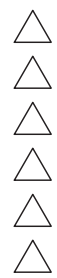
EXISTING CONDITIONS
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DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPC
7401 PASEO DEL VOLCAN NW
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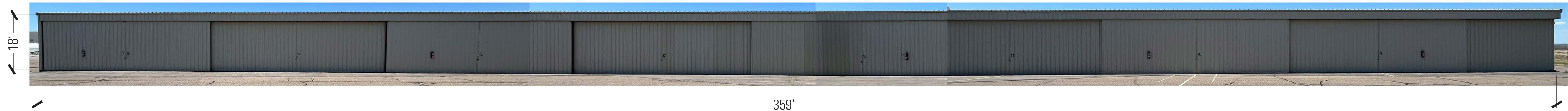
BUILDING ELEVATION

SHEET NO.

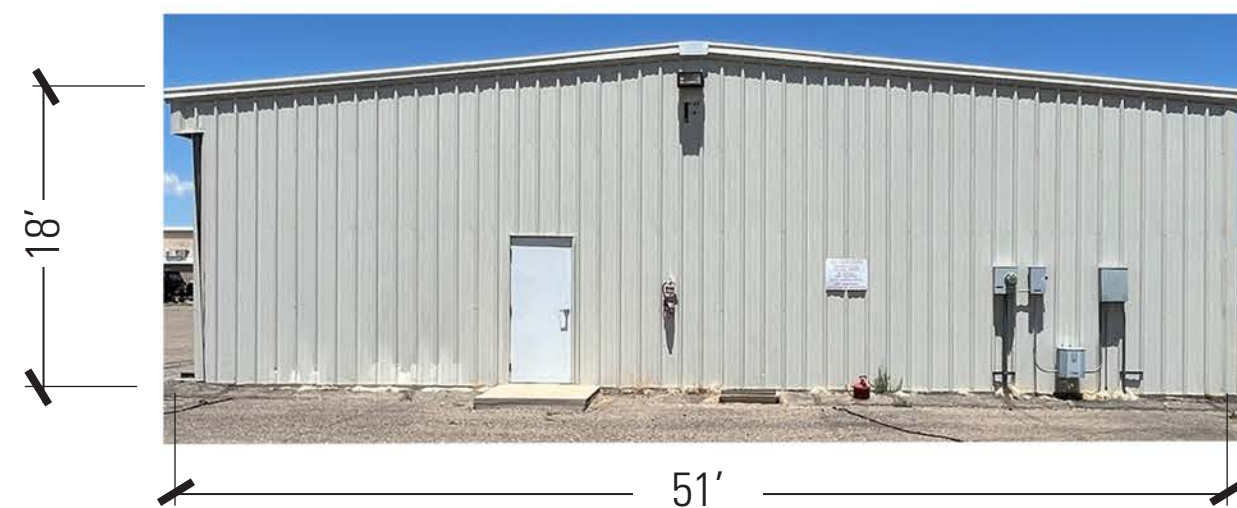
AE3.12

[illegible]

BUILDING Q1: T HANGAR ELEVATION



TYPICAL ELEVATION - NORTH SOUTH

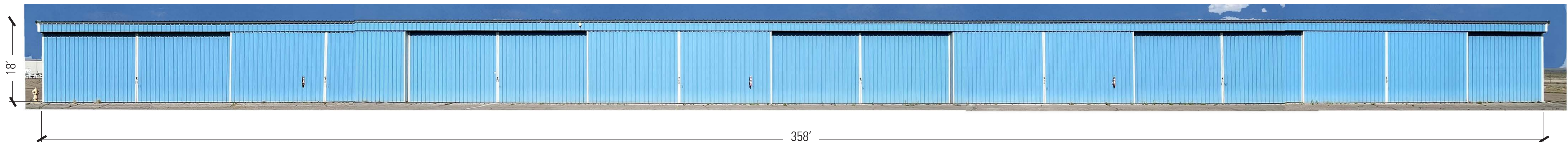


EAST ELEVATION



WEST ELEVATION

BUILDING Q2: T HANGAR ELEVATION



TYPICAL ELEVATION - EAST AND WEST



NORTH ELEVATION



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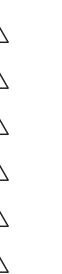
Architecture III Progress

DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPC
7401 PASEO DEL VOLCAN NW
ALBUQUERQUE, NM 87121

401 PASEO DEL VOLCAN N

ALBUQUERQUE, NM 87121

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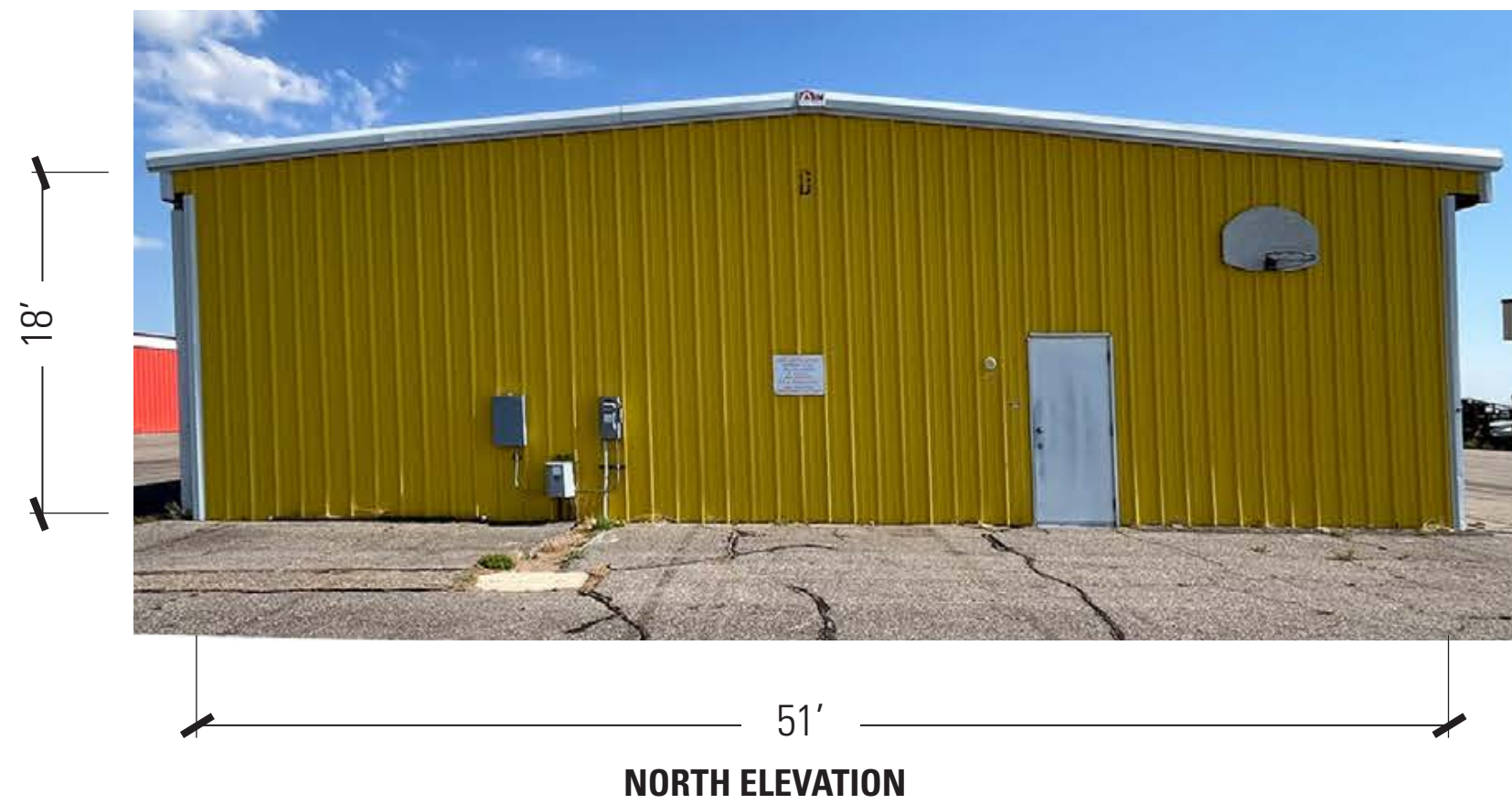
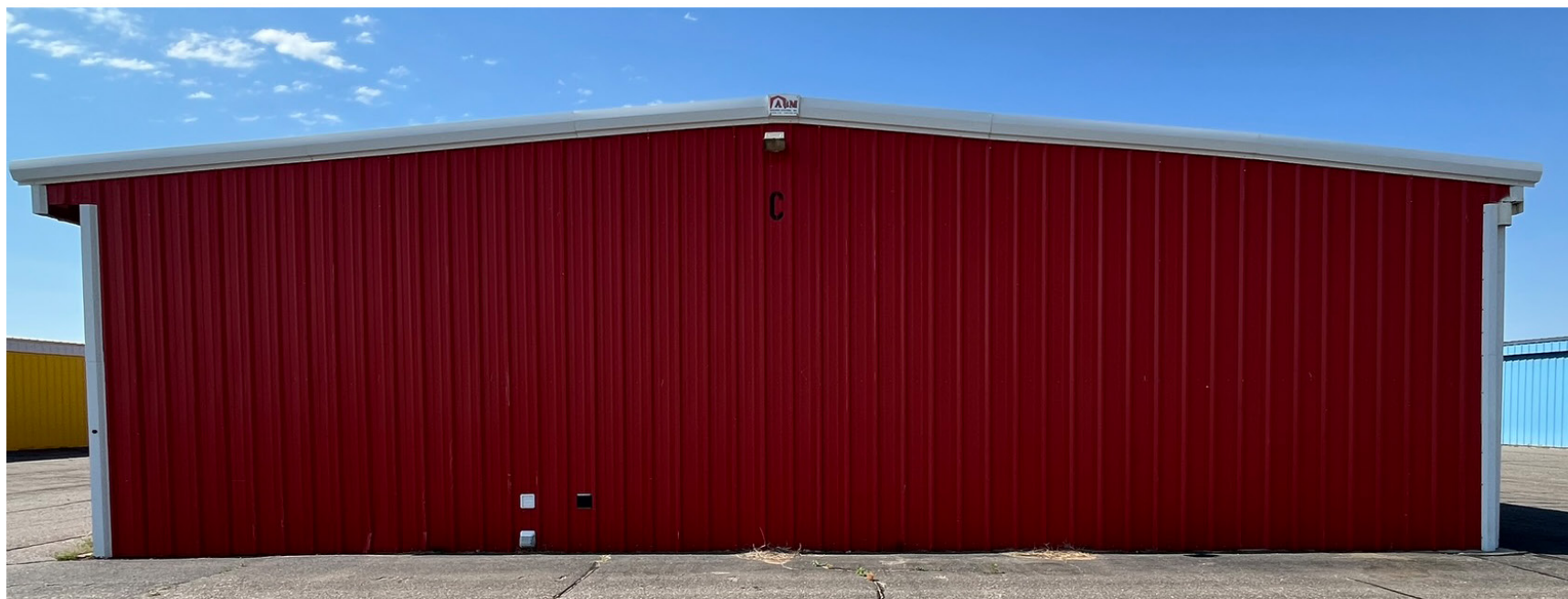
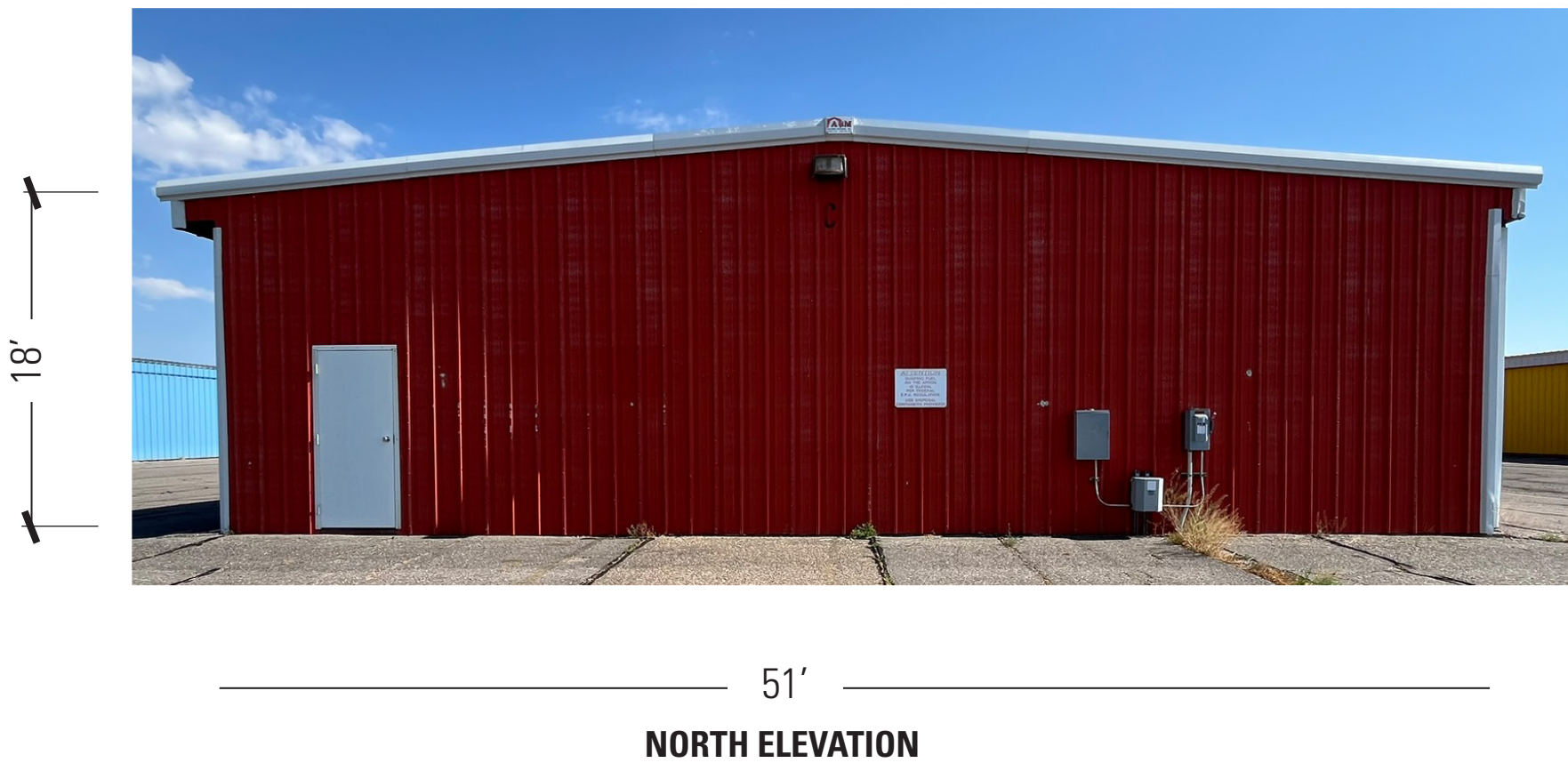
BUILDING ELEVATION

EET NO.

AE3.13

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PROJECT

PR# 2018-001577
CASE# SI-2024-00994

**DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPC**
7401 PASEO DEL VOLCAN NW
ALBUQUERQUE, NM 87121

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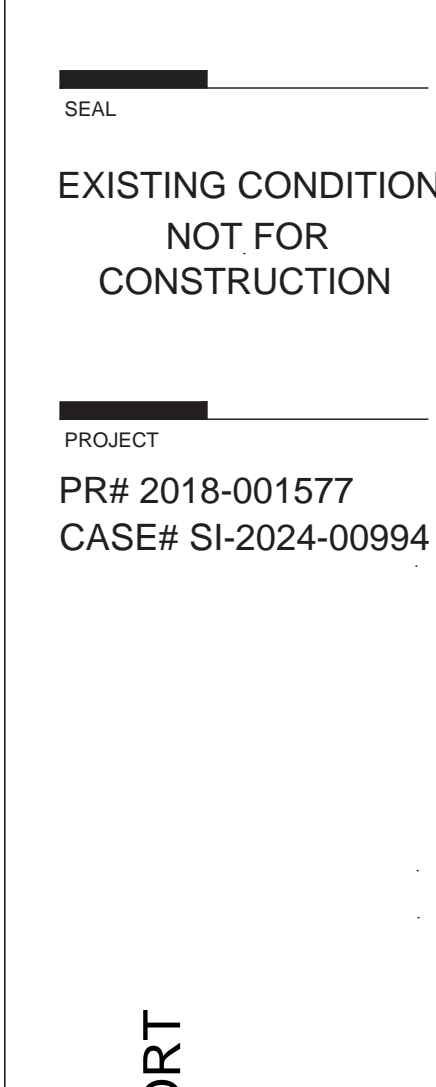
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BUILDING ELEVATION

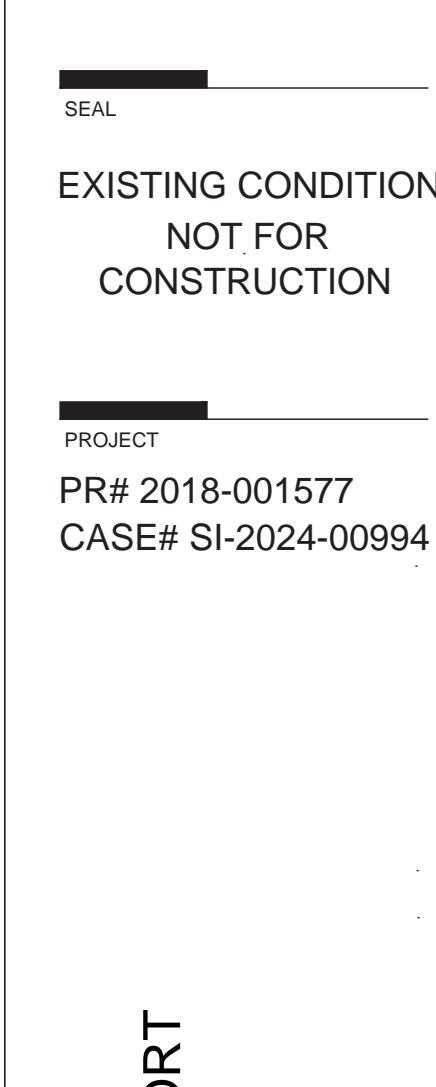
SHEET NO. AE3.14

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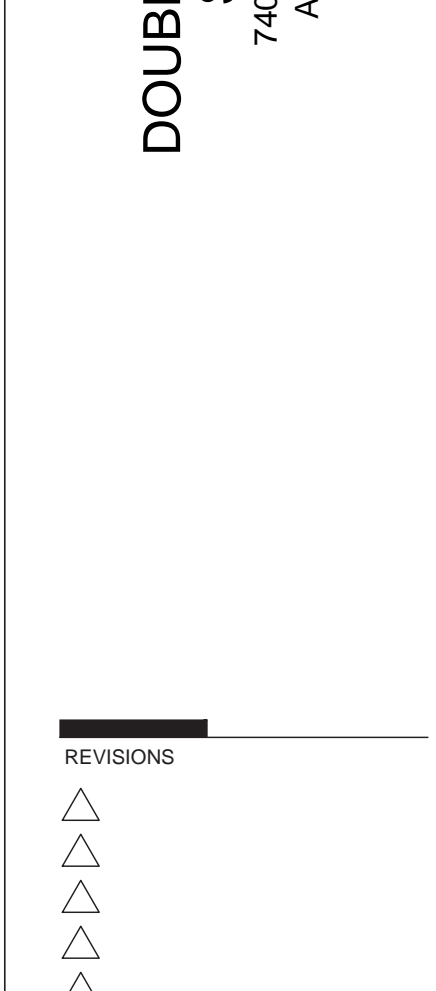
Architecture in Progress



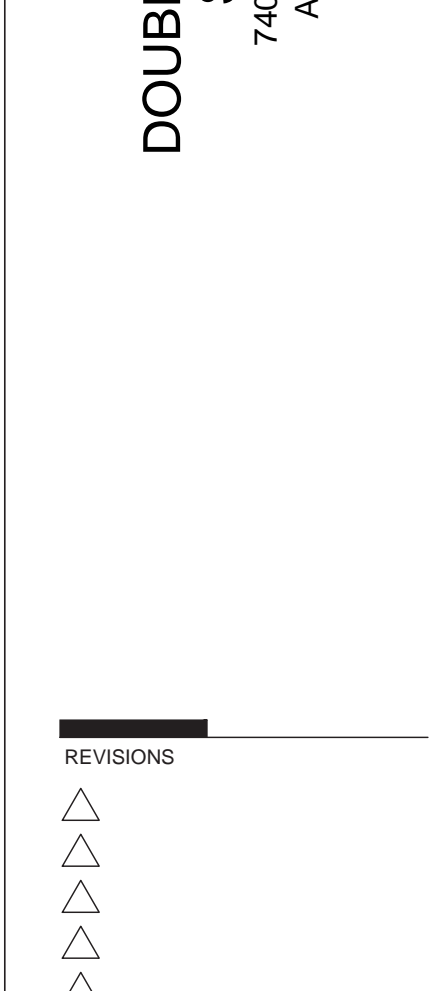
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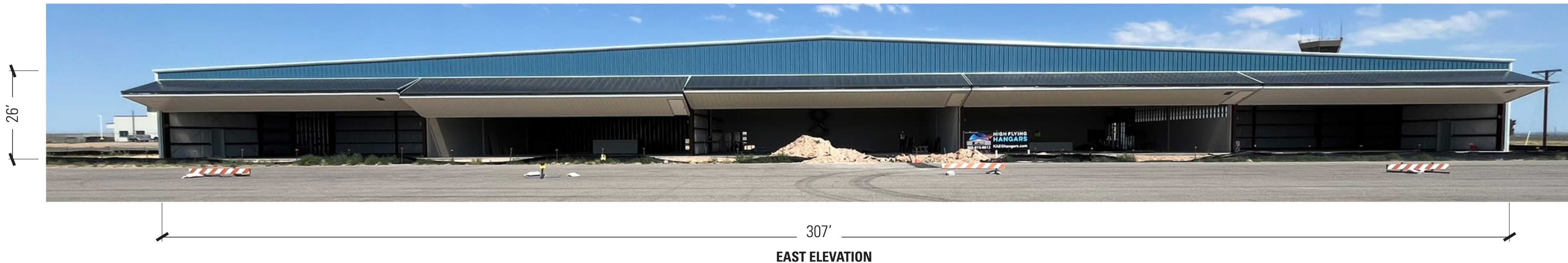
DRAWING NAME

BUILDING ELEVATION

AE3.15

[illegible]

BUILDING T1: FLYING HIGH HANGAR



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DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPC
7401 PASEO DEL VOLCAN NW
ALBUQUERQUE, NM 87121

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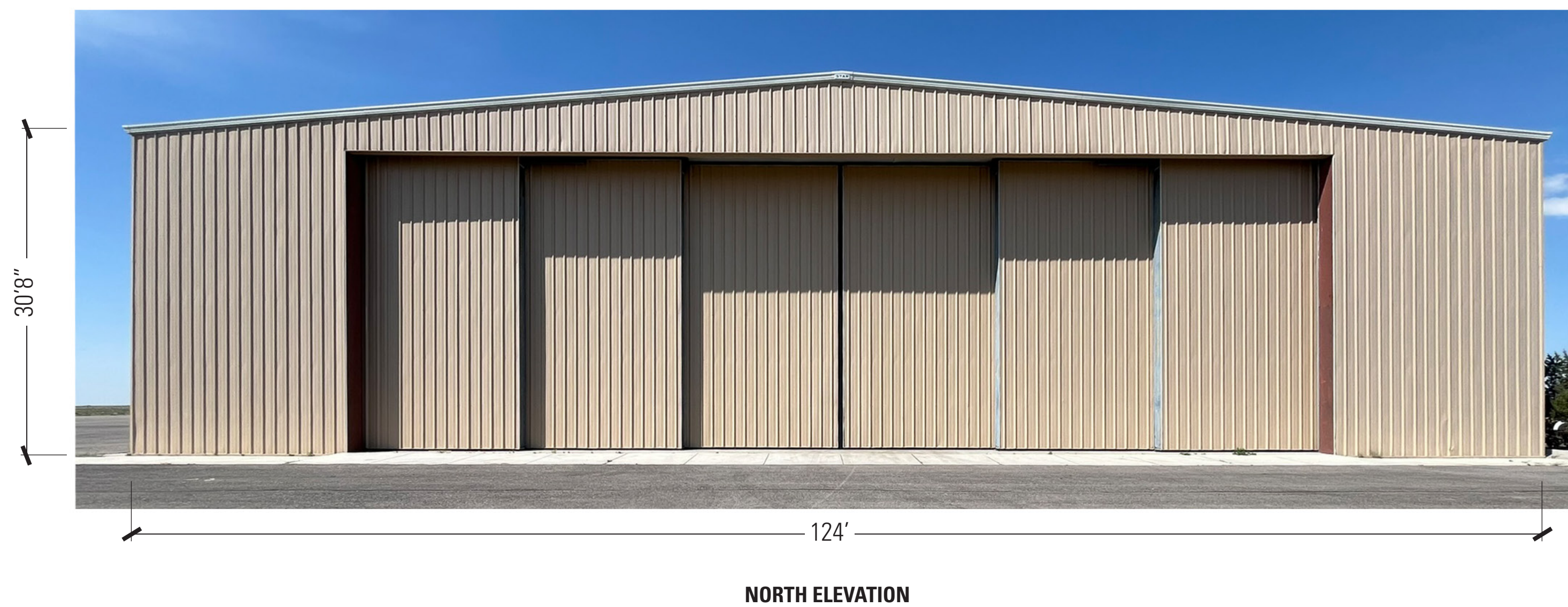
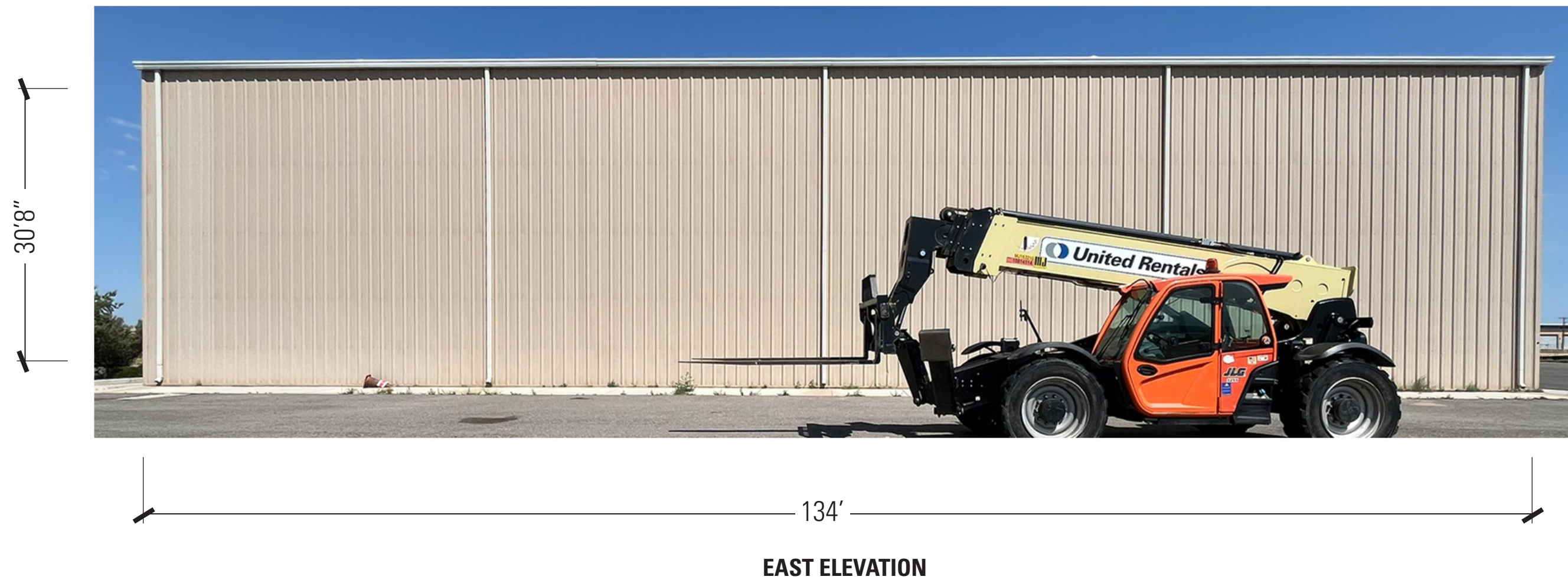
BUILDING ELEVATION

SHEET NO.

AE3.16

[illegible]

BUILDING U: EXECUTIVE HANGAR



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DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPC
7401 PASEO DEL VOLCAN NW
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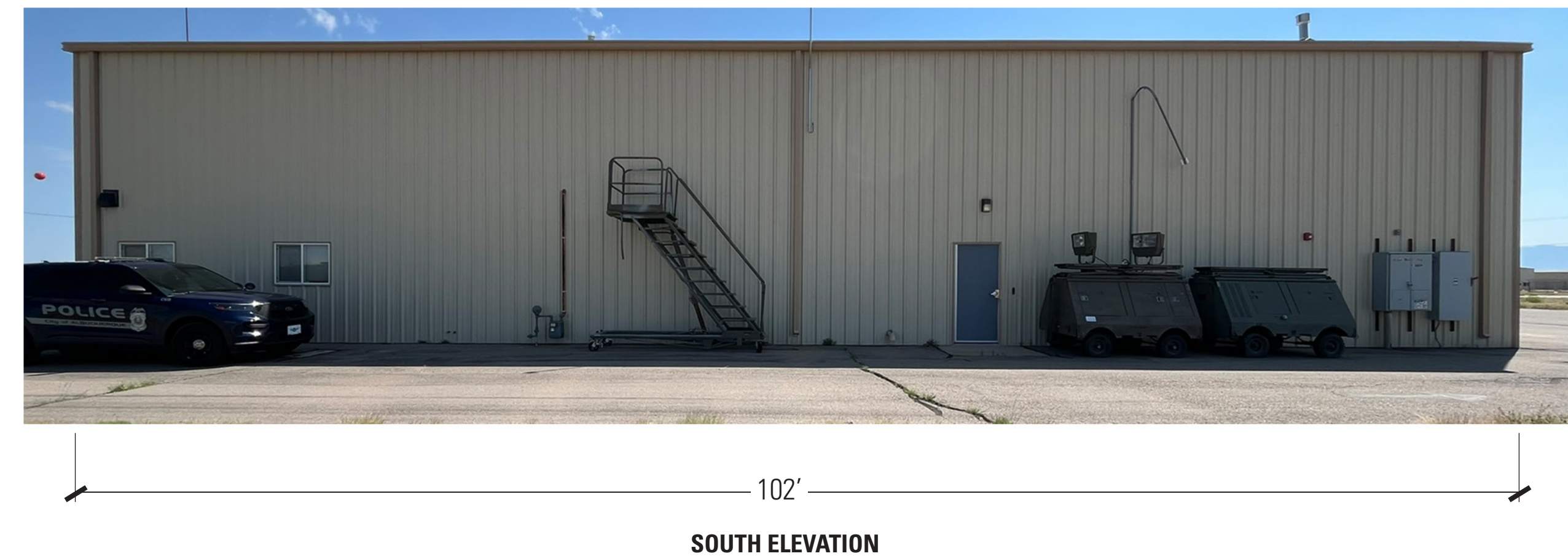
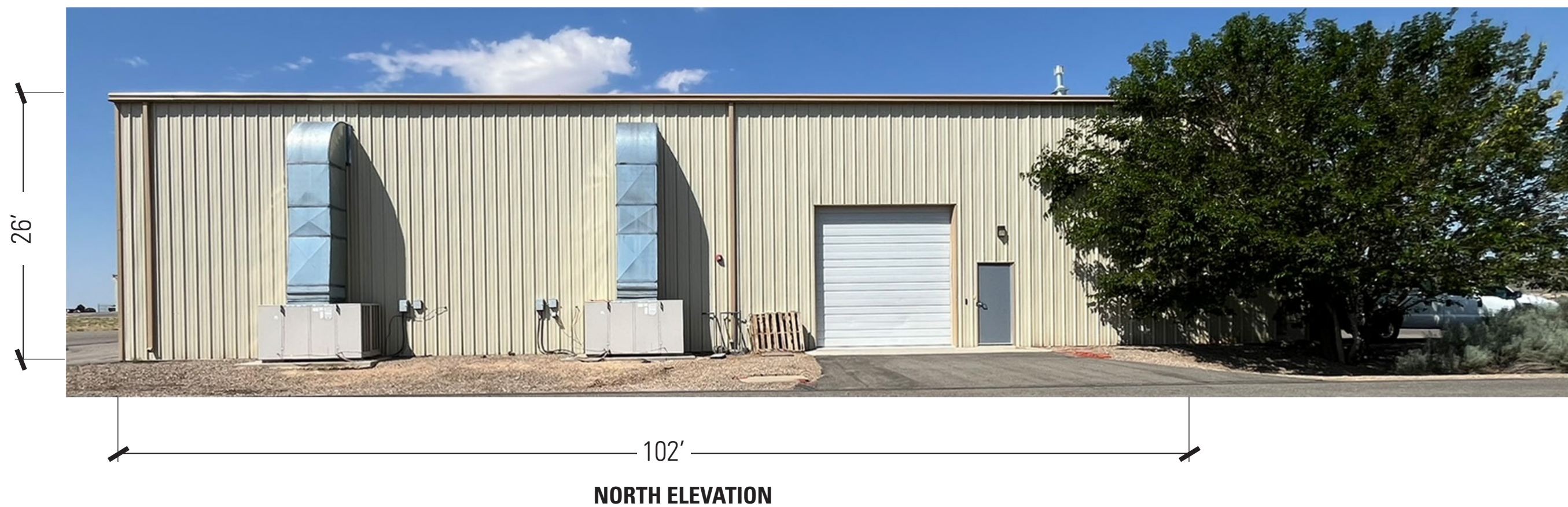
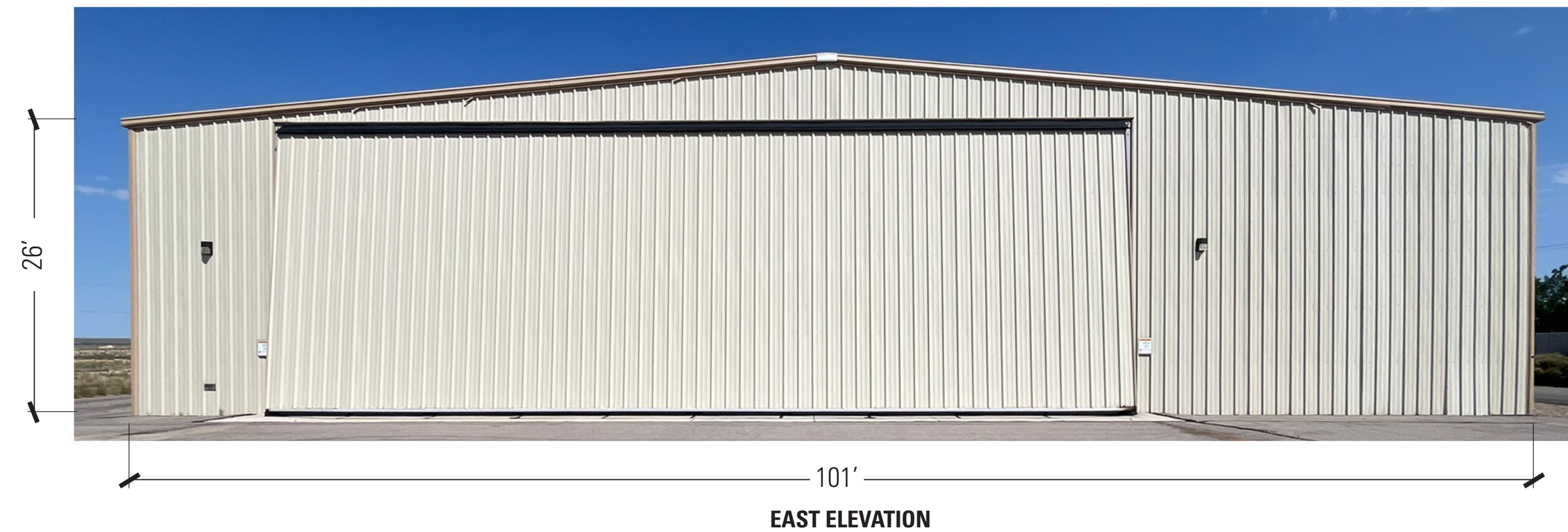
DRAWING NAME
BUILDING ELEVATION

SHEET NO.

AE3.17

[illegible]

BUILDING V: EXECUTIVE HANGAR



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SEAL

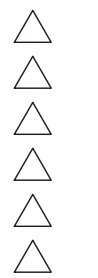
EXISTING CONDITIONS
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PROJECT

PR# 2018-001577
CASE# SI-2024-00994

**DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPC**
7401 PASEO DEL VOLCAN NW
ALBUQUERQUE, NM 87121

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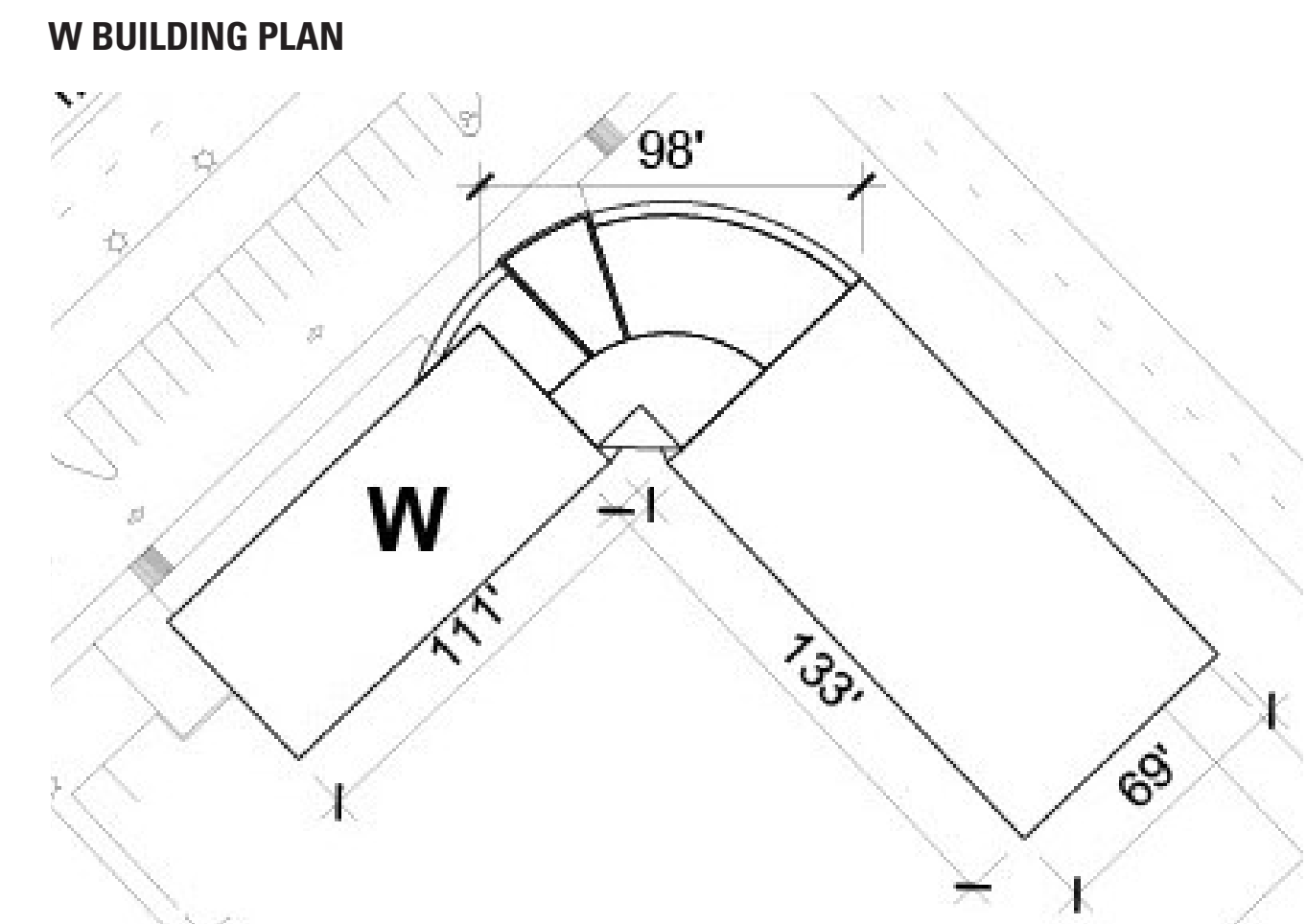
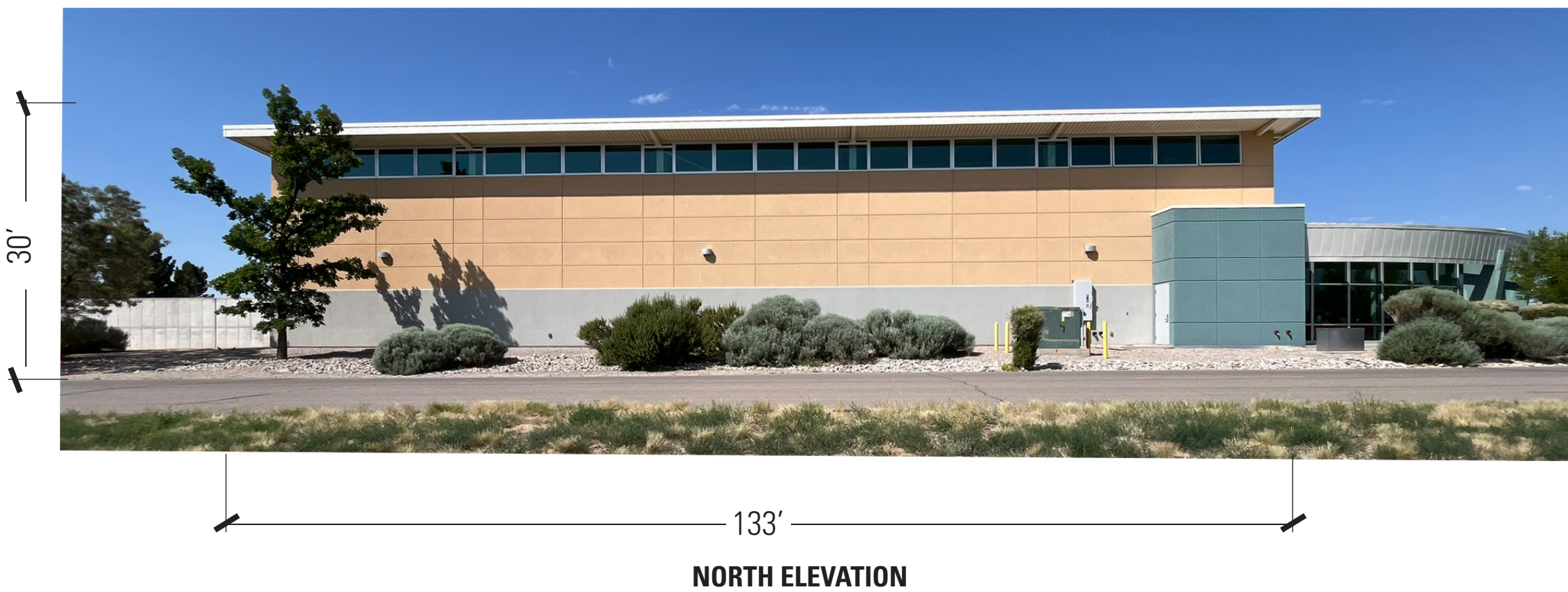
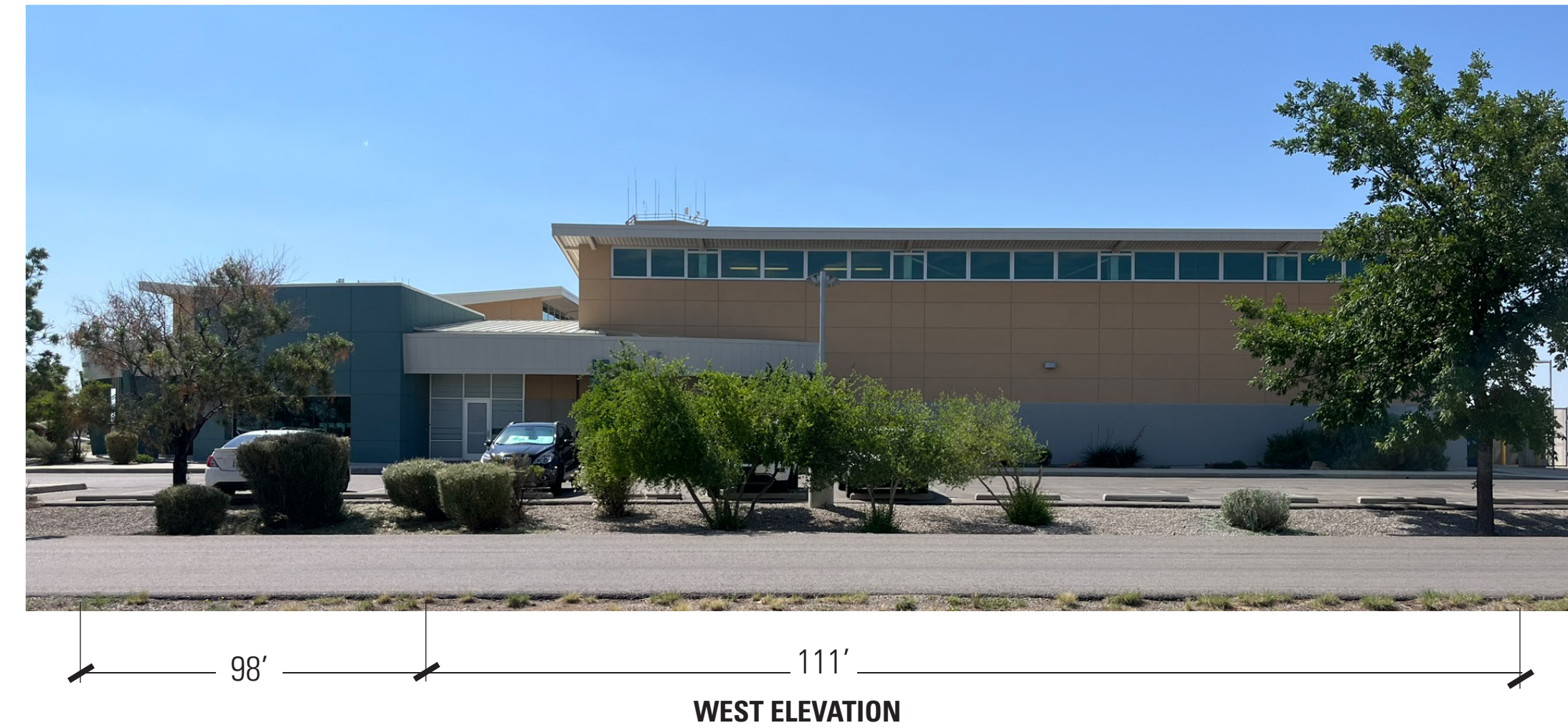
BUILDING ELEVATION

SHEET NO.

AE3.18

[illegible]

BUILDING W: AIRFIELD MAINTENANCE FACILITY



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SEAL

EXISTING CONDITIONS
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PROJECT

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**DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPIC**
7401 PASEO DEL VOLCAN NW
ALBUQUERQUE, NM 87121

REVISIONS



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DRAWING NAME

BUILDING ELEVATION

SHEET NO.

AE3.19

[illegible]

BUILDING X: AIR TRAFFIC CONTROL TOWER



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DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPC
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ALBUQUERQUE, NM 87121

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DRAWING NAME

BUILDING ELEVATION

SHEET NO.

AE3.20

BUILDING Z: NM DEPT. OF PUBLIC SAFETY



EXISTING CONDITIONS
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PROJECT
PR# 2018-001577
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**DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPC**
7401 PASEO DEL VOLCAN NW
ALBUQUERQUE, NM 87121

REVISIONS	
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DRAWN BY	AM
REVIEWED BY	VE
DATE	08/5/2024
PROJECT NO.	21-0082.014

DRAWING NAME

BUILDING ELEVATION

SHEET NO.

AE3.21

[illegible]

SOUTHWEST AERONAUTICS, MATHEMATICS AND SCIENCE ACADEMY



Dekker
Architecture in Progress

**DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPC**
7401 PASO DEL VOLCAN NW
ALBUQUERQUE, NM 87121

SIONS

OWN BY	AM
LEASED BY	VE
08/5/2024	
PROJECT NO.	21-0082.014

BUILDING ELEVATION

T NO.

AE3.22

SITE AMENITIES



SEAL

EXISTING CONDITIONS
NOT FOR
CONSTRUCTION

PROJECT
PR# 2018-001577
CASE# SI-2024-00994

**DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPC**
7401 PASEO DEL VOLCAN NW
ALBUQUERQUE, NM 87121

REVISIONS	
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DRAWN BY	AM
REVIEWED BY	VE
DATE	08/5/2024
PROJECT NO.	21-0082.014

DRAWING NAME

BUILDING ELEVATION

SHEET NO. AE3.23

REFUSE ENCLOSURES



Dekker
Architecture in Progress

SEAL

EXISTING CONDITIONS
NOT FOR
CONSTRUCTION

PROJECT
PR# 2018-001577
CASE# SI-2024-00994

**DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPC**
7401 PASEO DEL VOLCAN NW
ALBUQUERQUE, NM 87121

REVISIONS	
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DRAWN BY AM	
REVIEWED BY VE	
DATE	08/5/2024
PROJECT NO.	21-0082.014

DRAWING NAME
REFUSE ENCLOSURE
DETAILS

SHEET NO. AE3.24

FREESTANDING SIGNS



GENERAL SHEET NOTES

- A. THIS SITE PLAN DEPICTS ALL KNOWN CONDITIONS AS DETERMINED FROM FIELD OBSERVATIONS AND DIGITAL RECORDS. IN THE EVENT THAT REVISIONS TO THIS SITE PLAN AND RELATED SHEETS IS REQUIRED, THE BURDEN OF DOCUMENTING COMPLIANCE WITH IDO STANDARDS FOR ELEMENTS LIKE
- B. ~~ALL DIMENSIONS ARE APPROXIMATE AND BASED ON FIELD OBSERVATION AND DIGITAL RECORDS.~~
- C. PARKING LOCATIONS WERE DETERMINED FROM FIELD OBSERVATIONS AND DIGITAL RECORDS.
- D. ALL BUILDING ELEVATIONS SHEETS DEPICTS KNOWN CONDITIONS FOR ELEMENTS SUCH AS BUILDINGS, SITE LIGHTING, REFUSE, FENCING ETC. ARE DETERMINED FROM FIELD OBSERVATIONS. IN THE EVENT THAT REVISIONS TO THE BUILDING ELEVATION SHEETS AND RELATED SHEETS IS REQUIRED, THE BURDEN OF DOCUMENTING COMPLIANCE WITH IDO STANDARDS WILL BE ON THE APPLICANT OF THAT REQUEST.
- E. EXISTING SIGNAGE WAS DOCUMENTED FROM FIELD OBSERVATIONS. IN THE EVENT SIGNAGE REVISIONS ARE REQUIRED, THE BURDEN OF DOCUMENTING COMPLIANCE WITH IDO STANDARDS FOR ELEMENTS LIKE TOTAL SIGNING AREA OF A SPECIFIC BUILDING OR DEVELOPMENT WILL BE ON THE APPLICANT OF THAT REQUEST.

[illegible]

KEYPLAN

The key plan map shows the study area (indicated by a red rectangle) located within a larger site. The map includes a grid system and various labeled areas, including 'STATION 1', 'STATION 2', 'STATION 3', 'STATION 4', 'STATION 5', 'STATION 6', 'STATION 7', 'STATION 8', 'STATION 9', 'STATION 10', 'STATION 11', 'STATION 12', 'STATION 13', 'STATION 14', 'STATION 15', 'STATION 16', 'STATION 17', 'STATION 18', 'STATION 19', 'STATION 20', 'STATION 21', 'STATION 22', 'STATION 23', 'STATION 24', 'STATION 25', 'STATION 26', 'STATION 27', 'STATION 28', 'STATION 29', 'STATION 30', 'STATION 31', 'STATION 32', 'STATION 33', 'STATION 34', 'STATION 35', 'STATION 36', 'STATION 37', 'STATION 38', 'STATION 39', 'STATION 40', 'STATION 41', 'STATION 42', 'STATION 43', 'STATION 44', 'STATION 45', 'STATION 46', 'STATION 47', 'STATION 48', 'STATION 49', 'STATION 50', 'STATION 51', 'STATION 52', 'STATION 53', 'STATION 54', 'STATION 55', 'STATION 56', 'STATION 57', 'STATION 58', 'STATION 59', 'STATION 60', 'STATION 61', 'STATION 62', 'STATION 63', 'STATION 64', 'STATION 65', 'STATION 66', 'STATION 67', 'STATION 68', 'STATION 69', 'STATION 70', 'STATION 71', 'STATION 72', 'STATION 73', 'STATION 74', 'STATION 75', 'STATION 76', 'STATION 77', 'STATION 78', 'STATION 79', 'STATION 80', 'STATION 81', 'STATION 82', 'STATION 83', 'STATION 84', 'STATION 85', 'STATION 86', 'STATION 87', 'STATION 88', 'STATION 89', 'STATION 90', 'STATION 91', 'STATION 92', 'STATION 93', 'STATION 94', 'STATION 95', 'STATION 96', 'STATION 97', 'STATION 98', 'STATION 99', 'STATION 100'. The map also shows various roads and boundaries.

A black, rectangular, recessed light fixture is mounted on a light-colored wooden wall. The fixture has a dark, possibly black, frame and a translucent, slightly textured lens. It is positioned in the center of the frame, with the wood grain of the wall visible around it. The lighting is even, and the fixture appears to be a standard indoor lighting unit.



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EXISTING CONDITIONS
NOT FOR
CONSTRUCTION

PR# 2018-001577
CASE# SI-2024-00994

**DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPC**
7401 PASEO DEL VOLCAN NW
ALBUQUERQUE, NM 87121

DRAWING NAME
LIGHTING DETAILS

SHEET NO.

AE3.26

A. THIS SITE PLAN DEPICTS ALL KNOWN CONDITIONS AS DETERMINED FROM FIELD OBSERVATIONS AND DIGITAL RECORDS. IN THE EVENT THAT REVISIONS TO THIS SITE PLAN AND RELATED SHEETS IS REQUIRED, THE BURDEN OF DOCUMENTING COMPLIANCE WITH IDO STANDARDS FOR ELEMENTS LIKE

[illegible]

A photograph of a fenced-in area, likely a sports field or parking lot, with a chain-link fence and a tall light pole. A large shadow is cast across the foreground gravel area.

A large, grey concrete retaining wall with a curved top edge, set against a blue sky with scattered clouds. The foreground is covered in gravel and small, sparse green plants.

A wide-angle photograph of a large, empty gravel lot. In the background, there is a concrete wall, some trees, and a building on the left. The sky is clear and blue.

A photograph showing a fenced-in area with sparse, dry vegetation. The fence is made of chain-link fencing and runs across the middle of the frame. Behind the fence, there are some low-lying bushes and a small, light-colored building. The sky is blue with scattered white clouds. The foreground is covered in dry grass and small plants.

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TESTING CONDITIONS
NOT FOR
CONSTRUCTION

ECT

2018-001577
SE# SI-2024-00994

SITE PLAN-EPC
7401 PASEO DEL VOLCAN NW
ALBUQUERQUE, NM 87121

DISCUSSION

OWN BY	AM
LEASED BY	VE
08/5/2024	
PROJECT NO.	21-0082.014

PLUMBING, FENCING, AND SEENING DETAILS

T NO.

E3.27

J) PROPOSED DESIGN STANDARDS

DESIGN STANDARDS

The Double Eagle Airport Design Standards (DEII Design Standards) are intended to create a basic level of consistency for site, landscape, architecture, and lighting across disparate types and scales of projects at DEII airport. The DEII Design Standards establish a framework that strengthens the identity of the airport as individual sites within it are developed.

The DEII Airport site as a whole is zoned as NR-SU, Non-Residential Sensitive Use, and follows the approval process as laid out in the City's Integrated Development Ordinance. These DEII Design Standards will be used to inform new development within the boundaries of the airport. In addition to the City of Albuquerque Planning Department's approval process, all buildings, structures, and site plans for projects at DEII shall be reviewed and approved by the City of Albuquerque Aviation Department to ensure compliance with relevant Federal Aviation Administration (FAA) regulations. The DEII Design Standards will also help provide a framework for review of proposed site developments. Subsequent site development plan requirements for developments on lease areas of 5 acres or less or that contain buildings of 50,000 sq.ft or less shall be delegated to the City of Albuquerque's Development Facilitation Team (DFT) as a Minor Amendment. Development greater than these thresholds shall be delegated to the Environmental Planning Commission (EPC).

A. INTRODUCTION

The DEII Design Standards provide a framework to achieve the following goals. Unless stated otherwise, the DEII Design Standards are assumed to apply to all areas within the DEII boundary.

- Create an attractive built environment that promotes the unique sense of place and identity of DEII, and supports opportunities for non-aeronautical, revenue generating development.
- Define a common aesthetic for site, architectural and landscape design that results in a consistent identity across the DEII property.
- Encourage quality developments that further the successful operation of DEII and are compatible with the surrounding visual context.

B. PROCESS

- All development shall comply with the City's IDO and the Development Process Manual (DPM), except where FAA regulations supersede or is stated otherwise within these DEII Design Standards.

C. SITE DESIGN

Site design shall be utilized to create a functional environment for employees and visitors. Buildings on site should be designed in a manner that reinforces the identity of DEII. Future buildings shall be laid out to work with overall airport circulation corridors, with flight pathways and approaches, and be respectful of adjacent/proximate open spaces.

- Buildings shall not be located within 150 feet of any property lines that abut Major Public Open Space. See more detail about buffering in the screening section.
- Cross access easements between adjoining tracts are encouraged.
- Minimum and maximum dimensional standards are summarized below:

DEII DIMENSIONAL STANDARDS SUMMARY TABLE	
STANDARDS	REQUIREMENT
Setbacks	
Setback from Major Public Open Space	150 feet
Setback from Atrisco Vista Blvd	150 feet
Setback from Double Eagle Rd	12 feet
Setback from Adjoining Properties or Lease Lines	25 feet
Taxilane Setback	35 feet
Building Height	
Within 250' of Atrisco Vista Blvd ROW	60 feet, maximum
All Other Areas	85 feet maximum*, provided they comply with FAA FAR Part 77
Parking Setbacks	
Setback from Major Public Open Space	100 feet**
Setback from Atrisco Vista Blvd	25 feet**
Setback from Property Line or Lease Line	10 feet, except when a shared parking lot is provided
*See Building Height section for exceptions.	
**See Screening section for appropriate screening measures for parking lots.	

D. ACCESS AND CIRCULATION

DEII is uniquely situated adjacent to a designated Major Public Open Space (Petroglyph National Monument) while operating as an airport servicing many types of vehicular traffic. Atrisco Vista along the east side of the airport providing access to and from Interstate 40. To provide safe and appropriate transportation infrastructure the DEII Design Standards shall govern.

- Access off Atrisco Vista Blvd shall be coordinated between the City's Aviation Department, NMDOT, and other applicable agencies.
- Sites should be designed to minimize conflicts between automobiles, trucks, bikes, and pedestrians to create an organized system of entrances, driveways, parking lots, and delivery areas.
- The City's Aviation Department shall designate specific areas for the pickup and drop-off of passengers and guests utilizing rideshares, taxi, and guest transportation within the aviation areas.
- Passenger and guest transport vehicles, including rideshare, taxis, shuttles, etc., shall proceed to the designated drop-off and pickup location at DEII.
- Passenger and guest transport vehicles should not enter the airfield gates or travel elsewhere without explicit permission and/or escort by DEII personnel.
- The City's Aviation Department shall designate delivery routes for the aviation areas of the property. All tenants must submit commercial delivery routes to DEII for approval.
- A minimum six (6) foot clear pedestrian pathway shall be provided from the primary entrance of the primary buildings to larger site circulation systems and to adjacent public rights-of-ways as feasible. Pedestrian pathways may consist of simple striping on asphalt.
- Commercial delivery trucks must enter DEII within the secured air operations area, follow defined DEII delivery routes, and must be escorted at all times; all unescorted commercial delivery traffic is prohibited.

E. SETBACKS

The use of building and parking area setbacks is required to provide space for the creation of a visually attractive entrance streetscape, provide a safe and compatible environment for aircraft, service vehicles, and pedestrians throughout DEII, and provide buffers to adjacent public open space. Elements required within these setbacks will be pedestrian walkways, screening devices, and landscape improvements.

- Minimum building setbacks shall be as follows:

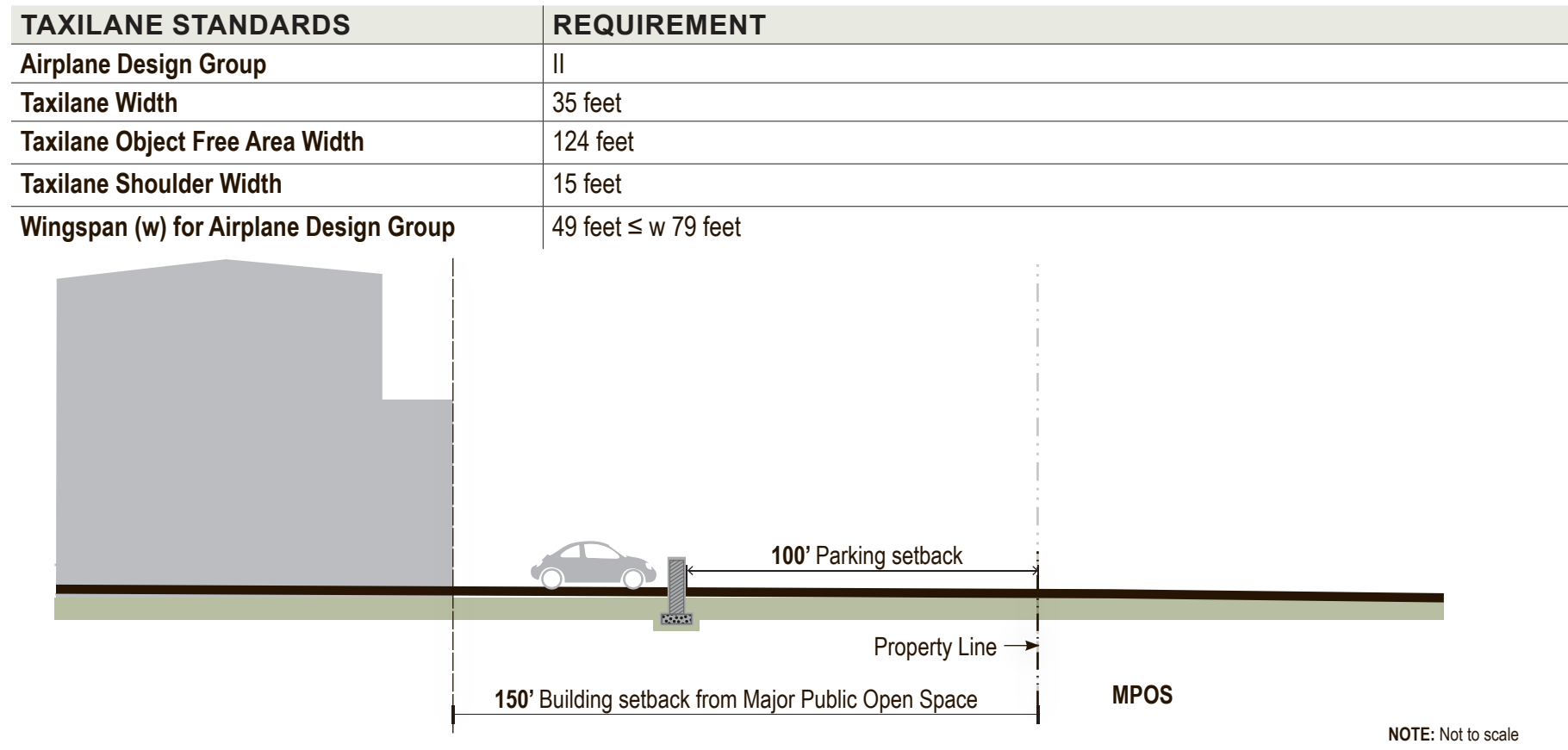
BUILDING SETBACK STANDARDS	REQUIREMENT
Setback from Major Public Open Space	150 feet
Setback from Atrisco Vista Blvd ROW	150 feet
Setback from Double Eagle Rd	12 feet
Setback from adjoining property or lease lines	25 feet
Taxilane Setback	35 feet

- Buildings shall not be located closer to the airfield than the Building Restriction Line as shown on the "Airport Layout of the Plan" of the DEII Master Plan.
- Buildings shall maintain a clear line of sight from the air traffic control tower controller positions to airfield pavements on the airfield.
- Minimum parking lot setbacks shall be as follows:

PARKING SETBACK STANDARDS	REQUIREMENT
Setback from Major Public Open Space	100 feet*
Setback from Atrisco Vista Blvd or other public right-of-way not specified	25 feet*
Setback from Double Eagle Rd	6 feet
Setback from Property and Lease Line	10 feet, except when a shared parking lot is provided
*See screening section for appropriate screening measures for parking lots.	

F. TAXILANES

- Aircraft circulation throughout DEII shall be developed with minimum interaction with vehicular traffic. Taxilane standards are based on an average tailing speed of at least 20 mph and wingspan and wingtip clearance. Taxilane access must have a minimum right-of-way width of 150 feet with a minimum paved surface width of 50 feet.
- Taxilane shoulders must be either stabilized or paved to reduce the possibility of blast erosion and engine ingestion problems.
- Minimum separation between centerline of taxilane to structure (parked aircraft, structure, etc.) is 0.70 times the wingspan of the largest airplane, plus 10 feet. Setback standards have been designed to accommodate all Type I and II aircraft.
- Taxilanes shall be designed as follows:



- DEII facilities located adjacent to the airport taxilane system or service corridors are assumed to support aviation related activities, which may require outside storage of aircraft as well as airfield maintenance equipment.

G. PARKING

Parking areas should be designed to mitigate heat/glare through the provision of landscaping, minimize the visual impact of parking areas, and provide accessible and safe circulation within and adjacent to the parking areas.

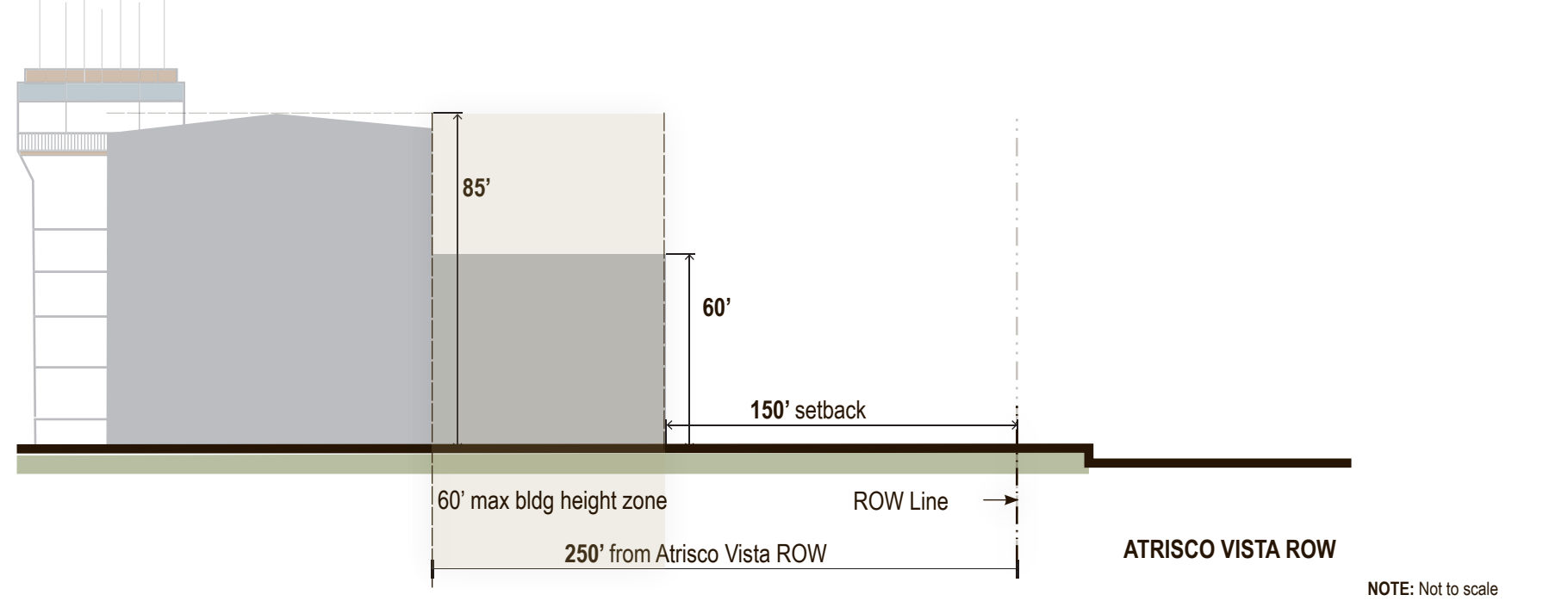
- Off-street parking areas, including minimum off-street parking requirements, space size, aisle widths, pedestrian pathways, and screening, shall comply with Section 14-16-5-5 of the City's IDO and the DPM.
- The City's Aviation Department shall approve the design of all parking areas on DEII property.
- Car/vanpool preferred parking shall be provided near building entrances and shall be clearly demarcated, provided they do not conflict with FAA safety regulations and are ADA compliant.
- Bicycle parking at a ratio of one (1) bicycle rack per 50 vehicular parking spaces shall be provided to encourage non-vehicle use. Bicycle racks should be conveniently located near building entrances.
- Bicycle racks or lockers shall be installed per the City's DPM.

H. ARCHITECTURE

In general, all buildings should comply with the building design standards of the City's IDO. However, in cases where the DEII Design Standards contradict the City's IDO, the DEII Design Standards will govern. The focus of these architectural standards is to create a cohesive and safe environment with the DEII property boundaries. Buildings should convey an aesthetic appropriate to the middle Rio Grande Valley and dramatic setting of the West Mesa. Architectural design should respond to climate, views, solar access, and aesthetic considerations. Building designs should express the nature of aviation, as appropriate to the specific permissive use. Specific architectural styles shall not be dictated. However, the architectural style of the non-aeronautical uses shall be complementary and include common design elements.

BUILDING HEIGHT

- Maximum allowable building heights shall not exceed 85 feet, except as follows:
 - Within 250 feet of Atrisco Vista Blvd right-of-way where maximum shall not exceed 60 feet
 - All building heights and structures shall comply with FAA FAR Part 77 regulations.
 - Maximum allowable building heights do not apply to critical aeronautical support infrastructure such as Air Traffic Control Towers.



BUILDING DESIGN

- The City's IDO Building Design Section 14-6-5-11 does not apply to the NR-SU zone, however, the scale, proportion, and composition of fenestration of facades of non-utilitarian buildings shall be designed to create visual interest, as approved by the City's Aviation Department.
- All primary and accessory buildings must comply with Major Public Open Space setback requirements.
- The major entry to each primary non-utilitarian buildings shall be clearly defined, face the primary circulation corridors, and connect to pedestrian pathways to the maximum extent feasible.
- Columns, corner articulation, overhangs, awnings, gutters and scuppers, breezeways, and soffits shall be carefully dimensioned and detailed to provide a human scale and visual interest.
- Special attention shall be given to the articulation of aircraft hangar facilities through use of color, materials, or building offsets. Metal structures are acceptable, provided they visually tie to other building elements, such as the front office, storage areas, etc.
- Any glass or metal elements must comply with FAA standards addressing glint and glare.
- Temporary buildings are subject to the temporary structure standards of Section 14-16-5-4-3 of the City's IDO and shall be allowed for a maximum period of one (1) year. After one-year, temporary structures must be removed or improved to meet DEII standards.

MATERIALS AND COLORS

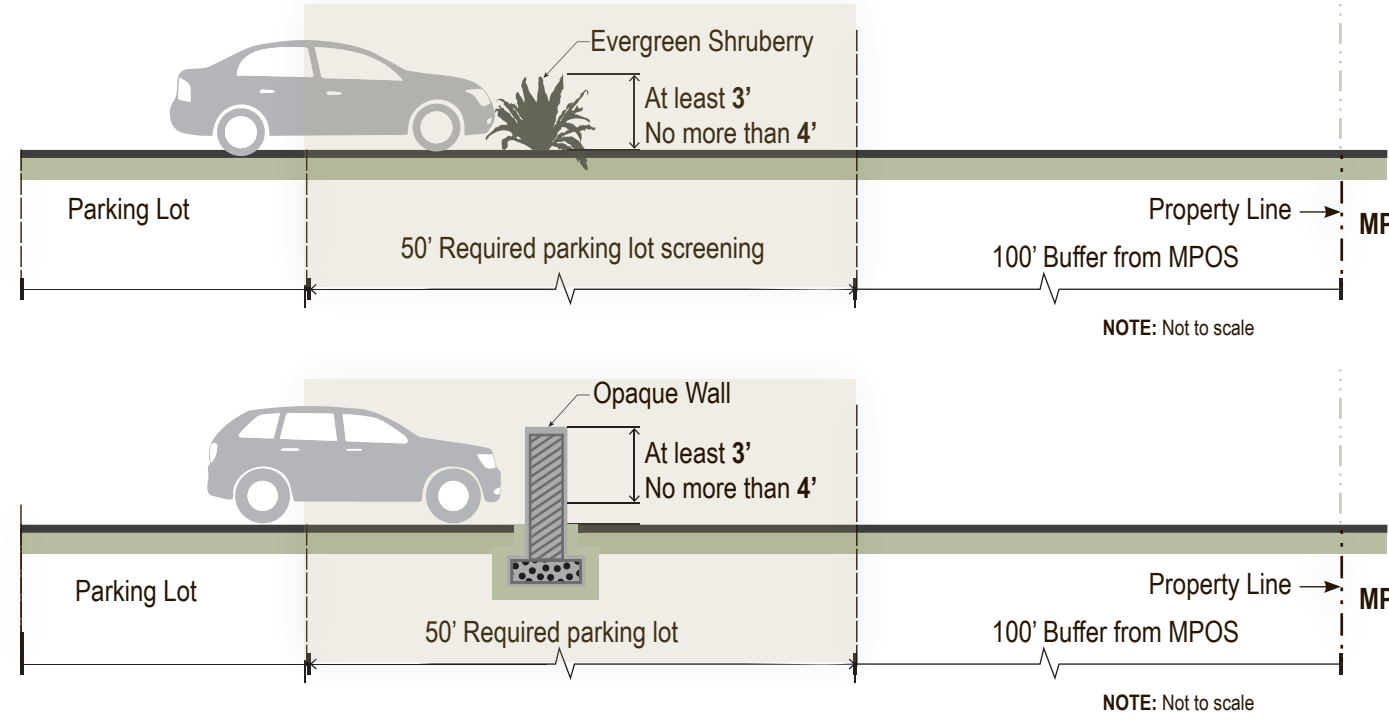
- Acceptable colors of exterior building materials for all development on DEII should take their cues from the native New Mexican landscape palette, including but not limited to: sand, terracotta, gray and charcoals, blues and teals, and sage green. Due to the proximity to the Major Public Open Space, subdued colors are preferred.
- Buildings sited within 250 feet of the property line of Major Public Open Space shall:
 - Utilize colors that blend with the surrounding natural environment and generally include yellow ochres, browns, dull reds, and grey greens.
 - Limit the colors of exterior surfaces of structures such as mechanical devices, roof vents, and screening materials to those with light reflective value (LRV) rating between 20 percent and 50 percent.

- The use of accent colors and materials is encouraged to bring out detailing to better articulate or give scale to a building, including glazed tile, wood trim, tile roofing, paint, metal, etc.
- Special consideration shall be given to roof structures. The use of contrasting colors between roofs and walls is encouraged to help differentiate the planes of building masses.
- Prohibited building materials include the following:
 - Plastic or vinyl building panels, awnings, or canopies
 - Exposed, untreated precision block or wood
 - Highly reflective surfaces
 - Materials with high maintenance requirements
 - Buildings that are predominately white in color

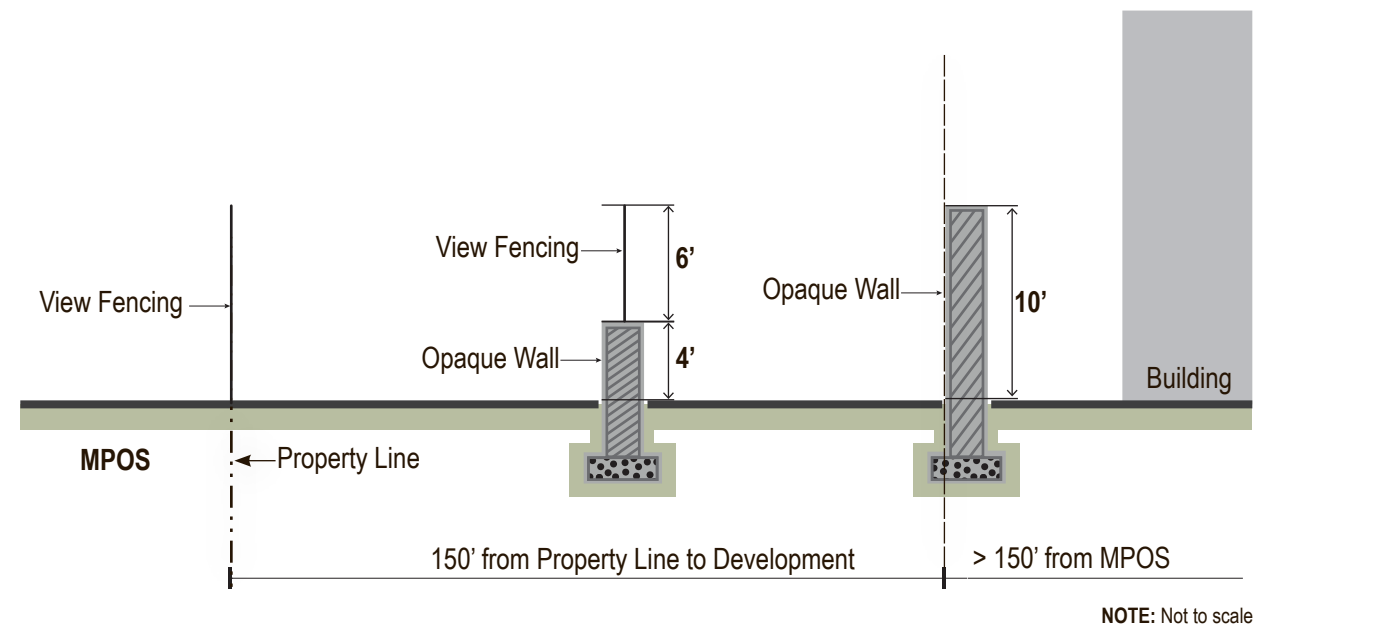
I. SCREENING

Screening of less attractive areas such as loading, service, and/or mechanical area, including but not limited to exterior refuse facilities, mechanical equipment, storage yards, and loading docks, should be accomplished using walls and fences or landscape buffers that are designed to be an integral part of DEII's overall aesthetic. All such functions, except for airplanes viewed from taxilanes, should be designed in an aesthetically pleasing manner, or screened from Atrisco Vista Blvd, and from any designated Major Public Open Spaces.

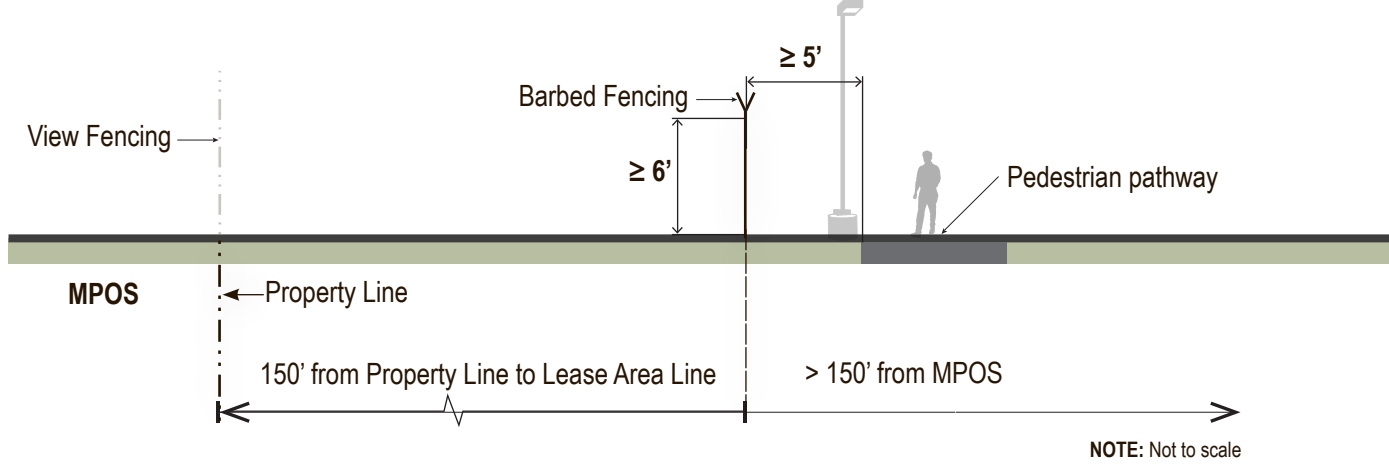
- Parking lots located within 150 feet setback of Major Public Open Space, 50 feet from the right-of-way edge of Atrisco Vista Blvd or Double Eagle Rd shall be screened through one of the following mechanisms:
 - A strategic building placement that blocks the view of the parking lot, provided buildings comply with minimum building setbacks,
 - An opaque wall a minimum of three (3) feet in height but not more than four (4) feet in height,
 - A minimum 6 foot wide landscape buffer with a continuous line of evergreen shrubbery a minimum of three (3) feet in height but not more than four (4) feet in height, or
 - By other means that comply with FAA and are approved at the discretion of the City's Aviation Department.



- Perimeter wall/fences, if provided, shall comply with Section 14-16-5-7 of the City's IDO, except:
 - Maximum wall heights shall be based on the NR-GM zone.
 - Perimeter wall/fence within 150 feet of Major Public Open Space shall comply with view fencing standards as defined in the City's IDO which require any portion of the wall above 4 feet in height to be at most 50 percent opaque. Chain-link fencing may constitute as view fencing within DEII as approved at the discretion of the City of Albuquerque Aviation Department.



- Barbed tape, razor wire, barbed wire, or similar materials are permitted, provided they meet all of the following criteria:
 - The wall/fence is at least 6 feet in height.
 - The wall/fence is set back at least 5 feet from any sidewalk or pathway. If any portion of the sidewalk or pathway is within the lot line, the setback shall be measured from the edge of the sidewalk closest to the wall.
 - The wall/fence is located more than 150 feet from Major Public Open Space.



- Loading, service, and refuse areas located within 250 feet of Atrisco Vista Blvd, other public streets, or Major Public Open Space shall be screened from view by an opaque decorative wall or fence or landscaping at least six (6) feet tall that is compatible with materials and design of the building with which it is associated. Passenger loading areas do not require screening.
- Mechanical equipment shall be screened from Atrisco Vista Blvd, other public right-of-way, and the Major Public Open Space pursuant to Section 14-16-5-5-6 of the City's IDO and be compatible with materials and design of the building with which it is associated.

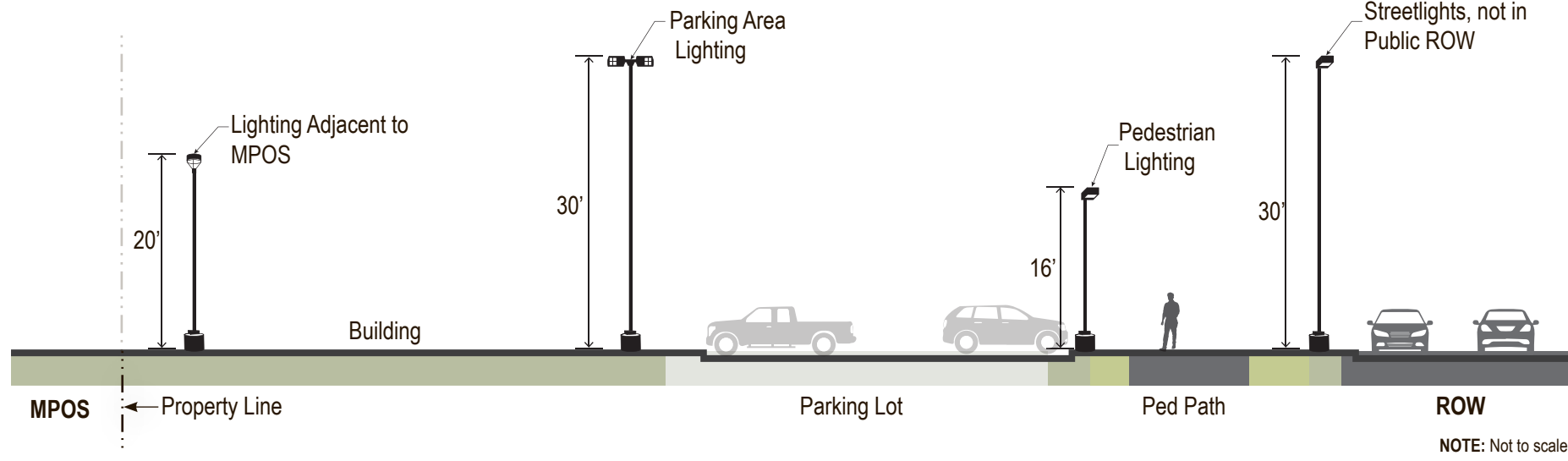
J. LIGHTING

Lighting should enhance the safety, security, and visual aesthetics of DEII. The primary objective of site lighting shall be to maximize public safety without impacting the FAA requirements of the site or generating unnecessary glare or reflection to adjacent properties or Major Public Open Spaces.

- All lighting shall generally comply with the outdoor lighting standards of Section 14-16-5-8 of the City's IDO. The City's Aviation Department may deviate from lighting standards where needed due to safety, aviation or other site related uses.
- Specific lighting fixtures styles shall be established by / reviewed by the City's Aviation Department and shall be consistent with other projects within DEII property. Lighting fixtures shall be fully shielded horizontal lamps to eliminate light trespass beyond the property line and no light source shall be visible from the site perimeter except where FAA regulations and standards may vary.

3. The maximum height standards of light fixtures shall not exceed the following:

LIGHTING STANDARDS	REQUIREMENT
Streetslights, not including public ROW	30 feet
Parking Area Lights	30 feet
Pedestrian Lighting	16 feet
Adjacent to Major Public Open Space	20 feet



4. Lighting along Atrisco Vista Blvd shall take into consideration potential impacts to the adjacent Major Public Open Space.
5. Lighting shall not impinge upon airfield sight planes, nor shall it be directed at aircraft movement areas, runway approaches, or aviation towers.
6. Controlled, directional lighting should be used to highlight public spaces and walkways.
7. The pattern of light pooling from each fixture shall avoid glare or reflection on adjacent properties, buildings, or roadways.

K. SIGNAGE

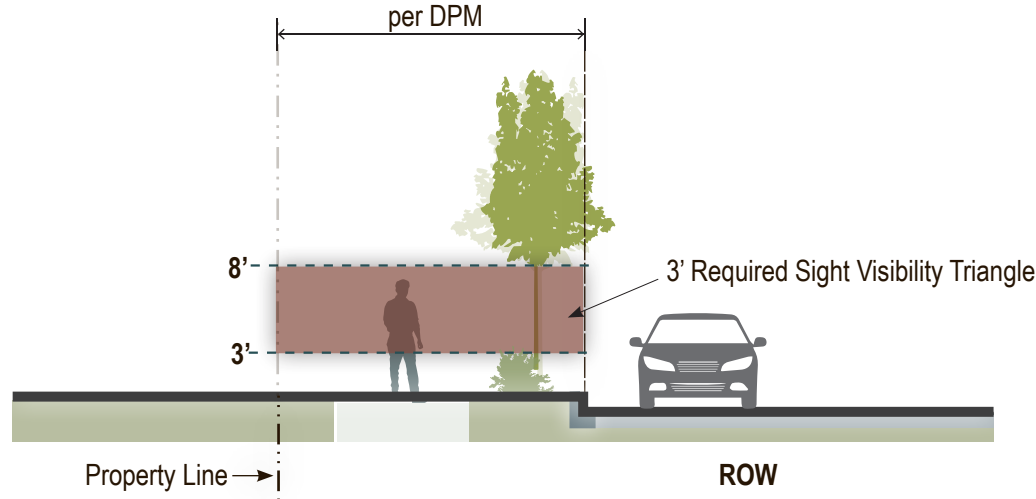
Signage shall create a sense of arrival to DEII and ensure proper wayfinding for visitors, employees, and deliveries.

- Signage design and locations shall be approved at the discretion of City's Aviation Department.
- Monument signs not exceed 15 feet in height and a maximum of 200 sq. ft. in sign area. The colocation of tenants on fewer overall monument signs is encouraged and signs shall be coordinated to be complementary to established precedents on DEII property.
- Directional signs to direct visitors and delivery services may be provided at the discretion of the City's Aviation Department.
- Wall-mounted signage shall not exceed 25% of the façade area upon which it is mounted.
- Prohibited signs include the following:
 - Brashly colored signs with moving or flashing lights.
 - Animated, flashing, or electronic signs.
 - Portable signs.
 - Pennants, or ribbons may be permitted for special, time-limited events at the discretion of DEII. Temporary signage must be removed by the agreed-upon timeline and must comply with FAA requirements.
- No sign may overhang a property line, intrude upon any architectural or decorative features, or interfere with clear sight requirements of the City's DPM.

L. LANDSCAPING

Site landscape shall serve to enhance the visual aesthetic of the campus and aid in reinforcing the street presence and pedestrian experience without interfering with the site's primary aviation function. The primary focus for site landscape shall emphasize visitor and employee centric spaces of the campus including the main entry, primary buildings entrances, pedestrian pathways, and outdoor seating and gathering spaces. The intent is to provide year-round color and interest and reflect the natural environment of New Mexico landscape.

- Aeronautical uses shall not be required to comply with the City's IDO landscaping to ensure compliance with FAA requirements. The Aviation Department may provide landscaping on forward facing public buildings to create a welcoming environment at their discretion.
- Streetscape landscape improvements shall be provided within the required building setback along Double Eagle Road or other on-site rights-of-ways to create a sense of presence and arrival into DEII. Plant materials within right-of-way may be counted toward their overall net lot requirements.
- Street trees on public ROWs shall be required every 25 feet, provided they do not interfere with FAA regulations or aeronautical operations.
- Future development tracts within DEII property shall be maintained in their natural condition until developed. If these areas are disturbed, they shall be re-vegetated with native seed mix appropriate for this region.
- All plant material shall be maintained by the tenant in a living, attractive condition. All areas shall be maintained and be free of weeds.
- Plant materials shall not interfere with clear sight requirements of the City's DPM. Trees and shrubs between 3 and 8 feet in height (as measured from the gutter pan) shall not be acceptable in this area.



- Development tracts adjacent to the Major Public Open Space, with the exception of areas within the Airport Runway Protection Zone of the City's IDO, should focus landscape improvements within required building setbacks to provide a visual barrier. Plant coverages and selections within this buffer shall avoid the appearance of a hard, straight lines to mimic the natural environment of the adjacent open space.
- A specific landscape palette should require review by the City's Aviation Department's USDA Wildlife Biologist and conform to current FAA regulations, including those restrictions of specific plants and trees that attract animals that conflict with aviation operations.
 - The use of native or regionally adapted plant materials that will thrive in local conditions with less fertilizer, water, and maintenance as feasible.
 - Fruit and nut trees or shrubs, which may attract wildlife, shall not be permitted on DEII.
 - High water use turf is restricted to patio or public gathering areas within DEII.
- Landscape design shall incorporate active and passive water harvesting techniques to reduce potable water use for irrigation where feasible without interference to site operations.



- All landscape areas shall be stabilized with mulches or plant material to keep soils from washing or blowing away. Gravel mulch, bark mulch, and similar materials are acceptable as a top dressing for landscape areas. An automatic underground irrigation system is required to support all site landscaping.
- The system shall be designed to avoid over-spraying onto pedestrian paths, buildings, fences, etc.

M. SUSTAINABILITY

Green architectural design is encouraged, with a particular emphasis on creating facilities that do not rely on carbon-producing sources of energy. DEII strongly encourages the use of sustainable building and site practices emphasized by rating systems such as LEED, Envision, PEER or similar to promote environmental stewardship and resource efficiency. DEII supports the use of solar photovoltaic, wind turbines (if compliant with glint and glare studies), ground-source heat pumps, and other alternative energy production facilities on site.

- Stormwater control measures shall be designed to manage the first flush and control runoff generated by contributing impervious surfaces.
- Buildings shall be oriented to take advantage of heat gain in the winter where possible while coordinating with shading strategies to inhibit solar gain in the summer.
- Roofs shall drain water to areas which are landscaped appropriately for such run-off.
- Permeable paving may be used on site within parking areas, or pedestrian areas provided it does not compromise the ability to carry the applicable loads required for parking, vehicular activity including freight trucks, or trash hauling vehicles.
- Parking islands, where provided, shall allow flow of water as required to maintain proper drainage of the site.
- Grasses and other ground vegetation should be placed near project edges to help filter and slow runoff as it exits and enters the site.

N. UTILITIES

Utility systems shall ensure the minimum expectations for the design of new or renovated service connections at DEII. The systems shall enhance the safety measures of FAA and the aesthetic of DEII.

- Developers shall contact PNM's New Service Delivery Department to coordinate electric service and options for the location of electric service connection. Any existing or proposed public utility distribution easements are to be indicated on subsequent Site Plan for Building Permit utility sheets. PNM's standard for public utility distribution easements is 10 feet in width to ensure adequate, safe clearances.
- All electric distribution lines shall be placed underground.
- Transformers, utility pads, and telephone boxes shall be appropriately screened with walls and/or vegetation when viewed from the public right-of-way. Screening of transformers, utility boxes and other utility structures shall not impede access to the equipment and shall provide for safe maintenance and repair by utility workers.
- Transformers, utility pads, and ground-mounted equipment screening shall allow for access to utility facilities. All screening and vegetation surrounding ground mounted transformers and utility pads are to allow 10 feet of clearance in front of the equipment door and 5-6 feet of clearance on the remaining three sides for safe operation, maintenance, and repair purposes. Refer to the PNM Electric Service Guide at www.pnm.com for specifications.
- When an above-ground backflow prevention device is required by the City of Albuquerque. If prefabricated fiberglass enclosures are used, they shall be appropriately screened from view by walls and/or landscaping.
- Battery Energy Storage Systems (BESS) shall comply with DEII Master Plan setback and screening standards and City's IDO Airport Protection Overlay, Noise Protection Sub-area Zone of the City's IDO. Day-Night Noise Levels shall comply with Section 14-16-3-3.
- Coordination with the Albuquerque Bernalillo County Water Utility Authority (ABCWUA) is necessary for each development to serve sites for water, fire protection, and sanitary sewer service.
- Coordination with other dry utilities including but limited to NM Gas, Comcast, Lumen and other communication providers is required based on development needs.

SEAL

EXISTING CONDITIONS
NOT FOR
CONSTRUCTION

PROJECT

PR# 2018-001577
CASE# SI-2024-00994

DOUBLE EAGLE-II AIRPORT
SITE PLAN-EPC
7401 PASEO DEL VOLCAN NW
ALBUQUERQUE, NM 87121

REVISIONS



DRAWN BY	AM
REVIEWED BY	VE
DATE	08/5/2024
PROJECT NO.	21-0082.014

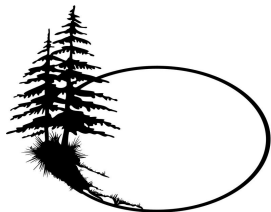
DRAWING NAME
DESIGN STANDARDS

SHEET NO.

SD 4.2

EPC Public Comments
48-hour Materials

First Name	Last Name	Your email	Your Address	Interested Party	Is this case Citywide?	Case Type	Project Number	EPC Case Location Description	Comment	Comment PDF	Submission date	Case Number
Baxter	Sosebee	mb422@comcast.net		Resident	No	Master Plan (City)	PR-2018-001577	Double Eagle II master plan	See attached PDF	Opposition to DEII Master Plan.pdf	8/12/2024 14:16	SI-2024-00994 Double Eagle II Airport Site Plan
Evelyn	Rivera	rioreal@earthlink.net	4505 Chadwick Rd NW	Neighborhood Association Repres	No	Site Plan - EPC - Major Amendme	Project # PR-2018-001577 SI-2024-00994 – Site Plan	Double Eagle Airport	I write in opposition to proposed development in this area that would allow building heights to exceed 36 feet. The proposed plans should be thoroughly vetted by the Tribes whose culture and heritage is embedded in this area. Thoughtful consideration should be given to protecting the Petroglyph National Monument, an asset unique to Albuquerque with historical and cultural significance. Additionally, the area lacks the infrastructure to accommodate intense uses.	N/A	8/12/2024 20:27	SI-2024-00994 Double Eagle II Airport Site Plan
Jane	Baechle	Jane.Baechle@gmail.com	7021 Lamar Ave NW, Albuquerque, NM, 87120, USA	Neighborhood Association Repres	No	Site Plan - EPC [new]	PR-2018-001577	This case requests approval of a Site Plan-EPC for the Double Eagle II airport located on the West Mesa and west of Atrisco Vista and Petroglyph National Monument.	Dear Commissioners, Attached is a letter outlining the position of the Santa Fe Village Neighborhood Association Board on this project and site plan application.	SFVNA DEII Site Plan.pdf	8/12/2024 21:21	SI-2024-00994 Double Eagle II Airport Site Plan



Santa Fe Village Neighborhood Association

5601 Bogart St. NW Albuquerque, NM 87120

SFVNA2014@gmail.com

Date: August 12, 2024

To: Jonathan Hollinger
Chair, EPC

From: Jane Baechle
Representative, SFVNA

Re: Project # PR-2018-001577
SI-2024-00994 – Site Plan-EPC

Dear Chair Hollinger and Commissioners,

On behalf of the Santa Fe Village Neighborhood Association (SFVNA), I write again to affirm our opposition to building heights as originally proposed in this site plan application that would have permitted structures between 60 and 85 feet in height. We appreciate the analysis of Planning staff as documented in the Staff Report. Their assessment of the impact of building heights on the heritage landscape of this area is congruent with ours.

I will refrain from repeating all of the ABC Comp plan goals and policies with which the original proposed building heights conflict. I note, however, that our prior public comments have opposed proposed changes to the Double Eagle II based on their actual or potential conflict with the ABC Comp Plan. Specifically we have cited its mandate to “Protect, reuse, and/or enhance significant cultural landscapes as important contributors to our heritage and rich and complex identities” as defined in Goal 11.3 Cultural Landscapes, “POLICY 11.3.1 Natural and Cultural Features: Preserve and enhance the natural and cultural characteristics and features that contribute to the distinct identity of communities, neighborhoods, and cultural landscapes” and every one of its seven sub-policies which outline specific measures to accomplish protection of the landscape.

The ABC Comp Plan specifically cites the Petroglyph National Monument in its list of heritage landscapes in “POLICY 11.3.4 Petroglyph National Monument: Regulate adjacent development to protect and preserve the Petroglyph National Monument– its volcanoes, petroglyphs, and Northwest Mesa Escarpment – as a priceless cultural landscape and community resource that

provides physical, cultural, and economic benefits. Sub-policy b directs that development “Preserve and protect the Monument from growth and development pressures on the West Side.” Protection of Community Identity, Natural and Cultural Features and the provision of Context Sensitive Design are critically important to ensure that development within the city serves *all* of its residents and others who value and respect the landscape including those who view it while looking at the western horizon.

The SFVNA leadership and multiple individual residents of Santa Fe Village are deeply committed to protecting the Petroglyph National Monument and the natural and cultural landscape of this entire area, not solely the escarpment that virtually surrounds our homes in Santa Fe Village. We recognize and value the heritage and link to those who have come before us and those who will follow us that this landscape represents. We expect to continue to engage on every proposal to ensure that any development respects the central contribution to our heritage that the Monument and surrounding landscape represent.

I am well aware of the IDO language addressing requirements for notification and public meetings. I appreciate the Open House provided on June 20, 2024 and the Aviation Department meeting with a delegation of interested members of the WSCONA Executive Committee. Nonetheless, the natural and cultural landscapes across the City belong to all of us and particularly to Tribal Nations who have stewarded this land for hundreds of years before it became the City of ABQ. Robust public engagement requires that all possible efforts to notify and hear voices and every possible good faith effort to engage have been made.

Genuine public engagement is even more vital in this matter. The Double Eagle II airport lies within the West Mesa Community Planning Area. To date, no area assessment for this CPA has even begun; no effort has been made to actually engage those of us who live in the West Mesa CPA to speak to what we view as the essential features and elements of our community identity. Yet, this site plan application proposes to dramatically and permanently alter the landscape which is a central feature of our area.

Commissioners, you have considerable authority in this case. I respectfully request that if you approve any plan for further development in the Double Eagle II, that it adheres to the Goals and Policies of the ABC Comp Plan, respects the analysis of Planning Staff and includes all of the conditions they have outlined including those that limit building heights to 36’ and require measures to protect the Petroglyph National Monument and the natural and cultural landscape.

Thank you for your attention.

Respectfully submitted,

Jane Baechle
Representative, SFVNA

Opposition to DEII Master Plan

EPC, please add to your public record for 15 Aug 2024 meeting

EPC Project # PR-2018-001577
SI-2024-00994 – Site Plan-EPC

From: Baxter Sosebee, a concerned citizen of Albuquerque currently residing in Petroglyph Estates

I attended an open house in Jun 24 about the DEII master plan. At this meeting many concerns, including mine, were raised about the proposed building heights. Apparently these concerns were ignored since Part C in design standards paragraph allows 60 and 85 foot building heights.

I oppose the master plan as written.

The argument that the control tower is 85' and therefore other buildings can be the same height just doesn't pass the giggle test. A control tower is essential for airport safety to provide sight lines to both air traffic and ground movement. It also covers a small physical footprint on the ground. I can't imagine a business structure that requires height for the same essential reasons. Others will comment on the impact to views, cultural sensitivity and environment so I will not repeat here. Proposed heights only serve developer profit while ignoring environmental and safety impacts. Instead I will point to existing structure heights at both DEII and the ABQ sunport which fall into the 3 story height, 25-36 feet or less. I would recommend the master plan authors review the IDO and set a similar height restriction that allows for a 3 story structure but no more. 3 stories is plenty for any film production or light manufacturing operation without the negative impacts of a 85' limit.

Thank you for considering my input,
Baxter Sosebee

Tuesday, August 13, 2024

August 5, 2024, Re: Double Eagle II Site Plan – EPC application comments; Project # PR-2018-001577 SI-2024-00994 – Site Plan-EPC

Dear Environmental Planning Commission,

Thank you for the opportunity to provide feedback on the site plan and design guidelines for the Double Eagle II airport located west of Petroglyph National Monument.

I am writing on behalf of myself. I am the current President of the Westside Coalition of Neighborhood Associations (WSCONA), a member of the ICC Working Group, an elected Commissioner of the Paradise Hills Special Zoning District, and a retired planner and federal consultant. My firm was retained for planning, development, and land review issues.

WSCONA and our member neighborhood associations have yet to receive several prior notifications for this project provided by the City of Albuquerque Aviation Department. However, members of our WSCONA Land Committee met with Albuquerque Aviation staff last month concerning this project and omissions in the planning process, community input, Federal Aviation Administration planning, and public comment requirements, and the unintended impacts of these plans on neighborhoods and the Petroglyph National Monument.

WSCONA is on a brief summer hiatus. Our membership's next regular meeting is on September 4, 2024. Before our summer break, we reviewed concerns with our membership, and both the planner and associate director were guest speakers during recent prior WSCONA meetings.

Our member neighborhood associations continue to be deeply concerned about the potential impacts on the Petroglyph National Monument due to the abbreviated planning review. We met with DEII and Aviation staff on July 23, 2024, and the design criteria were provided to us for our review on July 31, 2024 by EPC Planning Staff. We understand that the City of Albuquerque Aviation Department is no longer proposing to update the Double Eagle II Airport Master Plan and is instead requesting the Environmental Planning Commission (EPC) approval of a Site Plan and design guidelines.

This request is premature. I concur with the letter from Nancy Hendricks, Superintendent of the National Parks Service Petroglyph National Monument, concerning the heights of proposed buildings and view sheds. These heights could potentially obstruct views of the monument, impacting its aesthetic and potentially its cultural value.

After conversations with CABQ Aviation Department staff in July, it became clear that consultants to the aviation department didn't complete any roads or traffic studies to ascertain the impacts of the proposed design guidelines on the fragile volcanic ecosystem or plant and animal habitats. The potential economic impacts of these guidelines, such as noise vibration and incredible wear to road surfaces from truck traffic, are significant. These guidelines have marketing goals without ascertaining the actual market or positive or negative impacts on other existing city or county economic zones. The relocation of existing companies already located in Albuquerque or Bernalillo County to the DEII complex may impact the short-term performance goals of staff but have lasting economic impacts on other parts of the city and certainly on this unique, fragile, and priceless environment. The primary industries in Albuquerque Bernalillo County are tourism and film, and this plan is undoubtedly detrimental to the environment of the Petroglyph National Monument.

In previous testimony before the EPA, CABQ Aviation staff represented environmental reviews as completed for the whole site when, in fact, only approximately one hundred acres were covered by the document. The Aviation Department omitted critical notifications to persons with standing (near communities and tribal nations identified by prior and current agreements with the National Park Service or the City of Albuquerque) required before submission to the FAA.

This planning effort, in its current state, is incomplete compared to prior DEII Master Planning efforts and the FAA federal guidelines in FAA, ARP SOP 5.1. There is a clear need for a more thorough and comprehensive planning effort. See FAA Order 1050.1F for more detail on actions subject to NEPA.

"Specific FAA actions subject to NEPA review include, but are not limited to, grants, loans, contracts, leases, construction and installation actions, procedural actions, research activities, **rulemaking** and regulatory actions, certifications, licensing, permits, plans requiring approval, and legislation proposed by the FAA. A CATEX refers to a category of actions that do not individually or cumulatively have a significant impact on the environment. A CATEX is not an exemption or waiver of NEPA review; it

is a level of NEPA review. An EA or EIS is not required if a proposed action falls within the scope of a CATEX described in FAA Order 1050.1F and 5050.4B and the following conditions can be met: 1) there are no extraordinary circumstances; 2) any extraordinary circumstances that are present can be eliminated or resolved through conservation. ARP SOP No. 5.1 Effective Date: June 2, 2017 2 measures included in the project design; or 3) any extraordinary circumstances that are present can be otherwise resolved through the completion of special purpose law requirement(s).”

Please don't approve as currently presented.

Thank you;

Elizabeth Haley M ARCH
6005 Chaparral Circle NW, Albuquerque New Mexico 87114
elizabethkayhaley@gmail.com
505 908 5376

8/12/2024 20:30

Project # PR-2018-001577 SI-2024-00994 – Site Plan
Double Eagle Airport

I write in opposition to proposed development in this area that would allow building heights to exceed 36 feet. The proposed plans should be thoroughly vetted by the Tribes whose culture and heritage is embedded in this area. Thoughtful consideration should be given to protecting the Petroglyph National Monument, an asset unique to Albuquerque with historical and cultural significance.

Additionally, the area lacks the infrastructure to accommodate intense uses.

Evelyn Rivera
Neighborhood Association Repres
rioreal@earthlink.net
4505 Chadwick Rd NW

August 13, 2024

Re: Double Eagle II Site Plan:

Project # PR-2018-001577,

Case # SI-2024-00994-Site Plan-EPC

Dear EPC Chair Mr. Hollinger and fellow Commissioners,

WSCONA members are very supportive of nearby Petroglyph National Monument and volunteer our time to protect these resources. We do this not only for ourselves but for the benefit of all of Albuquerque, as well as visitors who come to the west side to see the petroglyphs, visit our volcanoes and mesa top, which have incredible views. It is extremely important to maintain the view shed west of Petroglyph National Monument with compatible development that complements the natural resources and preserves the views.

Last April 18th, EPC reviewed Double Eagle II proposed changes to include 6 non-aeronautical uses. Concern was expressed that the new uses not conflict with the aviation activity nor impact the Monument; and requested an Impact Study be done to ensure this. Last week we learned that the EPC is now reviewing a site plan with design standards for the Double Eagle II airport. This did not give us time to organize a facilitated meeting to go over these standards.

We support the letters from the National Park Service and from Santa Fe Village NA, that are included in the staff report as they reflect the same concerns and values.

Fortunately the staff report contains a very comprehensive history of the Double Eagle Airport and made good recommendations in its conditions of approval.

Here are additional comments from our members:

1. In reviewing the staff report **we support Condition # 6.** Design standard (A) of approval in maintaining the building heights at a maximum of 36 ft. in height: "Building and **structure heights shall be limited 36 feet** at the DEII Airport except for critical aeronautical support infrastructure such as Air Traffic Control Towers." This height is consistent with the height of the buildings that currently exist, and preserve the view shed."

2. We also **support the staff report condition to move the required ROW trees to the interior portions of the Double Eagle II development**, where they would be more beneficial. **Thereby maintaining the natural vegetation** along the Atrisco Vista Blvd. corridor, which is attractive and preserves the natural and rural character of the area.

3. Double Eagle II, on the outskirts of the City, is a reliever airport for the Sunport. It is a rural airport designed for smaller aircraft surrounded by open space. It is not a downtown urban area. We want to make sure that the site plan indicates that the new non-aeronautical uses be compatible and not conflict the existing aircraft, nor impact the Monument.

4. This site is adjacent to Petroglyph National Monument a significant natural and cultural landscape that the Community and Tribes value. High density on the outskirts of town is inappropriate for this location and should not be considered.

5. Increased vehicle traffic is also a concern with all these recent changes. The site plan should not strive to be intensive for that area, which will overly increase the traffic volume.

Has a traffic study been done? If not, will it be done?

6. **Parking should not be reduced.** There is no available mass transit. We want to maintain adequate parking for existing and future uses.

7. Walls and Fences: Currently the fencing around the perimeter of Double Eagle is post and wire fencing, which is compatible with the rural open space character.

8. Landscaping: We like the beauty of the native plants in this area, and support using native plants exclusively in the landscape plan for the Double Eagle II Airport. We support maintaining the existing vegetation and avoiding disrupting the natural vegetation. This will reduce weeds, such as goat heads, and tumble weeds, which are flammable and can become a nuisance to both the airport and the Monument. We have been trying to educate people to not disrupt the native landscape to avoid this problem. Therefore, avoid or minimize any unnecessary disturbances. For areas that are disturbed we support that weeds be controlled and the area be reseeded with native plants.

9. **The view shed is critically important** as mentioned in the staff report. During the creation of Petroglyph Monument we learned from the Tribes that the volcanoes have special significance as they are centrally located in the middle between 4 major mountain ranges as noted in the National Park Service letter. We agree with the staff report that the proposed buildings heights of 60 to 85 ft are too high and should not go higher than 36 ft.. The aviation department, at recent meetings, said the building heights would be 30-36 ft. **We also support condition # 8 "Development shall not occur east of the right-of way (ROW) of Atrisco Vista Blvd and adjacent to MPOS or within 330-feet of the Petroglyph National Monument."**

10. **Lighting should be at a minimum** in order to maintain dark sky ordinance. Colors should be earth tone and non reflective to blend with the natural environment.

11. Also, be aware that **BESS (Battery Energy Storage System)** is mentioned in the proposal. **We recommend that that not be near building structures** or equipment as they have been known to be explosive and or catch on fire which take days to put out.

12. Question: **Has there been Tribal engagement on this?** A member from the Laguna pueblo was at the August 5th Council meeting last week to comment during Council's review of the non- aeronautical uses at Double Eagle II indicating that Laguna was not notified on these changes, and would like more tribal engagement on these proposals. **We recommend more engagement with Community and Tribes.**

Sincerely,
Rene' Horvath - WSCONA Land Use Director

Alfredo Salas, Megan Jones, Misa Bloom
c/o Environmental Planning Commission Council Members
City of Albuquerque Planning Department
Plaza del Sol Building
600 Second NW
Albuquerque, NM 87102

**EPC 48 Hour: LTR DEII Re: Double Eagle II Site Plan – EPC application comments;
Project # PR-2018-001577 SI-2024-00994 – Site Plan-EPC**

Dear Environmental Planning Commission,

Thank you for the opportunity to provide feedback on the site plan and design guidelines for the Double Eagle II airport located west of Petroglyph National Monument.

I am writing on behalf of myself. I am the current President of the Westside Coalition of Neighborhood Associations (WSCONA), a member of the ICC Working Group, an elected Commissioner of the Paradise Hills Special Zoning District, and a retired planner and federal consultant. My firm was retained for planning, development, rulemaking and land review issues.

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Please don't approve as currently presented.

Thank you;

Elizabeth Haley M ARCH
6005 Chaparral Circle NW, Albuquerque New Mexico 87114
elizabethkayhaley@gmail.com
505 908 5376

EPC Hearing

August 15, 2024

Item #3 DEII

Jonathan Hollinger: Let's move to agenda item number 3. Who will be presenting for staff?

Catherine Heyne: Good morning. Commissioner, and Chairs, or Chair and Commissioners. My name is Catherine, and I will be presenting this morning.

Jonathan Hollinger: Morning, Ms. Heyne. will you state your name and address for the record?

Catherine Heyne: Yes, my name is Catherine Heyne, and my address is 600 Second Street NW, Albuquerque.

Jonathan Hollinger: Thank you. Will you raise your right hand? Do you swear to tell the truth, under penalty of perjury?

Catherine Heyne: I do.

Jonathan Hollinger: Very well. You have 5 min. Please present.

Catherine Heyne: Alright! Let me share my screen. All right, is everybody seeing that?

Jonathan Hollinger: Yes.

Catherine Heyne: Okay. Alright. Good morning, Mr. Chair. Commissioners and members of the public. My name is Catherine Heyne, and I'm 1 of 2 staff planners assigned to this case. Megan Jones, Principal Planner, also reviewed this case, and, as you know, she's now on maternity leave. This is agenda Item number 3, PR-2018-001577, case number SI-2024-00321. This request is for a Site Plan – EPC for the Double Eagle II Airport. The submission documents, the existing as-built airport facilities and proposed design standards required for the Double Eagle II Airport under the NR-SU zone district, a recorded site plan with design standards will establish a baseline for any proposed future development and are to be negotiated here at the EPC. The site plan shall be in compliance with the associated Master Plan for the airport. The portion you see here outlined in red is not part of the request. As this area is part of either the Eclipse Aviation Campus Site Plan for Subdivision or the Aerospace Technology Park Site Development Plan for Subdivision. The Double Eagle II Airport is an approximately 4,200 acre site, located at 7401 Paso Del Volcan NW, which sits to the northwest of Atrisco Vista Boulevard NW and Shooting Range Access Road intersection. This property is bounded by Major Public Open Space, the City of Rio Rancho and unincorporated Bernalillo County land, and is within a quarter mile west of the Petroglyph National Monument. The subject site is about 8 and a half miles northwest of downtown Albuquerque. The subject site is zoned NR-SU and lies in the Double Eagle II Airport Employment Center, as designated by the Comprehensive Plan and the Double Eagle II Airport Protection Overlay zone, as designated by the Integrated Development Ordinance. About 3 quarters of the surrounding undeveloped land is in an Area of Change, with the airport runways in the southeast and parcels in the far northwest corner of the property in Areas of Consistency. The Double Eagle II Airport is classified as a regional reliever airport for the Albuquerque International Sunport and is owned and operated by the City of Albuquerque Aviation Department. The EPC is

the final decision-making body for this request. As mentioned, the proposed design standards will establish a baseline for any proposed future development and are to be negotiated here at the EPC. After review staff had recommended several changes to the proposed design standards through Conditions of Approval. First, we proposed a 36-foot building and structure height in proposed Condition 6. A building and structure height shall be limited to 36-feet at the DEII, at the Double Eagle II Airport, except for crucial or critical aeronautical support infrastructure structures such as air traffic control towers. The photos on the left show the visual impact of existing structure heights at the Double Eagle near the volcanoes. The photo on the right shows the viewshed with the existing fulfillment center along Atrisco Vista Boulevard to the south of the airport in Bernalillo County. After meeting with the applicant, we discussed additional height options for revised condition for the western and northwestern boundaries within the Major Public Open Space, a setback of 150 feet. That allows, excuse me, I'm going to start that again. No development shall be allowed within 150 feet except view fencing on the perimeter and seating with native grassland mix appropriate for the area between 150 and 250 feet from the western and northwestern boundaries within, or with the Major Public Open Space. The maximum building height shall be 36-feet, as seen in the diagram below for the eastern boundaries along Atrisco Vista Boulevard building and structure setback shall be 150-feet. I'm going to start that one again, too. Excuse me, for the eastern boundary. Along Atrisco Vista Boulevard a setback of 150 feet shall be required. View fencing shall be allowed on the perimeter. Parking lots shall be located at least 100 feet from the eastern perimeter lot line. Landscaping shall be located at least 50 feet from the eastern perimeter lot line except for seeding with native grassland mix appropriate for the area. Between 150 and 250 feet from the lot line of Atrisco Vista Boulevard the maximum building height shall be 48 feet. Any more than 250 feet from any perimeter lot line the maximum building height shall be 65 feet, we are recommending. Its conditions are negotiable. And we- the applicant has suggested a 25 foot setback from Atrisco Vista Boulevard, but we have recommended potential greater setbacks to either 100 feet or 150 feet along this corridor, as proposed in condition 6B. The right-of-way tree requirement along Atrisco Vista shall be removed, and trees shall only be planted in areas with dense development and regular outdoor use or to minimize heat as part of hardscapes. The top photo shows the open views from the corner of Atrisco Vista and Shooting Range access road to the northwest. The chain link fence is the Double Eagle Airport boundary, and the vehicle can be used as scale, and illustrates how anything in front of the volcanoes has a significant impact on the viewshed and character of the landscape. The second photo is an example of trees in the landscape at the Southwest Aeronautics, Mathematics, and Science Academy Building, located within the southern portion of the airport, and that is not part of this request. Proposed condition 6C, Design standards regarding cluster building. We're not in conflict with the FAA safety regulations or aviation functionality design standards regarding clustered buildings or conservation design shade structures and shading over parking shall be added to design standards with agreed upon regulations to promote integrated site design that is considerate to the natural features and topography staff believes in such a place that where possible, and not contrary to FAA regulations, that on-site buildings are grouped on a smaller portion of the total available land, in order to better use land and protect open space shade structures versus trees can allow for better sight lines and offer shade. We proposed condition 8. Development shall not occur east of the right-of-way of Atrisco boulevard, and adjacent to Major Public Open Space, or within 330 feet of the Petroglyph National Monument. However, Staff leaves that as presented. Revisions for Condition 6A, and adjacent to Major Public Open Space, or within 330 feet of the Petroglyph National Monument can be removed. We do have 2 additional updates. One is a condition of approval, and one is a text update in the findings as seen here. Condition 6D, solar hook-up, shall be required for all new development. We recommend removing this and adding this as a finding solar power. Other forms of

EPC Minutes – Item #3 – 8/15/2024

alternative energy shall be encouraged for all new development in finding 15, Policy 5.3.4, conservation development. We found a text error, and we recommend updating it because the addition of fencing impacts view distances. View fencing should retain the feeling of open space and the view shed while helping maintain a built aesthetic. The request is generally consistent with applicable comprehensive plan goals and policies. Proposed conditions of approval, number 6A, and 8, address the goals and policies that the request is not consistent with. According to the office of neighborhood coordination, there are no affected neighborhood or organizations that were required to be notified. All property owners within 100 feet of the subject site were notified as required. Nonetheless, the city aviation department did conduct a public open house on June 28th 2024 to neighbors to provide project context and to solicit feedback on proposed design standards. At that time the primary concerns included light, sound, building height, fencing and screening and sufficient setbacks. Staff received 2 letters prior to staff report posting and answered one phone call that included one response in opposition, one calling for deferral and one concerned with general impacts to the Petroglyph National Monument and Airport functioning. Five letters were received as 48-hour material comments included concerns regarding the proposed building height, allowances of 60 feet to 85 feet, being too tall for the sensitive lands and views of the area and the impact to the Petroglyph National Monument, in particular negative impacts to the environment, the inclusivity of public engagement- particularly input from area or the lack of input from area tribes, increased lighting and hazards of potential added uses. The request meets the review and decision criteria in IDO subsection, 14-16-6,(i)(3), with conditions of approval. It is consistent with the Comp Plan in the NR-SU zone district and related development regulations and complies with the IDO and other city regulations. The proposed development would provide needed infrastructure and public improvements and mitigate significant adverse impacts on the proposed site and the surrounding area. And with that, staff requests a revision to finding number 19, seen here, to fix the IDO citation for the decision criteria so that the request meets Site Plan - EPC review and decision criteria, IDO subsection, 14-16-6(i), and not j, as seen in the current text of the findings. Regarding project number PR-2024-001577, case number SI-2024-00321 staff recommends approval of the Site Plan – EPC, to as built airport facilities and proposes design standards required for the Double Eagle II airport under the NR-SU Zone district per staff findings 1 through 22, and conditions of approval, 1 through 10, with the revisions. And with that, I stand for questions.

Jonathan Hollinger: Thank you, Ms. Heyne, I appreciate your presentation. I have one question that came up during your presentation, and was in regards to the height limitation. Seems like there's a bit of opposition. Was the 36-foot height limit a new addition, and did that come from the facilitated meetings?

Catherine Heyne: We recommended that in the staff report, and then there was the applicant, who thought that would not be sufficient in height, and I'm sure that we'll talk about that in the following presentation. And so, we met with them, and we spoke about other options. And this is what we came to agreement with.

Jonathan Hollinger: Understood. Okay, thanks for that. Commissioners, questions for staff?

Gary Eyster: Eyster.

Jonathan Hollinger: Vice Chair.

Gary Eyster: Thank you, Chair. Thanks for that presentation and for your very good staff report, Ms. Heyne. I'm wanting to establish what our job is today. This is called an as-built Site Plan. But it's also got like, like on page 422 in the Site Plan, it's got these heights of 60 feet or 85 feet, or whatever the revision might be. So that's not as-built. That's not an as-built part. That's almost like, okay, it shows what was built. And it also shows... But it's also a going forward EPC Site Plan, isn't it? And we're talking about those heights today. And we're approving certain heights today, if that's what we do?

Catherine Heyne: That is correct. This is an NR-SU special use zone and design standards, since can be negotiated. And since there was no past site plan for this site, these design standards will be submitted as part of that Site Plan.

Gary Eyster: I understand. Thank you very much.

Jonathan Hollinger: Thank you, Vice Chair. Commissioners, other questions?

Daniel Aragon: Question.

Jonathan Hollinger: Aragon.

Daniel Aragon: Thank you, Chair. Thanks for the presentation, Ms. Heyne. I do have a quick question. On the northern edge of this property, there's a large power line corridor, and those power line structures do exceed the height limitations. Are those exempt?

Catherine Heyne: Those...

Daniel Aragon: That's in the little road looking area. On the top. On the north, just south of the solar facility.

Catherine Heyne: I am familiar with those. I will have to defer to Mikaela or Ms. Renz-Whitmore. Thank you.

Mikaela Renz-Whitmore | CABQ Planning: Thank you, Mr. Chair, Commissioner Aragon. The IDO zoning requirements do not regulate utility poles.

Daniel Aragon: Thank you. Just wanted to make sure we're on the same page.

Jonathan Hollinger: Thank you, Commissioner Aragon. Thank you, Ms. Renz-Whitmore. Commissioners, other questions?

Adrian N. Carver: Mr. Chair.

Jonathan Hollinger: Who is that?

Adrian N. Carver: Carver.

Jonathan Hollinger: Commissioner Carver.

Adrian N. Carver: Thanks, Mr. Chair. I have a couple of quick questions. What is the building allowable building heights in that that red section on the screen, the part that is not included in this request?

Catherine Heyne: I know that the existing building is 42 feet high. I have not seen what those are, and we could look those up.

Adrian N. Carver: Okay, that I think that information might be helpful as we continue to talk about the site. And then on what date was this application filed?

Catherine Heyne: This application would have been filed in July. And maybe the applicants, I don't have that information off the top of my head, but maybe the applicant could help with that.

Adrian N. Carver: Okay. And I guess, so on what date was the tribal notification and engagement consultation amendment to the IDO chapter?

Mikaela Renz-Whitmore | CABQ Planning: I'll jump in, Mr. Chair, Commissioner Carver. This would have been under the 2022 IDO document. So, the tribal engagement didn't kick in until after the deadline for cases this month. So, the tribal engagement requirements that are now in place were not in place when these applications at this hearing were accepted.

Adrian N. Carver: Thank you. That's what I needed.

Jonathan Hollinger: Thank you both. Any other Commissioners? Seeing none, we can move to the applicant presentation.

Manny Manriquez: Greetings everyone. This is Manny Manriquez, Deputy Director for Innovation and Commercial Development. City of Albuquerque's Aviation Department. We have purview over the Sunport and Double Eagle II Airport.

Jonathan Hollinger: So will you state your name and address for the record.

Manny Manriquez: Yes, Manny Manriquez, 5516 Vista Lejana NE, Albuquerque, New Mexico, 87110.

Jonathan Hollinger: Thank you. And will you raise your right hand, and do you swear to tell the truth, on the penalty of perjury?

Manny Manriquez: I do swear to tell the truth, under penalty or perjury.

Jonathan Hollinger: Thank you. Okay, you have 10 min. Please show your presentation.

Manny Manriquez: Thank you, Mr. Chair, so I do not have slides. I just have some remarks. And then our consultant Dekker will present, and they do have some slides. So Double Eagle II development has been planned strategically with very careful pacing, and is intended to support the sustainability of the airport. And this is because, non-aeronautical development of properties surrounding the airfield allows us to generate revenue. and that will help us to support the ongoing operations of Double Eagle II, which at this time must be subsidized by revenues from the Sunport. So, it is not self-sustaining, and the goal is to make it self-sustaining. We have engaged with many of our neighbors. In December our Office of Equity and Inclusion for the City of Albuquerque, sent, actually contacted by email, Sandia, Laguna and Isleta Pueblos to ensure they had the correct contact information of the people who should receive a letter and therefore the letter was sent out by the Office of Equity and Inclusion to seek any comment on the Double Eagle II Master Plan text amendment, which was recently passed by the City Council. We held an open house, a public open forum at Double Eagle II on June 20th. This was intended to discuss the Master Plan text amendment and ensure that members of the public had an opportunity to ask questions, engage and understand the development plans of the Aviation Department for Double Eagle II Airport. We've also had several meetings related to development at Double Eagle II, including with the State Land Office, with our partners at Bernalillo County and the State of New Mexico Economic Development Departments and the West Side Coalition with whom we have discussed this several times. We had a meeting with them at the Sunport on July 23rd. I want to emphasize that our Master Plan text amendment, as well as this Site Plan, are interim steps toward a full update of the Master Plan, which will include a lot more public engagement and the formation of an advisory group. And, so this is really just a necessary step for the allowable uses so that we can move forward with some of our development plans. But they're being shaped and with careful consideration, given proximity to Major Public Open Space and the Petroglyph National Monument. So I just wanna also point out that this development is not happening in a vacuum. The context, the broader sort of economic development context in the region is that in addition to some of the recent developments, the Amazon facility and neighboring facilities in between those facilities and Double Eagle II, the County of Bernalillo, along with Sandoval County, the State of New Mexico, and several regional and interstate partners, with the support of the Federal Government via grant opportunities that were pursued and then awarded by the Department of Transportation's Build America Bureau. They have pursued an I-40 trade port project that is a 6,000 acre development. So they'll be our neighbors, as well as the new cemetery that's being planned for fallen veterans by the VA. So that's the broader context. And it's just very important to us as we move forward that folks understand that there is going to be a transformation on the West Side Double Eagle II, and therefore the Aviation Department have a critical role to play in shaping that development. But it is of paramount importance to us that we continue this public engagement process that we've already started. And so, we pledged to do that. And that will conclude my remarks, and I will stand for questions if there are any.

Jonathan Hollinger: Thank you, Mr. Manriquez. You said that, I believe Mr. Derek would also be sharing presentation. Would that be coming next?

Manny Manriquez: Yes, that would be a staff from the Company Dekker. The architecture design firm, Dekker, does have a presentation.

Jonathan Hollinger: Okay. I guess we can start with questions for you. If there are questions from the Commissioners? I had one for you. You mentioned an I-40 trade port agreement. Can you elaborate on that a little bit?

Manny Manriquez: Yes, so the I-40 trade port is a neighboring project to Double Eagle II. The City of Albuquerque issued a letter of support when the County of Bernalillo, along with Sandoval County and the State, were applying for a raise grant from the Department of Transportation and so we have proclaimed our support for the project, though we are not a member of that consortium. The reason I mentioned that project, and there is information online as well as some news coverage about the I-40 trade port project, is that this is a development project of national significance that will be our neighbor. And in terms of shaping the development of Double Eagle II, we understand that we really need to observe the broader context, work in tandem with our neighbors, and that very much includes all the groups that I mentioned, as well as the tribes and pueblos. So that's just an important point to emphasize given kind of the regional context that exists there.

Jonathan Hollinger: Thank you for that clarification.

Manny Manriquez: You're welcome.

Jonathan Hollinger: Are there other questions? Very well, let's move to the second portion of your presentation.

Manny Manriquez: Very well, thank you very much. I'd like to introduce Mr. Will Gleason and his team from Dekker, and they will take it from here.

Dekker: Good morning, Mr. Chair, Commissioners. Will Gleason, principal urban planner with Dekker, along with Jessica Lawless and Rebecca Shank, and we would like to just walk through a short PowerPoint with you. Can you see the screen that shows Dekker?

Jonathan Hollinger: Mr. Gleason, before we get started, will everyone be presenting, or just yourself?

Dekker: I would swear all of us in just in case.

Jonathan Hollinger: Well, let's go ahead and get that started. Name and address for the record.

Dekker: Will Gleason, Dekker at 7601 Jefferson NE, Albuquerque, New Mexico, 87109.

Jonathan Hollinger: Thank you. And do you all reside at the same address?

Dekker: Yes, we all work at the same address.

Jonathan Hollinger: Okay. Let's just get your other 2 names on the record, and then we'll swear you in.

Dekker: And Jessica Lawless, also with Dekker. Rebecca Shank with Dekker.

Jonathan Hollinger: Well, thank you all. Will you raise your right hand? You swear to tell the truth, under penalty of perjury?

Dekker: I do. I do. Yeah.

Jonathan Hollinger: Okay, thank you for that. Please continue.

Dekker: Thank you. And I know that a lot of this has been covered by Manny and by Catherine, so I will not belabor these. But you know the fact that this is such a large site. And that really, what we're doing today is is documenting existing conditions, and then proposing design standards for aeronautical uses. As Catherine mentioned, we are very aware of our surroundings and that there's different characters of the open space on all sides here. From a shooting range to the Petroglyph National Monument. So, and we appreciate that sensitivity, and that flexibility to have slightly different buffers on different sides of this very large parcel. And I just want to reiterate, because I know this is not the most straightforward approval process, that, as Manny mentioned, this is part of a larger initiative, and that that update to the overall Master Plan is something that that Dekker is not a part of, but that is going to start later this year, and will be a much more comprehensive update. We are just dealing with the aeronautical uses right now. We intend to come back to you this fall with a text amendment with another Site Plan - EPC submittal similar to what we are reviewing today for the non-aeronautical uses. And this is because of that. The timing of when the text amendment for non-aeronautical land uses was approved by City Council. So, it's not the most straightforward process. But this is what we need to do. And so we anticipate coming back to you with another amended set of design standards to address things that are not specific to an airport operation. And I think that's really this. What we're doing today is to meet the condition of the EPC decision in April for the text amendment to come back and say, hey, show us what's really there, and propose some design standards that really can be used for future development. So, moving on to the context, I just want to reiterate what Manny was saying. This is really, even though we are, we have a lot of Major Public Open Space around us. This is an Area of Change. This is an area that's designated as an Employment Center, and that it is accessed by a regional principal arterial. So, we're trying to find that right balance between appropriate development for this area respecting the boundaries, but allowing the development that we anticipate to happen, and to not be hampered by standards that are not appropriate for that site. A lot of our work for this was really documenting the hangers, the tower, all the ancillary facilities associated with the airport, and that is the bulk of the 1st few sheets of the submittal and we really, you know, there were not a lot of records for this. So, we really had to go out and do a lot of this just with our own measurements, and piecing together the pieces that we had. So, we think we have a pretty good document. Now that really shows what's on the ground there, and where parking is, where buildings are, where utilities are and so that is really, that's been the bulk of our work, and that you're approving here. And again, we've documented all the buildings as best we can, signage and really drill down to what we could really measure and document in here, so that as you go, as we go forward and we have amendments to this overall site plan, that we have a good basis for tracking those changes. The main part of this was that in the Master Plan for Double Eagle there is an airport layout plan that served as the Interim Site Plan. But it really does not have very much detail. And it's really more about showing how this complies with FAA standards to be to have setbacks from taxiways. And again, we are not proposing any development in this submittal that you're reviewing here today. The design standards are something that are required for an NR-SU property and the all the proposed design standards applied just to aeronautical uses at this point, and to any

accessory uses. So, this is again something that the City's asked us to do, and it does will not apply to non-aeronautical uses because we're not addressing those here and so that's why we will come back to you, for why we anticipate coming back to you for another round of this and that these design standards, you know, it being an NR-SU, we really are trying to match the design standards to the land uses and the zoning that is as close to what the proposed uses are that we anticipate. And those proposed uses are things that are more of a business part, some very light industrial uses. And so, we're really looking to those zoning categories for the standards that are appropriate for what the overall site these are. This is just an excerpt from our design standards and, as you've probably seen, are about 2 pages. They're not that extensive and this, for instance, is showing what we're proposing for setbacks and for some of the other dimensional standards, such as height. We do agree with what Catherine proposed as an alternative condition in terms of overall maximum heights of 65 feet and 48 feet along within 250 feet along Atrisco Vista, and 36 feet on the western and northern sides of the property. And we'd also like on the parking setbacks, we would really appreciate that 25 foot setback from Atrisco Vista versus a 50 or 100 foot setback in terms of where parking could start. Buildings would be set back a minimum of 150 feet from the western side of Atrisco Vista Boulevard. So, showing this more to show that this is the kind of nature of the design standards here, and just trying to document so it's clear for future decisions what the rules, the ground rules are. I know that Manny went over this and Catherine. So, I don't think I need to belabor that, and again, as I said, we've coordinated the conditions of approval. The ones that Catherine enumerated earlier, we are good with all our conditions, except, I think, 6C in terms of shade structures on parking within the airport because of FAA regulations and the sort of prioritizing airport operations. We feel like shaded parking is great, and we want to have that, especially where there's general parking for the for general aviation, but not require that all over the site. Especially like next to hangars and things like that. It's just not appropriate, or it's not really workable in terms of airport operations. So, we would like to modify that condition. Any others, Jessica? Keep going to the others? No, I think these are all good, right? And then again, just reiterating that you know right now, the airport is operating under FAA rules in terms of height, which is 150 feet. We had proposed 85 feet, because that was what we anticipated could be the upper limit of some of the uses that are anticipated in the future. And really that if we go to 65, we cannot go lower than that in terms of what we anticipate for proposed uses on the site. And I know that the setbacks from the Major Public Open Space are somewhat varied, but we think that we have a good understanding with the City, and that we have clear setbacks in terms of setbacks from Major Public Open Space. Again, back to the height, that we are looking back to the IDO in terms of what are maximum heights and uses that we're, in land uses that we're anticipating in our BP and our LM and our GM. They're all 65 feet, and so we feel like that is, there's good policy support for that maximum height and this is just showing setbacks. We can come back to this, but that we would have 150 foot setback from Atrisco Vista and overall. We would have a protection zone of over 400 feet from Major Public Open Space on the along the east side of the property. And I just want to point out that last graphic does not reflect the amended condition, so that upper height would be, right, that would be 55, and this would be 48, and I know, I think I heard our beep so I think that we will run through this. And again, this is about the covered parking and that we're amenable to putting the shady parking on public facing general parking areas and the solar hookups we like. That's moved to findings. So, we're okay with that. And elevations is a minor point. But we'd like a little forbearance in terms of measuring signs that are on existing buildings where we need to get out really, really tall ladders in order to even figure out what those heights are. And really put that burden on future applicants, rather than trying to get down to the inches in terms of what a particular sign is on the existing campus. With that we'll stand for questions.

Jonathan Hollinger: Thank you, Mr. Gleason. Thank you also for respecting the time. We don't want to push through your presentation by any minute. But I had one question, and it was in regards to the modified condition. I think that you're requesting, I got a little confused, and it's in regards to the shaded parking. Are you asking to remove the shaded parking requirement?

Dekker: Chair Hollinger. Yes, we're asking to remove the shaded parking requirement.

Jonathan Hollinger: Understood. Okay, I just wanna make sure I got that right.

Dekker: Yes.

Jonathan Hollinger: So yeah, thanks for your presentation. Commissioners, questions for the applicant?

Gary Eyster: Eyster.

Jonathan Hollinger: Vice Chair.

Gary Eyster: Thank you Chair. Thank you, Mr. Gleason. You indicated a couple of times that the heights we're looking at today are just around aeronautical uses. Did I get that right?

Dekker: Mr. Chair, Commissioner Eyster, that is correct. We are only dealing with aeronautical uses today.

Gary Eyster: And down the road, the City may be wanting to look at other uses, to lease land, or however they might do it. And at that time those would all be up for their own determination of maximum heights, or how far they would be from Atrisco Vista, or something like that?

Dekker: Mr. Chair, Commissioner Eyster, if a non-aeronautical use came through, we would first need to have updated design standards that address, non-aeronautical uses. And that's what I was referring to in terms of that. We're gonna have another round of this either in September or October. To review the similar set of, or an amended set of design standards that anticipate those uses.

Gary Eyster: Thank you. So for today we have an amended condition. I think that says 65 feet when we're more than 250 feet from a Atrisco Vista and 48 feet between 150 and 250. You talked about lower height standards for the west and the north parts of the site. Why is that? That's not near the Petroglyphs, is it?

Dekker: Mr. Chair, Commissioner Eyster, that is not near the Petroglyphs. The Petroglyphs stop on the east side of Atrisco Vista. There are the major chunks of public open space. And this was an effort to be sensitive to those other portions of Major Public Open Space. And that, given the size of this, you know, most sites that we deal with around the city are an acre or less, and the fact that this is more than 4,000 acres, it makes the kind of scale of this very different.

Gary Eyster: I'm not clear, oh, I'm sorry I thought you were finished. Go ahead.

Dekker: And I, I.

Gary Eyster: Oh, thank you, I'm not clear why we would show special respect for the West or North, when we don't show it for the Petroglyphs.

Dekker: I think you slipped. So, if I may clarify Eyster, so I think that the clarification there, is that we would have lower heights along the eastern edge that faces the Petroglyph National Monument, and then that buffer might allow a little higher height on it, on the west and north.

Gary Eyster: Good. So, I understood Mr. Gleason to say 35 feet on the west and north, but.

Dekker: And excuse me for interrupting Commissioner Eyster, but we are having, that is, the proposition on the table is 36 feet to the west and north and 48 feet along Atrisco.

Gary Eyster: So, I can't understand that, when we have a national monument on the East.

Dekker: Mr. Chair, Commissioner Eyster. We are going above and beyond what is in the IDO right now in terms of setbacks and consideration for adjacency to Petroglyph National Monument and we feel like this is a, that we are going at least 3 times further away from the boundary of a Major Public Open Space before building anything, than is currently permitted with the IDO.

Gary Eyster: I don't want to drag this out, but my question was why the special consideration on the West and the North, more than on the side adjacent to the Petroglyphs?

Dekker: Mr. Chair, Commissioner Eyster, I would actually, this is a condition; these numbers were proposed by City planning yesterday, and so I would...

Gary Eyster: Okay.

Dekker: I would ask them.

Gary Eyster: I will do that. Thank you.

Mikaela Renz-Whitmore | CABQ Planning: Mr. Chair, Commissioner Eyster, the difference is on the west the land is abutting the Major Public Open Space, whereas on the east you've got the border of 155 foot Atrisco Vista. So that abutting versus adjacent rule is what's partly driving that condition.

Gary Eyster: And the same on the north, I gather?

Mikaela Renz-Whitmore | CABQ Planning: Correct.

Gary Eyster: Thank you. Ms. Renz-Whitmore. Thank you. Chair.

Jonathan Hollinger: Thank you, Vice chair. Thank you, Mr. Gleason. Thank you, Ms. Renz-Whitmore. Commissioner's other questions?

Daniel Aragon: Aragon.

Jonathan Hollinger: Commissioner Aragon.

Daniel Aragon: Just kind of a general question, and I've been reading the staff report on this site plan. This is a pretty substantial piece of property on the west side. 4,200 acres is very large to be controlled by, you know the City. And where I'm going towards is I didn't notice anywhere that provides for multi-use trails within this development. Is that something that needs to be spelled out on the site plan, or is that something that can happen? With the amount of open space and the amount of people that use Paseo del Volcan as a kind of a long range, major bike path. There's a lot of bikers that use it. It'd be really nice to see some multi-use trails within this facility to give people – pedestrians - extra places to walk, and bicyclists' safer places to ride.

Dekker: Chair, Mr. Aragon. I fully, I think that's a great idea. I believe that the larger master plan update will be the right kind of venue for creating that framework. There's, I know that there is a bike lane and bike trail, I believe, proposed along with Atrisco Vista. There's a bike lane there now, and maybe Manny wants to elaborate on this question?

Manny Manriquez: Yes. Mr. Chair, Commissioners, thank you very much. Commissioner Aragon, I just want to emphasize that typically, in the immediate vicinity of the airfield there, there are conditions there that are not safe for trails, for bicycles or or pedestrians. However, in the areas further out from the airfield we will sort of look at mixed use access and potential through-ways. But, as Will stated, that's definitely a consideration for the Master plan Update that we'll be engaging in. It's about a year long Process, might take a little bit longer than that, depending on the pace of consultations. But you know, given the special nature and the safety sensitive status of the airport, that's always going to be a, you know, a challenge that we'll be thinking through.

Daniel Aragon: I agree completely. I just wanted to plant the seed.

Manny Manriquez: Thank you, sir.

Jonathan Hollinger: Aragon. Commissioners, any other questions? Seeing none at this point we can move to public comment. Ms. Chavez?

Christina Chavez- Gonzales: Yes, Chair Hollinger and Commissioners. Our first speaker is going to be Bernadette Hardy.

Jonathan Hollinger: Morning, Ms. Hardy, are you with us?

Bernadette Hardy: Oh, good morning, Chair Hollinger and committee. My name is Bernie Hardy. I just have a general public comment. A lot of this stuff I don't really understand.

Jonathan Hollinger: Let's get your full name and address for the record.

Bernadette Hardy: Bernadette Hardy. I'm Jemez Pueblo and Diné. I work for NAEVA. My address is 7900 Menaul SE, 87110. and I put my email in the chat.

Jonathan Hollinger: Thank you. Will you raise your right hand? When do you swear to tell the truth, under penalty of perjury?

Bernadette Hardy: Yes, sir.

Jonathan Hollinger: Thank you. You have 2 minutes. Please proceed.

Bernadette Hardy: Thank you. Hello. Good morning, Committee members and Chair Hollinger. My name is Bernadette Hardy. I'm Jemez Pueblo and Diné. I work with NAEVA. We stand in strong opposition to the Double Eagle II Airport Site Plan for the following reasons: We feel this considered development attempt will jeopardize the natural cultural beauty of the Petroglyphs which is an ancient sacred site to us. But you know, it's a monument to the City of Albuquerque. And it will violate some of the view protections. Not to mention there has not been significant tribal consultation or tribal input. We haven't seen any of the responses they said that they received. I would like to see some of them. And also, the staff report clearly states, the so-called viewshed is important to our tribal communities, but design fails to comply with requirements to protect that viewshed. We just ask that EPC rejects the plan and move forward with a comprehensive tribal input and community input. I also don't see any of the small aircraft pilots here. Or, how it would affect their usage of the space, too, if it becomes a commercial place. So just putting that out there. Thank you for your time. I appreciate it.

Jonathan Hollinger: Thank you for your comments this morning, Ms. Hardy. Mr. Gleason, would you like to address the tribal input?

Dekker: Mr. Chair, I would actually defer to Manny on that question.

Jonathan Hollinger: Very well. Mr. Enriquez, would you like to address the tribal input?

Manny Manriquez: Yes, Mr. Chair. Thank you very much. Ms. Hardy, thanks for your comments. Yes, so we do plan to engage in a much more considerable engagement with the tribes and pueblos moving forward, especially when we began our master plan update. But, for the time being, our Office of Equity and Inclusion followed the rules and contacted the local pueblo representatives. And the FAA did as well, when we were applying for our release for non-aeronautical use of one of the properties as well as the environmental review. That said, I just want to be sensitive to Ms. Hardy's remark about building heights, because, that is very important to us. And so, we've heard the public and we've actually discussed this at considerable length with our tenants out at Double Eagle II in various forums. We actually have meetings with our tenants out there, roughly quarterly. And so, we've had about 3 or 4 of those sessions over the last year. So, they've had a lot of time to give input in this initial phase. And as far as the building heights and protecting the viewshed specifically, this is the reason why we're going well below the maximum allowable heights that FAA allows. And so, this is really in consideration of a lot of the buildings and structures that are already in the interior of the property of Double Eagle II. EPC Minutes – Item #3 – 8/15/2024

Eagle II, and not to exceed some of those heights, especially for hangar facilities. And so, from Atrisco Vista Boulevard, and from the National Petroglyphs of that viewshed will be protected given the heights that we're observing today.

Dekker: And Chair, if I could just add to that. The design standards do have some very specific measures to mitigate the impact to Petroglyph National Monument. We have specific colors that buildings need to be. That was the result of consultation with the National Park Service. We have screening requirements that are reflective of those conversations as well. The IDO does not have specific language about what to do around Petroglyph National Monument, and none of it is about height. Other than to say that if you are... The only viewshed protection overlay is really looking East and looking toward the Sandias. It is not anticipated to be trying to make the whole western horizon not have any sort of building structures on it. And so we, I believe, that we're doing our ... we have a good faith effort to minimize the impact. But I believe that it is a big change in City policy to say that nothing is going to be on the western horizon in terms of building development.

Jonathan Hollinger: Well, thank you, Mr. Gleason. Mr. Manriquez, in regards to the tribal engagement, you said that you plan to communicate with them more. How will you do that? Just to ensure that in regards to Ms. Hardy's comments, to saying that you didn't receive that information, what will be your plan moving forward?

Manny Manriquez: Yes, thank you, Mr. Chair, for that question. So, we have retained a special advisor by the name of April Hale. This is to really supplement the capabilities that the City already has in place through the Office of Equity and Inclusion. Ms. Hale is Diné. She's a member of the Diné tribe. But she also has very good relationships with the Indian Pueblo Cultural Center and our various Pueblo neighbors. And so, we are going to be working with her and the Office of Equity and Inclusion to ensure that the reach of our consultations is even broader than it has already started to be. And so, that's really very intentional, because we do want to ensure that we're being good neighbors and that we're doing those consultations really above and beyond what's required by statute.

Jonathan Hollinger: Okay. Thank you for that. Clarification Commissioners? Any additional comments or questions at this point from Ms. Hardy's comments?

Adrian N. Carver: Mr. Chair, I have a point of order. This is Carver.

Jonathan Hollinger: Mr. Carver.

Adrian N. Carver: Hi, yeah, I understand that we are on the public comment portion of this agenda item, I think it's appropriate to, for us to get through all of the public comment and allow staff and applicants to respond in their next part of the agenda. I'm not sure that public comment is intended to be a back and forth like this.

Jonathan Hollinger: I hear you, Commissioner Carver. It was mentioned during some of our conversations that it could be an effort to minimize cross examination, so to just try and address some of these questions so they don't get lost. That was the intent. But thank you for that. Being said, Ms. Chavez, who signed up next?

Christina Chavez- Gonzales: Yes, Chair Hollinger. Our next speaker, is going to be Alysia Coriz.

Jonathan Hollinger: Good morning.

Alysia Coriz: Good morning. Greetings.

Jonathan Hollinger: Address for the record. Please.

Alysia Coriz: Yes, Alicia, Coriz and address is 7906 Menaul Boulevard NE Albuquerque, New Mexico, 87110.

Jonathan Hollinger: Thank you, and will you raise your right hand, and you swear to tell the truth, under penalty of perjury?

Alysia Coriz: I do.

Jonathan Hollinger: Thank you. You have 2 min.

Alysia Coriz: (Native language intro). My name is Alysia Coriz. My organizational affiliation is with NAEVA. I wanted to, you know, kind of recap some of the updates, and as well as provide some you know, inquiries into tribal consultation as well. So, at the City Council meeting, when this proposal was presented, my colleague had asked about tribal consultation efforts that were made as the proposed area impacts the Petroglyphs, which are sacred sites to many Pueblo nations. The answer that was received in regards to tribal consultation was that outreach was made to Laguna Development Corporation in December. While this effort was made, this does not constitute adequate tribal leadership consultation within our organizing efforts. We are actually working to follow up with Laguna Development Corporation for their feedback on this conversation. Being that Laguna Development Corporation is an entity within the Pueblo of Laguna, it does not constitute the same, as you know, meeting with the tribal leadership, the Governors and tribal council. And so recently our organization had held a conversation with Pueblo Laguna, Governor Wilfred Herrera, Jr. regarding this issue, and he stated that he was personally not consulted. Therefore, this issue held no opportunity for meaningful tribal consultation with the public nations to be informed about this impact to the sacred sites and cultural landscape. And with today serving as a feast day for the Pueblo of Laguna, Governor Herrera and his staff are unable to join us. This morning however, it has been the request of Governor Herrera to pause on any future decisions until meaningful tribal consultation can be conducted with not only the Pueblo of Laguna, but with all tribal leadership of the public nations that this area impacts. Thank you. I yield my time.

Jonathan Hollinger: Thank you for your comments this morning. Ms. Chavez, who's next?

Christina Chavez- Gonzales: Chair Hollinger, Commissioners. Our next speaker is going to be Elizabeth Haley.

Jonathan Hollinger: Morning, Ms. Haley.

Elizabeth Haley: Good morning.

Jonathan Hollinger: Please say your name and address.

Elizabeth Haley: My name is Elizabeth Haley. My address is 6005 Chaparral Circle NW 87114.

Jonathan Hollinger: Thank you. Do you swear to tell the truth on a penalty, perjury?

Elizabeth Haley: I do? I do?

Jonathan Hollinger: You have 2 min.

Elizabeth Haley: I represent WSCONA, the West Side Coalition of Neighborhood Associations. We had a vote last night to... with 18 of our Member Association represented in that vote, and we voted to inform the EPC that we do not support this proposal as it's currently written. We feel that it is an inverted process that a Master Plan and master planning components and comment should have been done first, because some of the areas presented in this proposal are not as-built areas or under previous master plans. They are whole new areas that haven't been subject to a master planning document that can be vetted and reviewed. An oral report at the EPC doesn't take the place of a master planning document. We think the City and its planners should adhere to Federal Aviation Administration standards, and environmental studies, and the requirements for sites that could be affected. That includes conducting the kind of feasibility and environmental studies that, if bypassed, could make receiving future federal infrastructure funding extremely problematic. It should have observed the viewshed by maintaining building heights to a maximum of 36 feet, with the exception of airport control towers. Also complying with staff suggestions for natural New Mexico colors and native plants. Development should not be permitted east of the right-of-way of Atrisco Vista Boulevard adjacent to public major open space, and within 330 feet of the National Monument. Significant tribal and community engagement processes should be adhered to. Access traffic and adequate part of parking study should be done, and the EPC should approve design guidelines only after the applicants qualifies the additional need for water, sewer and other in infrastructure and estimates the actual cost of the proposed development, and has conducted a regional impact study and analysis of the proposed development. Thank you.

Jonathan Hollinger: Thank you for your time and your comments this morning. Ms. Chavez.

Christina Chavez- Gonzales: Chair Hollinger, our next speaker, is going to be Lan Sena.

Jonathan Hollinger: Mr. Sena.

Lan Sena | CCP | Policy Director: Thank you, Mr. Chair. My name is Lan Sena, policy director for the Center of Civic Policy. My address is PO Box 27616, Albuquerque, New Mexico, 87125.

Jonathan Hollinger: Thank you. And do you raise your right hand and do you swear to tell the truth, under penalty of perjury?

Lan Sena | CCP | Policy Director: Yes.

Jonathan Hollinger: Thank you. Can I ask? Are you a Councilor?

Lan Sena | CCP | Policy Director: I was the former City Councilor for District 1. Yes.

Jonathan Hollinger: Okay. Just wanted to make sure. I was gonna consult Legal, but not a problem. Good morning. You have 2 min.

Lan Sena | CCP | Policy Director: Thank you, Mr. Chair, and thank you, Commissioners. We support the comments of NAEVA, Ms. Hardy and Coriz and the findings of the City of Albuquerque Planning Division. For this application, the location of Double Eagle sits within an area that is culturally significant and environmentally sensitive. The land is adjacent to Major Public Open Space, and it is adjacent to secret sites. The Petroglyph National Monument that is named specifically within the Comprehensive Plan, which calls for regulation of adjacent development to protect and preserve the site. Protection of our environment and preserving our cultural heritage are core to New Mexico values. The comprehensive plan embeds these values in public policy, although the plan names broad categories for land uses on the West Side and allows opportunities for economic development. What's missing is an overall vision for the kind of economic development that is appropriate and desirable for that region. We welcome any opportunity for new, good quality jobs. But given the environmental and cultural importance of this area, we should consider how we can encourage minimally impactful uses and urge development that can integrate into the natural environment. I urge you to deny the site app... plan application, and also urge the City of Albuquerque to consider developing a process to solicit public input on the types of economic development we want to see on the West Side. That would be appropriate for this area. Thank you.

Jonathan Hollinger: Thank you for your time this morning. Ms. Chavez.

Christina Chavez- Gonzales: Chair Hollinger, our next speaker is going to be Renée Horvath.

Jonathan Hollinger: Ms. Horvath.

Renée Horvath: Good morning!

Jonathan Hollinger: Morning. Start with the name and address.

Renée Horvath: My name is Renée Horvath. I live at 5515 Palomino Drive in Taylor Ranch, and I'm with the West Side Coalition, so I would like... we did have a meeting, as Elizabeth feely said, so I'd like 5 min to speak at least.

Jonathan Hollinger: Well, will you raise your right hand? You swear to tell the truth, and a penalty, penalty of perjury?

Renée Horvath: Yes.

Jonathan Hollinger: You have 5 min.

Renée Horvath: Okay, thank you. Yeah. I just want to kind of... I've been involved with the Petroglyph National Monument since its creation starting in the eighties, and that was a lot of work to create that. And then over the years, we have people from our area that volunteer at Petroglyph Monument. And this is a very important site, not just for us, but for all of Albuquerque. So we need to get this right. And so there was a lot of concern. We did not get notified initially about these changes on the non-aeronautical changes, but we heard about it through Pet... from the National Park Service. and so now we got involved. We've had a few meetings with them, like an open house meeting, but they didn't... We expressed the building heights are very critical, and that the uses don't conflict with the aviation or Petroglyph Monument. And I want to say that the volcanoes are very... The viewshed of this area is very important, not just for the visitors, but for the Native Americans that have always expressed that this is centrally located between the mountain ranges, and therefore it's sacred. And so the viewshed is critical. So when we met with the Aviation people, we told them we don't want another Amazon to stick up so that everybody in the city can see this building behind 9 Mile Hill. We don't want that kind of thing behind the volcanoes, and... but even along Atrisco Vista and looking from the volcanoes looking west, we don't need those tall buildings. Currently the existing buildings that are there now are 36 feet or less, except for the air control tower. We are always told that that at every meeting, well, the last meeting we had with Aviation, they said, if the buildings would be 30 to 36 feet, and then all of a sudden, we only got notified by the Park Service again, like, Hey, did you know there's going to be an August 15th EPC Hearing? And so I was like, Oh, really! And that the building heights are now 60 to 85 feet. So I, that is, that was not the impression they gave us. And so the existing buildings are like around 30, no higher than 36 feet off in the distance. These will be closer to the road. They will have more impact on the area. There's also a lot of other details that I put in my letter that need to be addressed. And so we.. I don't... We should not move forward, because now what we heard initially, is not what we're getting. And now we're really concerned; we don't really trust what's happening. There was a lot of concern about the non-aeronautical uses being proposed, and that just got approved by City Council, and even at that meeting there was someone from Laguna says, Hey, we're not, we were not notified. So now, a week and a half later, we're being here before you with the design standards, and they have not sat down with us to discuss these design standards. They just want to move... approved, and say later, we'll work with you. No, the design standards are entitlements, and we have no negotiation with them once it's entitled. And even if the IDO allows, this my understanding, this is non-residential special use. And so the building heights are negotiable and so we need to not approve this at this meeting, that this needs to move... work with the communities, the neighborhoods, the tribes, and Petroglyph National Monument to look at, What exactly are you proposing? Because now I'm getting concerned about, what are you proposing? What is the building layout? What is.... What is the appropriate heights? And we still feel like 36 feet is appropriate and the landscaping and the access. There's a lot of details that need to be discussed to get this right, because right now we got Petroglyph Monument. We got aviation. Currently, they tend to work pretty well together without impacts. But now we're filling in the spaces, and it's critical to get this right, or we will have impacts that we'll have to deal with down the road. And even Elizabeth Haley says this could affect funding federal funding to help with some of these efforts, so I would say, don't approve it today, and insist that they meet with all stakeholders to discuss this design standards get this right so that we all agree that we got it figured out, and then come back to you for approval. Well, those are my comments.

Jonathan Hollinger: Thanks for your time.

Renée Horvath: Thank you.

Jonathan Hollinger: Ms. Chavez.

Christina Chavez- Gonzales: Yes, Chair Hollinger, our next speaker is going to be Jane Baechle.

Jonathan Hollinger: Ms. Baechle!

Jane Baechle SFVNA: Thank you. I am speaking as the representative of the Santa Fe Village Neighborhood Association Board, so, like Ms. Horvath, I would like to be allowed 5 min, and I can assure you that this position has been reached in compliance with the NARO requirements of neighborhood associations.

Jonathan Hollinger: Let's start with your name and address.

Jane Baechle SFVNA: It's Jane Baechle, and the address is 7021 Lamar Avenue NW.

Jonathan Hollinger: Thank you. And will you raise your right hand? Do you swear to tell the truth, under penalty of perjury?

Jane Baechle SFVNA: I do.

Jonathan Hollinger: Is your association similar to Ms. Horvath's?

Jane Baechle SFVNA: We have had... This issue has been discussed, our position at multiple meetings, and subsequent to receiving notice of this meeting. I've had individual written and or in-person communications with every member of the board endorsing our position.

Jonathan Hollinger: Okay, let's go ahead and give you the 5 min. Please proceed.

Jane Baechle SFVNA: Thank you. So for the Santa Fe Village Neighborhood Association, our fundamental and unchanging position at every step in this process, beginning in April and continuing since that time has been that any development of any use at any time in this area must protect the natural and cultural landscape of the entire area as it's defined in the Comp Plan. And Ms. Sena referenced the Comp Plan Policy 11.3.4 regarding the Petroglyph National Monument, which includes in its language, in a sub-policy that it is intended to protect the monument and surrounding lands. The proposed height, building heights, and setbacks as they were presented by Planning staff today will absolutely fail to protect either the views or the cultural landscape that is associated with the Petroglyph National Monument. And this particular area, the views shown are going to be in very close proximity to the 5 cinder cones which represent the significant cultural heritage of this area. In our written comments we have cited multiple other Comp Plan goals and policy statements, which we believe are essential and reinforce the expectation, the intent of the Comp Plan to protect heritage landscapes, and specifically the Petroglyph National Monument. The language that calls this as-built Site Plan, and then goes on to establish height standards which did not exist to me is fundamentally contradictory. There is something

inherently at odds about saying you are documenting as an existing site, and then also introducing new standards. I participated at every step of the way. I spoke to you all in April, in opposition to the proposal, I attended the open house. I attended the WSCONA meeting with the Aviation Department. I absolutely appreciate those opportunities to engage, but they have not changed our fundamental view that building heights must be sufficiently low to protect the entire landscape, and that a building height on the neighborhood of 35 to 36 feet is consistent with that. So we respectfully request that the EPC respect the views of the National Park Service and members of the community, and ask that you ensure compliance in this matter with the intent of the Comp Plan. If that means revisiting this at another time, certainly after much more public engagement, we would certainly support that, and thank you for your time and attention.

Jonathan Hollinger: Thank you, Ms. Baechle. We appreciate your comments are heard. Vice Chair, would you be able to swear in the next Speaker? Chair needs just a 1 second recess.

Gary Eyster: I'd be happy to, Chair. Ms. Chavez, who is the next speaker?

Christina Chavez- Gonzales: Vice Chair Eyster, the next speaker is going to be Nancy Hendricks.

Gary Eyster: Hello, Ms. Hendricks, would you please state your name and your address?

Nancy Hendricks, Petroglyph National Monument: Nancy Hendricks. 6001 Unser Boulevard NW, Albuquerque, 87120. And that's my work address.

Gary Eyster: Thank you. Would you please raise your right hand? Do you swear to tell the truth, under penalty of perjury?

Nancy Hendricks, Petroglyph National Monument: I do.

Gary Eyster: Will 2 min be sufficient?

Nancy Hendricks, Petroglyph National Monument: It should be. Thank you.

Gary Eyster: If you need a little more, we'll try and do it.

Nancy Hendricks, Petroglyph National Monument: Thank you very much. And good morning, Mr. Vice Chair Eyster. Mr. Chair is out of the building, and the Commissioners, I appreciate the opportunity to speak with you. Today I'm speaking on behalf of the National Park Service at Petroglyph National Monument, and I was able to submit written comments last week. I appreciate the work that Double Eagle has done to involve us in this process and respond to our input on the design criteria. Frankly, I'm still somewhat confused about this process for the Master Plan update. I'm a little confused why the site design plan is being considered prior to the Master Plan revision. I would love that to be explained better to me at some point in time. And while they have addressed many of our concerns, we are, we continue to be concerned about the proposed building heights that extend up to 85 feet, and also the lack of dimensional standards proposed to be authorized by the Site Plan. We met with Double Eagle representatives, first at Petroglyph in April, and we reiterated our concerns about building heights

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at that time, and we were assured that the maximum building heights would be around 3 stories. So I felt like we were moving forward in a very positive direction when we received the site plan with the 85 foot tall building heights. We were pretty surprised by that, and even if amended to 60 feet, I think they would affect the views from the Petroglyph in particular, the volcanoes which are considered sacred by area Pueblo people, as we heard from previous speakers, and I've... as I've heard, personally working with Pueblo people at Petroglyph. The cultural landscape that is considered sacred extends from the volcanoes east of the Sandias, but it also extends west to the Rio Puerco and over to Mount Taylor. And these views are particularly important to the area Pueblo people. Well, I can't speak for the Pueblo people. I can say that this week is an important week for the Pueblo Laguna and the Pueblo Zia. They're both holding feast days today. And so if they're not present today, that is the reason why I would ask that you deny the Site Plan until we can better understand the impacts on the Petroglyph. I request a visual representation of what buildings at these heights would look like from the Petroglyph area, including from the trails at the volcanoes, and I request that you please defer this decision until a time that tribal representatives can be in attendance and consulted on this proposal. Thank you.

Gary Eyster: Thank you. Ms. Hendricks, is that enough time?

Nancy Hendricks, Petroglyph National Monument: Yes, thank you.

Gary Eyster: All right. Thank you very much for participating today. Ms. Chavez, who's our next public speaker?

Christina Chavez- Gonzales: Vice Chair Eyster, our next speaker is going to be Mike Voorhees.

Gary Eyster: Thank you, Mr. Voorhees. Welcome, please state your full name and address.

Mike Voorhees: Thank you. Vice chair, my name is Michael Voorhees. I live at 6320 Camino Alto NW in Albuquerque.

Gary Eyster: Thank you. Would you please raise your right hand? Do you swear to tell the truth, under penalty of perjury?

Mike Voorhees: I do.

Gary Eyster: Good are you speaking on your own behalf? Is 2 min sufficient?

Mike Voorhees: I am a member of the WSCONA Executive Committee, but I'm speaking on ... as an individual today. So 2 is fine.

Gary Eyster: Thank you, and we'll see, if you need a little more, we'll see if we can do it.

Mike Voorhees: Thank you. So I'm a commercially rated pilot with flight instructor privileges. I'm also an aerospace engineer, and I am confused as to why the aeronautical uses for the buildings at Double Eagle II need to be higher than 36 feet. I appreciated the limited engagement we've had with the division of the airport, the Aviation Division of the City where they assured us that 36 feet was really all that was needed. I know that

there are no FAA prohibitions at having a lower maximum height of 36 feet. So it seems like there's some sort of bait and switch going on that issue. And it's puzzling to me. I also want to point out to them the recently adopted Comprehensive Plan Policy 4.2.2. It's community engagement. It states facilitate meaningful engagement opportunities and respectful interactions in order to identify and address the needs of all residents. This whole process seems to be piecemeal. The notifications on the Pueblos others have pointed out has been limited. It certainly hasn't extended to the 29 pueblos and tribes that have real interest in the cultural viewshed out here. The neighborhood engagement has started to improve, but it's still quite limited. And the whole intent of the National Environmental Policy Act really asks for environmental analysis to be conducted before these kinds of changes are made. And since an update to the Double Eagle II Master Plan is going forward in the next year, we're told it really would be appropriate to engage in full studies and full community engagement before making piecemeal modifications to the Master Plan, so I'd ask that you at least stick to the 36 feet, but it would be better to defer this until all interested parties are fully engaged, and those concerns are addressed in a comprehensive way. Thank you very much.

Gary Eyster: Thank you, Mr. Voorhees, for being involved today, and you did raise a couple of questions in my mind which I will pose to the applicant. Thank you, Ms. Chavez, Chair... Or would you like control of the meeting back?

Jonathan Hollinger: Sure I just needed one second recess. Thank you for swearing in but yeah, I'll go ahead and take over again. So, Ms. Chavez. Thank you.

Christina Chavez- Gonzales: Chair Hollinger and Commissioners, our next speaker is going to be Kenneth Bobroff.

Jonathan Hollinger: Are you with us? Can't hear you if you're there. There, you! I see you.

Kip Bobroff: Good afternoon. My name is Kenneth Bobroff. I'm the acting government affairs director and in-house attorney for the Pueblo of Laguna.

Jonathan Hollinger: Will you state your name and address for the record? Oh, sorry I won't ask you to raise your right hand since you're driving. Do you swear to ...

Kip Bobroff: I pulled over.

Gary Eyster: Chairman. Very well.

Jonathan Hollinger: Okay, you have 2 min. Go ahead.

Kip Bobroff: So first of all, I'm speaking on behalf of the Pueblo Laguna, so I would expect the chair to grant additional time as necessary, based on that respect that we would we would ask for from the Commission. But I'll speak as quickly as I can. I recognize that after the fanfare that announced the City's commitment to consult with tribes and pueblos that we haven't quite worked out all the mechanics. But I would, on behalf of the Pueblo, I would ask that the EPC not act today. There has been very little consultation with the... certainly the Pueblo

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Laguna that I'm aware of. I did receive a last-minute notice of the meeting out at the airport through an unofficial channel. And I attended that and am, frankly, a little surprised that given this is moving forward, that we haven't received any attempt that I'm aware of to explain what is being done. What is being proposed? And I'll associate the Pueblo with the remarks of the superintendent. We appreciate the efforts of the Monument to respect and accommodate and support the religious and spiritual practices of the Pueblo cultural practices that are connected to the sacred lands upon which the monument and surrounding areas touch. The Pueblo also wishes to express its appreciation for the solidarity of the Neighborhood Associations and the efforts of the Native American Voters Alliance. All of those allies to make sure that the Pueblo people continue to have the ability to practice their culture and religion. You know, as regards to the area around the around the petroglyphs. So we look forward to establishing better processes to provide the kind of government-to-government consultations that are appropriate and to make sure that decisions are made with after those consultations, and not before. Those consultations have a tie, a chance to take effect. I appreciate the extra time. Mr. Chairman, thank you.

Jonathan Hollinger: Absolutely drive safe. Thank you for your comments this morning. Ms. Chavez, who's next?

Christina Chavez- Gonzales: Yes, Chair and Commissioners. Our next speaker is going to be Terry from the City of Albuquerque OEI.

Jonathan Hollinger: Hello. Are you with us?

Terry Sloan, CABQ OEI: Good morning, Mr. Chair.

Jonathan Hollinger: Good morning. Will you state your name and address for the record? Please.

Terry Sloan, CABQ OEI: My name is Terry Sloan, and I'm with the City of Albuquerque and the City's intergovernmental tribal liaison.

Jonathan Hollinger: And your address.

Terry Sloan, CABQ OEI: 5704 Bartonwood Place NE, Albuquerque, New Mexico, 87110.

Jonathan Hollinger: Thank you. Will you raise your right hand, and do you swear to tell the truth, under penalty of perjury?

Terry Sloan, CABQ OEI: Yes, sir.

Jonathan Hollinger: Thank you very much. Please proceed.

Terry Sloan, CABQ OEI: Thank you. I wanted to give an update on what tribal consultation was made on this project for the Mesa Studios. On December 13th 2023, I called the Pueblo of Laguna's Governor's office at 10:50 AM. I did not get the name of the person I spoke to, but in apprising them of why I was calling on this particular project, I was advised to send an email to Ms. Diane Kai, who's in in the Executive Secretary's office, and I was all surprised at that time. Unfortunately, Governor Herrera was off on for medical leave, and I was asked not to

send him an email also, because he was... they would absolutely respect him, his privacy and time to be away. And so that was sent, then, on December 14th, 2023 to Diane Kai, of the Executive Secretary's office. And at that time, as you all know, there was no tribal notification or engagement required by the City of Albuquerque. In my ask... I was asked by the Aviation to assist with this notification. And so they were actually attempting free prior informed consent to the tribe before it was required by the City. Now because of the IDO change, the tribal notification engagement is required, and that was in place and active on Octo... August 3rd, 2024. But, as far as I know, that was the only communication, because we did not hear any feedback from the Pueblo Laguna. It is also my understanding, too, that the Federal Aviation Administration has not received any feedback, too, from the Pueblo of Laguna on this project, and that is speaking with Christina Sandoval of the Aviation Department. And so that is the extent that we have communicated with the Pablo Laguna, and we have not heard back from them. At this point I have called the Governor's office and the tribe last week and have not received the call back. And I did call them this morning and have, and spoke with somebody, and asked that they could please give me a call back. So I'm expecting hopefully a call back. Otherwise I'll actually speak with the Governor directly myself on this issue. Thank you.

Jonathan Hollinger: Thank you for your time this morning. We appreciate you. Ms. Chavez, is anyone else signed up?

Christina Chavez- Gonzales: Chair Hollinger, at this time we have no further public comment. If you care to sign up to speak for this particular matter, please say so now. No further comments, Chair.

Jonathan Hollinger: Thank you, Ms. Chavez. Let's move to Staff closing.

Manny Manriquez: Mr. Chair, if I may. Am I allowed to respond to some of the comments?

Jonathan Hollinger: Absolutely. I assume we would address that under your closing for a little guidance from Commissioners. Does that seem appropriate?

Matt Myers: This is Matt Myers. Yes, and certainly the applicant will have an opportunity to speak after the staff gives their closing, and I think that'd be a good time for them to address some of the questions that have come up at that time.

Jonathan Hollinger: Thank you, Mr. Myers.

Manny Manriquez: Commissioners, appreciate that.

Jonathan Hollinger: So let's move to Staff closing.

Catherine Heyne: Alright. Thank you. Chair Hollinger and Commissioners. I just wanted to reiterate that this facility is located adjacent to Major Public Open Space and in close proximity to the major not... or the Petroglyph National Monument that protects a variety of cultural and natural resources, including both volcanic cones and various cultural sites, wide open vistas are part of this heritage. It is important that a healthy balance can be reached between the proposal development that will provide needed infrastructure and public improvements and

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the mitigation of developmental impacts at the site and the surrounding area. Thank you. And with that I yield my time.

Jonathan Hollinger: Thank you. Ms. Heyne. Commissioners, questions for staff at this point?

Gary Eyster: Eyster.

Adrian N. Carver: Go ahead, Gary.

Jonathan Hollinger: Let's go with Commissioner Carver.

Adrian N. Carver: Thank you, Mr. Chair. I am curious. Ms. Heyne, if you could. There was some questions about process and how we got here. Yeah, sounds like this is a little atypical. Could you help me understand how we got here again?

Catherine Heyne: I'm sorry. Could you repeat the question? I didn't quite understand.

Adrian N. Carver: Yeah, thanks. So I I'm trying to figure out how... what process we got here. Why... there was some questions from the public about why we're doing a Site Plan prior to the Master Plan. And just some questions about that. I'm wondering if you could respond from the staff perspective. How we got to this point.

Catherine Heyne: What I can... thank you, Commissioner Carver, for that question. As I understand the site, there was no site plan for the airport, the Double Eagle Airport, and in order to move forward with any future building, they will need to have a Site Plan in place. And as part of that Site Plan within the NR-SU district or zone district, it's required to also put forth design standards, and anything that is not within those design standards will default back to IDO standards. And however, since the SU zone district specifically states that those need to be determined, those need to be determined with that Site Plan. And that is what we're here to see for today.

Jonathan Hollinger: Mr. Carver, does that clarify your question?

Adrian N. Carver: I think, for the time being, yes.

Jonathan Hollinger: Thank you. Vice Chair. Did you have a question as well?

Gary Eyster: Yes, thank you, Chair. Ms. Heyne, if I... I may have missed this in the staff report. It was a very long staff report. But is there is there a drawing anywhere, a graph anywhere that shows the view as you look from the... a point in the Petroglyphs to the west. And whether you would see a 36, or 48, or 65-foot building at the Double Eagle Airport.

Catherine Heyne: Thank you for that question, Commissioner Eyster. I have in the presentation several overviews, site overview slides. I can turn back to those. I'm not sure if they will answer your question, but we can look at what I have presented.

Gary Eyster: That's worth a try. Thank you. While you're getting those, I would say to my fellow commissioners, I don't fully agree with the way that applicant characterized some of these discussions, that the view from the petroglyphs is only important to the East. It seems to me like the view from the Petroglyph Monument is important to the West as well. I know that I've been up to Aztec National Monument, and you drive through some of the really roughest little parts of town, and that takes away a heck of a lot from a monument. So which one of those would we look at, Ms. Heyne?

Catherine Heyne: This is from a Atrisco Vista Boulevard standing just in right at the entrance sign, and this is looking west.

Gary Eyster: And that's looking at as-built.

Catherine Heyne: That is looking at the as-built, and I have a couple of backgrounds here. Just let's see if I can get to them. You can see this is standing over by Tramway, looking west towards the... It was a cloudy day, but you can see the volcanoes here, and in the background. Mount Taylor. See!

Gary Eyster: So what I'm really.. what I'm really thinking of is, if I'm somewhere on the edge of the petroglyphs, what am I going to see up at the airport? How high up will I see? That would seem to me to be...

Catherine Heyne: Standing on the ground, so.

Gary Eyster: Yeah, but I don't know what how high those are. I get the impression that they are quite a ways off in the distance.

Catherine Heyne: Yeah, these are views to the East. So I did not get anything that's from the tops of the volcanoes, or from even the trails at the volcanoes.

Gary Eyster: Or from the monument.

Catherine Heyne: For the yeah, the monument.

Gary Eyster: Okay, well, those are helpful. They don't fully address my question. My question would be fairly simply done with some with a contour a topographic map that says I'm down here at the Monument, I'm looking west, and here's, you know, a profile is what I'm talking about, and I can't expect anyone to do that in the next hour, but that will sure help me in in my determination. Without that it's hard for me to say that this is a good idea. Thank you.

Jonathan Hollinger: Thank you, Vice Chair. Other Commissioners, questions for staff? Alright, seeing none. We can move to the applicants closing. Is that you?

Manny Manriquez: Commissioners.

Jonathan Hollinger: I guess.

Manny Manriquez: Yes, sir, thank you very much, Mr. Chair. I appreciate the opportunity to speak once more, and really just address some of the concerns and correct the record on a few points. I think Mr. Sloan already made it clear that his outreach was to representatives of the Laguna Pueblo, not the Laguna Development Corporation. I just want to note the reason why that came up is because I provided testimony at a City Council hearing on August 5th and it was about 1 am in the morning at that time. So at that time I was addressing a question from one of the counselors, and I said our 1st point of contact was the Laguna Development Corporation, because that was the one we'd spoken to an individual from that several months prior on a totally separate project, and that was the 1 point of contact I had for the FAA. So that was the starting point for the FAA. That was not the end point, and the FAA did a different set of outreach when they were conducting their NEPA review, their environmental review. We can gather more details about that for the future, if helpful. But that said, I would just want to emphasize that is a totally different process than Mr. Sloan engaged in in his outreach efforts with Laguna, the Laguna Pueblo. So just to clarify... also want to clarify a remark about FAA and adhering to their guidelines. We work very closely with the FAA, and the process whereby FAA conducts an environmental review is that when we have a concrete project, we then submit information about the nature of the project and the facilities to be built for FAA review at that time. They then conduct their environmental analysis. So that was done for the Mesa Film Studios Project. But FAA cannot review projects that don't exist yet. So really, the Master Plan text amendment update was just to allow for more allowable uses so that we could engage with entities that would be interested in developing at Double Eagle II. I want to also emphasize, there was a remark about encouraging minimally impactful uses and soliciting public input, so that again would be part of our Master Plan update. Because, and this is really important because we don't have other developments that we're currently working on other than Mesa Film Studios, so in order to do that engagement and to really understand the concerns, the desires, and the goals of the broader community insofar as they can help to inform and shape the commercial development that occurs at Double Eagle II, that that is important to us. And that's why we are emphasizing that we will engage in that process when we're updating the full Master Plan, and that effort will start later on in this calendar year and stretch out well into 2025. So we're not going to shortcut that. We are going to... We're going to focus a lot of energy, time and attention on that. There wasn't a remark about the building the building heights. I believe it was Ms. Horvath who said we that she was told that we would stick to only 35 feet structures. That's actually not correct. I'm not asserting dishonesty. I think there was a misunderstanding. 35 feet is the building restriction line. That is the height restriction adjacent to the airfield, and from there FAA allows a stepped-up increase for every 10 feet you can go to higher heights all the way up to 150 feet. What we're proposing and agreeing to is much lower than that. I certainly understand the desire for more kind of awareness and sensitivity to the viewsheds, as far as building heights go. We can certainly work on providing more information to that effect. That said, I want to also emphasize, I think the viewshed to the West as well as the East, are very important, so I would not assert that the views to the West are not important. I believe that they are important, and the staff from the Aviation Department spend a lot of time out at Double Eagle II. We love and respect the landscape. We love and respect the nature of the sites of our neighboring sites. So I apologize to Ms. Hendrix if you received the wrong impression. I think when you're talking stories at airports, it's a lot different than when you're talking stories and residential areas. Did I lose you? Oh, okay, sorry, it cut out for a second. I think I'm still with you. So there's a misunderstanding there, and we will clear that up. That said on tribal engagement, I want to address Mr. Bobroff. Yes, our intention is to engage in very significant

consultations. I'm glad you were present today, and I look forward to those discussions because the strategic vision of development at Double Eagle II is still taking shape. So we would very much like to engage with you and our other neighbors to help inform what that strategic looking or that strategic vision looks like, because I think you all can play a very important role in shaping that. So that said, I think I've addressed all of the major remarks or concerns. I do stand for questions. If there are any.

Tim MacEachen: Chair. Chair.

Jonathan Hollinger: I think I was muted. Thank you, Mr. Manriquez. Commissioner MacEachen, do you have a question? Comment?

Tim MacEachen: Yeah, I'm just trying to understand this, and I think everyone's spoken eloquently about it. And there might be so much information. There's a little confusion on what's being said. But if I'm hearing Mr. Manriquez correctly, I think it's kind of come down to a chicken and egg thing that there needs to be some things done in past for us to go forward on anything and be able to really dig into the depths of what the neighbors' concerns are and address the neighbors' concerns, but this has to go forward first before we get far enough along where we can address those concerns. Is that what I'm hearing?

Manny Manriquez: That's correct, sir.

Tim MacEachen: Thank you.

Jonathan Hollinger: Thank you, Commissioner MacEachen.

Matt Myers: Alright, sure Chair Hollinger, Matt Myers. Do you mind if I add something here?

Jonathan Hollinger: Please do.

Matt Myers: And I don't know if the applicant is finished with addressing the any questions that were brought up during public comment, but I think we should make it clear that after the applicant is done, and they address any other questions that had been asked, I think we should make it real clear that you can open it up to cross examination. And if and if there are any people from the public who did not get some specific questions answered, that would be their opportunity at that time. And I think, since this is a, you know, a case that has a lot of interest in it, and it's quasi-judicial, we need to make sure that that we make that opportunity clear for people. And maybe you are going to do that, anyway, Chair Hollinger. But I just wanted to bring that up.

Jonathan Hollinger: I always appreciate your feedback, Mr. Myers. Thank you. So being said Commissioners, do we have further questions for the applicant?

Gary Eyster: Eyster.

Jonathan Hollinger: Vice Chair.

Gary Eyster: Thank you, Chair. One thing I'm not clear on, Mr. Manriquez, is that this is listed in the staff report as an as-built Site Plan. And to me that says that it deals with all of the aspects that are there now. So why is it that we are putting in things like dimensional standards? And they just they don't seem like they've been well coordinated with the public or with the Native American community.

Manny Manriquez: Thank you, Mr. Eyster. Mr. Chair. I do think that this is perhaps an overly complicated effort, because, as you know, we're both talking about future standards as well as, you know, the as-built conditions. So I think we can do a better job moving forward of clarifying how we're addressing the processes, that said, because we do have a project that is now under lease. And that we're looking to, you know, really lay the groundwork for them to pursue their development. And I'm referring here to Mesa Film Studios. I think that that is part of the impetus for moving forward to address... state those standards that said, we're advised by the Planning Department in this matter, and so I would suggest that they also be allowed to address that question, because this was this was really a joint effort and discussion.

Mikaela Renz-Whitmore | CABQ Planning: Mr. Chair, Commissioners, if I may.

Jonathan Hollinger: Ms. Renz-Whitmore.

Mikaela Renz-Whitmore | CABQ Planning: So the as-built aspect of this is really about what's showing on the Site Plan on the ground. The future looking part are the design standards. Aviation is looking toward that next submittal that will amend this site plan to propose physically on the ground where the movie studio will come in what's in front of you today. If you don't like calling it as-built, that's fine. But I want you to understand that the site plan is only showing what does exist on the ground. The design standards absolutely will apply to any development until such time as the Site Plan is amended. So if the terminology is bothering you, stop using it. But really, both things are in front of you. You're approving the Site Plan that shows development on the ground, and you would be approving the design standards as proposed that would apply to any future development. And if you want to only show design standards that apply to what's on the ground, that would need different conditions related to the building heights.

Gary Eyster: I appreciate that, Ms. Renz-Whitmore. The Site Plan did say that it was an as-built Site Plan, and I don't think it should have, but thanks for clarifying it. So, Mr., maybe back to Mr. Marquez, as Mr. Gleason said, that these dimensional standards are just applicable to aeronautical uses. Did I understand that correctly?

Manny Manriquez: Councilor, I'm sorry, Commissioner. Commissioner Eyster. I do recall Mr. Gleason saying that, but if I can ask him to reiterate his comment.

Dekker: Yes, Commissioner Eyster, technically, right. Now, these design standards apply just to aeronautical uses. However, understanding the sort of iterative dynamic process that we're in, we are trying to do this not as... trying to do this once in terms of this debate about heights and maximum heights and anticipating future uses that would be non-aeronautical.

Gary Eyster: Good, that that answers my question. So I interpret that to mean that these heights that we are looking at actually will be expected to apply to non-aeronautical uses in the future. Maybe not today, but in a few months, when we see, hear it again.

Dekker: That is correct.

Gary Eyster: Thank you. That that helps me a lot. Just one last one for Mr. Manriquez, or maybe for Mr. Gleason. I asked Ms. Heyne about a profile where I'm standing down at the petroglyphs and looking west. And do I see this 65 foot building? That does not exist today, does it?

Dekker: Vice Chair Eyster, we do not have a graphic that really depicts that in a way, I think, that is... that is meaningful.

Gary Eyster: Understand. Understand. You could probably do that a couple hours from now, but we don't have it right now.

Dekker: That's correct.

Gary Eyster: Thank you.

Manny Manriquez: Thank you, Mr...

Gary Eyster: Thank you. Chair.

Manny Manriquez: Commissioner Eyster, if I may just add 1 point, I think a good point of reference is the height of the control... the air traffic control tower. I believe that height is a 102 feet. So you know, assuming a little more than a 3rd lower height threshold than that. That is the only structure right now that serves as an existing effective point of reference for how those heights might be, or how structures observing these heights might be viewable from the areas in question.

Gary Eyster: But I don't think I've seen a picture like that today. Or even... or a graphical profile. I would love to.

Manny Manriquez: Yeah, not from the angle you're asking about. So that's my understanding as well, sir.

Gary Eyster: I appreciate that. Thank you, Chair.

Jonathan Hollinger: Thank you, Vice Chair. Thank you, Mr. Manriquez. Commissioners, other questions? If there are none, per Mr. Meyers' reminder, I do think this would be a good opportunity to open cross examination. Ms. Renz-Whitmore, we might... We need to have a chat to figure out if any rules need to be amended, or we... where we stand in regards to that process. And I also see Mr. Myers.

Matt Myers: Yeah, I... Chair Hollinger, I think I can kind of talk us a little bit what we did last time. And I think, probably what we should do here is, I think we need to start by suspending... You need to make a motion. The

Commission needs to make a motion to suspend the rules of conduct, as it relates to cross examination, because we are not going to follow the procedures set out in the rules of conduct, okay, the EPC's rules of conduct, because what we're gonna do as we've done previously is open it up, okay? There are... People can raise their hand and they can ask questions. And then it'll, I think, ultimately, Chair Hollinger, you will say, you know, you're gonna allow most questions unless it's just clearly the question makes no sense. It's overly long. It's been asked and answered; it's irrelevant. Okay, but that's up to you, Chair Hollinger, to determine. But I think the way we've done it in the past, and maybe the most efficient way is to allow the public to ask the question into, you say, oh, go ahead and answer that, whoever it's been addressed to, unless it's just like a harassing question or irrelevant, in which case you might say we don't think that needs to be asked, because you ultimately are running the meeting, Chair Hollinger. So I think we'd start with a motion to suspend the rules in regard to the rules of cross examination.

Jonathan Hollinger: Appreciate your guidance. I could make the motion, but.

Tim MacEachen: Chair?

Jonathan Hollinger: MacEachen.

Tim MacEachen: So we've been at it almost 3 hours. Can we get like a 10 min break, and we'll come back and make your motion and go through this other stuff?

Jonathan Hollinger: Sure, I think this would be a great time to do that. 10 minutes suffice?

Tim MacEachen: Ten's fine for me.

Jonathan Hollinger: Alright, everyone. Let's go ahead and take a recess. We'll reconvene in about 10 min, and we'll keep going with the cross examination if needed. Well, it's officially 10 min. I'll see if everyone's had a chance to make it back.

-10 Minute Recess-

Gary Eyster: Chair, when you're ready, if you like, I'll move that suspension.

Jonathan Hollinger: Thank you, Vice Chair. I think we're all here. And there's Commissioner Likar. Well, okay, so hopefully, everyone had a good break. Where we paused was a potential suspension of the rules of conduct, cross examination. Vice Chair offered to make that motion, and we can proceed to see if anyone would like to cross examine. So Vice Chair.

Gary Eyster: Thank you, Chair. I move suspension of the EPC Rules of conduct as they relate to cross-examination.

Jonathan Hollinger: Thank you. Is there a second?

Tim MacEachen: Second. Commissioner MacEachen.

Jonathan Hollinger: Thank you, Commissioner MacEachen. We'll go to a roll call vote.

Adrian N. Carver: Mr. Chairman. This is Carver.

Jonathan Hollinger: Carter.

Adrian N. Carver: Yeah, I'm sorry. I just have one quick question. The... does our suspending the rules... Does this vote... How do we determine standing to do cross examination? Are we just going to let anybody interested do it?

Jonathan Hollinger: Per Mr. Myers consult, I believe that standing falls in my purview. However, he suggested, basically keeping an open floor.

Matt Myers: Yeah, and Chair Hollinger, do you mind if I just add to that real quick?

Jonathan Hollinger: Please do.

Matt Myers: Yeah, I agree with what Chair Hollinger said. And I and I think what we will do is err on the side of allowing it. And New Mexico courts have said that if you have almost any interest, okay, if you can demonstrate any interest in the issue at hand, okay, you would have standing. Okay. Sometimes there's standing... automatic standing, if you're within a certain distance or you're within a Neighborhood Association that has property in that area, but also New Mexico courts have interpreted it pretty broadly to mean, if you have any interest, so like, certainly in this instance, you know, that the tribes, you know, who are surrounding would have an interest, or their members and things like that. So I think it's very, very broad, you know.

Adrian N. Carver: Thanks for that clarification. Appreciate it.

Jonathan Hollinger: Mr. Myers. Thank you for the question, Commissioner Carver. So a motion was made by Vice Chair Eyster. I believe it was seconded by Commissioner MacEachen. We had some discussion. If there's nothing else we'll move to the roll call vote. Okay, Commissioner Aragon.

Daniel Aragon: Aragon, Aye.

Jonathan Hollinger: Thank you. Commissioner Carver.

Adrian N. Carver: Carver, aye.

Jonathan Hollinger: Mr. Likar. That was an aye. Commissioner Coppola.

Giovanni Coppola: Coppola, aye.

Jonathan Hollinger: Vice chair.

Gary Eyster: Eyster, aye.

Jonathan Hollinger: Mr. Halsted.

Renn Halstead: Halstead, aye.

Jonathan Hollinger: Mr. MacEachen.

Tim MacEachen: MacEachen, aye.

Jonathan Hollinger: That passes 8 to 0. Essentially, what we're doing is modifying our rules for cross examination. We want to give all members of the public another opportunity to ask any question that hasn't already been asked. We don't want to create an open dialogue. So in the past, what we've done is, make your questions specific. Try and keep them short. Address them to me, and then I can direct those questions to the appropriate person. You can also state, I'd like to ask this question, too, and it'll give a little control of the meeting, so we don't just run on and on, but it looks like some others... public are already signed up, and Mr. Myers.

Matt Myers: Yeah. Sorry, Chair Hollinger. Sorry to interrupt, and you know it would be nice if, when they ask the question, they do one at a time right? One clear question at a time. So the person they are asking the question of can answer it. Then they could ask another one, and you know I think, you know, after you know the... but I think that's how it should go. Thank you.

Jonathan Hollinger: Thanks, Mr. Myers.

Matt Myers: Yes.

Jonathan Hollinger: Okay, so let's also pay attention to make sure we're not asking the same question twice. And you know, let's make this reasonable. If you have a couple 3 questions, great. If you need more, I can address that. But we'll open the floor. Sorry, Ms. Chavez, who signed up first?

Christina Chavez- Gonzales: Yes, Chair Hollinger, our 1st cross-examination speaker is going to be Jane Baechle.

Jonathan Hollinger: Thank you, Ms. Baechle. You've already been sworn in. Let's proceed.

Jane Baechle SFVNA: Thank you. Mr. Chair. So I do have 2 questions. The first one I'm following... I'm trying to follow up on some comments made by Ms. Renz-Whitmore. But I would defer to you all who is the best person to answer this question. It's kind of a process question, and my understanding is the application before you is establishing building height standards. But nothing in this establishes true dimensional standards, so square footage or other things which would actually significantly impact the actual... the actual impact that a structure

would have on the landscape and one's appreciation of it. So if this only addresses building heights, is there... What is the next step? And is there a requirement to establish those other dimensional standards prior to moving forward on anything?

Jonathan Hollinger: Thank you. Where'd you go? You disappeared? So thank you for that question. Would you like to address this to Staff, or the applicant?

Jane Baechle SFVNA: to. I think it's a.. I think it's a process planning question of mine. Like I said, I'm responding to some ideas I had listening to Ms. Renz-Whitmore. So I think it's more planning question.

Jonathan Hollinger: Thank you. Ms. Baechle. Alright, Ms. Renz-Whitmore, would you like to address that? If not, you can defer.

Mikaela Renz-Whitmore | CABQ Planning: Mr. Chair. Commissioners, I'll take the first part, and then I'd like Catherine to answer the rest of it. So the design standards in front of the EPC today are more than building heights, and I'm going to let Catherine talk about what else is proposed. But any future building that gets added will have to come back to the Planning Commission for an amendment to this Site Plan to show where's the building? It would have to meet any of the design standards that could be approved today by the EPC. And at that time, if they needed to amend any of those design standards, they could ask. But the idea is that any future development comes back for an amendment to the Planning Commission and meets these standards as adopted by EPC, unless they request a change, which could be reviewed at that time. And, Catherine, would you just talk a little bit about the other kinds of design standards that are proposed?

Catherine Heyne: Alright, sure. Some of the other design standards that have been proposed are setback standards, parking standards, landscape standards, screening standards, and all those are in the design standard worksheets that were included in the Site Plan. And other than that, anything that isn't mentioned in those site design guidelines will then defer to applicable IDO sections, and that I can show you what that looks like from the IDO Table 5.9, if I can share my screen.

Jonathan Hollinger: Ms. Chavez, have you enabled that for her? Thank you.

Catherine Heyne: So these are different sections that would apply during the design standard decision-making. And so for anything, for example, dimensional standards that'd be applicable to most use or districts, as shown in 14-16-5-1, unless different standards are approved in the this NR-SU approval process. And so that is what we're working on at this moment. And we did talk about parking and look well, parking, landscaping, buffering, and screening. They have portions of walls and fences. They addressed outdoor and site lighting. They... we talked about briefly, solar access, building design, and signs. Those are the things that were all part of the application packet, and we can pull up design standards or the proposed site design pages, if you would like to see those.

Jonathan Hollinger: Ms. Heyne, perhaps we just pull those up, so everyone has them available.

Catherine Heyne: Alright, just, and Ms. Renz-Whitmore, did you have anything additional to add, while I'm pulling these up?

Mikaela Renz-Whitmore | CABQ Planning: I do not.

Catherine Heyne: So this is maybe a little bit big for the screen. If I get it too small I probably won't be able to read, but here are the design standards. There's 2 pages of design standards and, as shown by the applicant, they did show this dimensional standard summary table, which showed setbacks, building heights, and parking standards. They, as you see here, they had the 60 feet maximum, 80 within 250 feet. We've addressed that. And then all others, was the 80 foot height parking setbacks they had suggested or proposed, 100 feet with a setback from Atrisco at 25 feet and then the setback from property line or lease line was 10 feet. Except for when shared, parking lot is provided, and that's internal. We suggested, and part of our suggestion, with 36 feet is as-built so nothing there on site, except for the tower, is taller than 36 feet. But we did like, I said, as you saw, we did discuss different height standards, and one of the things that we kind of left open, although we did make a suggestion, was for setback standards, parking setback standards, and let me see if I can... I don't where they are anyways. I don't pulling them up, but we have landscaping, signage, lighting. And again, here's the parking setbacks and building setbacks rules and regulations that have been proposed. Is there anything more specific that I can answer?

Jane Baechle SFVNA: Well, I appreciate everything, but I guess it's not what I'm looking for, in a sense. So to me, I'm trying to get an appreciation of the mass or scale of any structure, and, let's say, a 60-foot tall structure, whose square footage is 2,000 square feet, is an incredibly different mass or scale, or impact on the landscape than a 60-foot high structure which is 10,000 square feet or 20 or 30. So what I'm trying to find out is, are there any standards about that portion? Are there any dimensional standards that relate to that? And if not, where do those come in? We can look at 14-16-5-1.

Mikaela Renz-Whitmore | CABQ Planning: So let me just say there, as far as I understand it, Ms. Baechle, there are no limits on massing or square footage, so the point at which the massing would be reviewed is when a proposal comes in to amend the Site Plan and shows the footprint of the building and the elevations.

Jane Baechle SFVNA: Okay. So not today, and essentially, not now. Right now it is, the language is what it is. It is what I'm hearing only addresses allowed heights.

Catherine Heyne: That is correct.

Jonathan Hollinger: Understood. Yeah, so thank you. You said you had a second question. Ms. Baechle.

Jane Baechle SFVNA: I do, and this one I'm responding to a subject that came up from Mr. Manriquez and that is some reference to the film studio, and I'm just curious where that stands in terms of communication with the City. Where is it between what I've heard about so far and eventually being a reality?

Jonathan Hollinger: Mr. Manriquez, would you like to address that?

Manny Manriquez: I would, I would, Mr. Chair. Thank you. Yes. So to address your question where it currently stands is the Mesa Media, LLC, which is the developer for the Mesa Film Studios site is under lease. We've executed that lease agreement with them for a Phase 1, 60-acre development on the northern portion of our Double Eagle II property. There is a roadway that enters the site that is about a thousand feet long and then that's from Atrisco Vista Boulevard on the north side, and then you enter the site. And just for clarification, that's about almost a mile north of the airfield. They will be... They are developing their site plan currently. And so their site plan will have to go before this body. That date, as I understand it, is to be determined. But that's essentially where they're at in the process: still in the planning phase.

Jonathan Hollinger: Thank you, Mr. Manriquez. Quite well.

Jane Baechle SFVNA: Thank you so much.

Jonathan Hollinger: Does that satisfy your cross examination?

Jane Baechle SFVNA: Yes.

Jonathan Hollinger: Okay, thank you for your time. Thank you for the questions.

Jane Baechle SFVNA: Thank you.

Jonathan Hollinger: Ms. Chavez, who's next.

Christina Chavez- Gonzales: Yes, Chair Hollinger, our next speaker, is going to be Renée Horvath.

Jonathan Hollinger: Ms. Horvath.

Renée Horvath: Okay. I was trying to... Yeah, I'm trying to do my video. I can't. Some reason you cannot start your video, because the host has stopped it.

Jonathan Hollinger: So if you can't get it started, we can still hear you, but just as a reminder.

Renée Horvath: Okay. Yeah. Okay.

Jonathan Hollinger: Address your questions to me, and then let me know where you'd like them to be directed, and we'll get the best answers we can for you.

Renée Horvath: Okay, thank you. Well, my question is, throughout this process with the Aviation Department, the 2 meetings that I remember is the one at the Double Eagle that we had in June and the one at the Sunport that we had on July 23rd with the Aviation staff in their office area. So the one at the Double Eagle was an open house meeting to discuss what people were concerned about. So one of the big issues, I mean, was the building heights and the viewshed. That was a big issue. But I don't remember going over those design standards that was just shown by Ms. Heyne, the planner. We didn't have any of those kind of discussions

correct of what the setbacks would be, or the building heights or exactly where the building layouts would be or the access correct? We never had that discussion at any of these meetings. We discussed our concerns. But isn't that correct that we did not go over these standards with you, despite we were told that you would engage the public?

Jonathan Hollinger: If I'm hearing is your question in regards to engagement, or in regards to the slides that she just brought up for us.

Renée Horvath: Well, I, just, I thought that I guess my question, we were told we would be engaged, and we thought we would be engaged with this information that's being proposed today, and this was not discussed at any of those meetings, correct? I'm asking the Aviation Department.

Jonathan Hollinger: So, Mr. Manriquez, would you like to address that?

Manny Manriquez: Sure I'd be happy to, thank you, Mr. Chair, Ms. Horvath. So we did not discuss the setbacks. Because we hadn't established those yet we did. We got some feedback on building height concerns, and had sort of, I would call it, a just a preliminary discussion, but we didn't at that time have the information that we needed to really more thoroughly discuss building height limitations other than our knowledge of the sort of the FAA requirements. We did have handy at that open house some the site plans for the Mesa Film Studios project, so some of that information was available. I wasn't standing at that table. I was sort of in the main lobby, addressing sort of the bigger picture questions.

Dekker: If I could, if I could add to that.

Manny Manriquez: Please, do.

Dekker: At the at the open house in June. We did have the draft design standards. They were on full sheets, and we had people available to discuss them, and we had interaction with multiple participants about the proposed standards. So we did have them there, and they were available. And we were open to discussions about them at that point. It's for that.

Manny Manriquez: Thank you. Thank you, Will. And I just I want to clarify. So what I'm referring to is the planning presentation that you saw today, as well as our sort of compromises around building heights. We did not have that ready. But yes, Will is correct that the design standards were presented. So, thanks for that clarification.

Renée Horvath: But that included the building heights.

Dekker: Absolutely included the building heights.

Renée Horvath: Because, it wasn't...

Dekker: 85 feet at that point. That was the proposed maximum building height at that point. 85 feet.

Renée Horvath: That. Well, that obviously wasn't clear, because at the July 23rd meeting at the Sunport, I asked Mr. Manriquez what if we expressed concern about the building heights then, and I asked specifically, what would they be? He said, 30 to 36 feet, and I don't remember him saying, Oh, but it can go up to 60 or 85 feet. I did not in any of these meetings did I get the impression that we would be going up this high. So I it, it's I, you know. You said that there would be public engagement. I thought we would address these design standards so that we're all on board with this before we move forward. And so once these design standards today get approved, it's a done deal. And my experience with entitlements just like you mentioned. The IDO allows this, and therefore we're not going to go address any of any concerns after that. Once you get these entitlements, it's a done deal regardless if we move forward from here on out, because we have no say in anything else after that, correct?

Jonathan Hollinger: So, Ms. Horvath, just make sure that we're not making comments now, we're asking questions. I appreciate that. And to keep this and from being an open dialogue, just make sure they're going through me, and we can... We can address those or point those in the right direction. So, Mr. Manriquez or Mr. Gleason would you like to comment further on that?

Dekker: I would just, I would just emphasize that we have documentation that we have the design standards at the open house. The intent of the open house was to share that information, which we did with everyone that was there. And so I guess I disagree that we are... that we were not showing the design standards during the process of our public engagement.

Renée Horvath: And so did you sit down and explain it specifically? Or was it just sitting on a board that wasn't apparent to the crowd?

Jonathan Hollinger: I think I'll take that one over and say for Mr. Gleason it seemed apparent that that information was there. Possibly it wasn't presented in a way that was obvious to... Mr. Gleason, would you confirm that?

Dekker: Mr. Chair. It was an open house format. We had specific people sitting at tables ready to answer questions and walk people through the standards that were shown on the sheets, and they were not little sheets. These were 24 by 30 inch sheets, so the information was there for anyone to see.

Jonathan Hollinger: Great. Thank you. Ms. Horvath, do you have additional questions?

Renée Horvath: Yeah. And so once these get approved, if something comes back in the future right up near the road, being very tall, the neigh... there's no nothing that the neighborhoods or the tribes or Petroglyph Monument can say, because it is approved today. You cannot go back down once you get an entitlement.

Jonathan Hollinger: Ms. Renz-Whitmore, I think we've touched on that. I think the question is, what recourse may she have? And I think what you said before was that they would have to come before us for any new building.

Mikaela Renz-Whitmore | CABQ Planning: That is correct, Mr. Chair, Commissioners, Ms. Horvath. The standards would be required going forward, and any future development has to come back to the Planning Commission for compliance with either those standards for the IDO where these standards are silent, and at that

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point they could ask for revisions, but if they don't, they have to comply with the standards that you might approve today.

Jonathan Hollinger: Thank you. Ms. Horvath. Additional questions?

Renée Horvath: So if we go forward with public engagement, there really is no more public engagement, because it's already decided today.

Jonathan Hollinger: And again, is, is that a question or a comment?

Renée Horvath: Well, it's kind of a comment? But it is a question that we're promised public engagement. But it's pretty much decided today. The things that we're concerned about is decided today and therefore there really won't be much public engagement. We'll just be told it's already a done deal.

Jonathan Hollinger: Mr. Manriquez again. I've asked you this earlier, your plan and engagement. Would you like to touch on that one more time?

Manny Manriquez: Yes, Mr. Chair, I'd be happy to, as far as Ms. Horvath's initial questions. Those were asked and answered. So I'll move to your question about public engagement. So the Master Plan update process that I mentioned that would start within this calendar year will indeed be a very robust process of public engagement to help us shape the kinds of developments that will occur at Double Eagle II Airport. And so members of the community will be invited to join us for discussion, and we will be formulating an advisory group as well.

Jonathan Hollinger: Ms. Manriquez, Ms. Horvath, do you have additional questions?

Renée Horvath: That's pretty much it. It just means that we'll listen to what they're going to propose, and we have no say further up. So that's why I promoted that we not decide on anything, that we do what they said to us. We will have public engagement, and we'll sit down and discuss these design standards, so we fully understand what's being proposed and discuss them and work out a deal with all stakeholders where we're all happy with. So that's where I'd like to go with this. So thank you.

Jonathan Hollinger: Thank you for your time. Ms. Chavez, who's next?

Christina Chavez- Gonzales: Hollinger. Our next speaker is going to be Mike Voorhees.

Jonathan Hollinger: Mr. Voorhees. Welcome back.

Mike Voorhees: Chair. Thank you. Chair Hollinger. So my question is based on the confusion that I have. I've heard some conflicting testimony here today. I would... It was, it was expressed that we're only looking at aeronautical uses and building heights as they apply to them today. And thus I expressed bafflement that there was a need for anything higher than 36 feet. And the testimony from, I believe Mr. Manriquez was saying that we need to approve this today so that the film production studio can move forward. But I'm under the understanding that that is a non-aeronautical use. And then I heard from the Planning staff that although we're

only looking at building heights as they apply to aeronautical uses, those same heights are going to be used for non-aeronautical uses for no apparent reason, so my question is., which, if any of these things is correct? And what is the justification for needing to do it today? If the Mesa Film Production Studio can move forward because it has to submit its own plan that's not subject to this. It... it's nonsense, non-sensical. And I don't know which one of those things is actually true or controlling.

Jonathan Hollinger: Thank you for the question. Ms. Renz-Whitmore, would you like to address that?

Mikaela Renz-Whitmore | CABQ Planning: Sure I'll take the piece of it that relates to my testimony. So I believe that, and I... please, Catherine, if this isn't true... I believe the building heights are written to apply to the entire site, and so until such time as that wording is changed, to only apply to the aeronautic uses. It would apply to any development that comes in subsequently. And it is true that the movie studios will have to come in to amend this Site Plan, at which time they either follow the standards that may get approved today, or they can ask for revisions. So it is up to the Commission to decide whether to change the building heights as written or as proposed in this Site Plan to be cover what exists today, or whether, as I believe, they are written to apply to any development in the future.

Gary Eyster: Chair. Are you continuing? Are you muted?

Jonathan Hollinger: I guess that's why no one's responding to me.

Ms. Amy. I was asking if you'd like to add additional information to that.

Catherine Heyne: Thank you. Chair Hollinger. I think Ms. Renz-Whitmore covered that well, thank you.

Jonathan Hollinger: Thank you. Where did you go. Mr. Voorhees? There you are. Things keep shifting. Did that satisfy your question? Do you have additional questions.

Mike Voorhees: Well, it's... it... It still didn't clarify that. Why we were instructed that this only applies to aeronautical uses today or why there's any urgency if the film studio will be able to provide their proposed Site Plan regardless of what would be decided today if the 30, if 36 feet were proposed as Staff originally recommended, and as we were assured in the meetings with the Aviation Department, that's what was necessary that then, should any amendment be needed for their specific studio, they would have an opportunity at that point to make adjustments. It so... that so... that's where I'm confused about the testimony as it stands there. There was an indication of urgency that doesn't seem to be apparent, based on the justifications and that original staff recommendations would be ample for the needs. So I just can't figure out why this push for even taller building heights is going forward. So whoever on the staff or Aviation can answer, that would be would be helpful.

Jonathan Hollinger: Thank you. And as I listened to the presentations I understood that the 35 or 36 is it's kind of been jumped back and forth was relative to the height or to the setback. If that's not clear, let's see what Mr. Manriquez has to say.

Manny Manriquez: Mr. Chair. Yes, it's correct that it's relevant to the setback. But it's also that 35 feet is the building restriction line. So any structures in the immediate vicinity of the airfield are restricted to 35 feet. So that's I think, why that number keeps coming up in discussions is because that's sort of the starting point or building restrictions, building height restrictions in the vicinity of the airfield. As far as why we're addressing this today. I think that it's this is setting a precedent for future development. And we understand and hear the concerns of the public, which is why we're observing and really willing to set height restrictions that are quite a bit lower than, for example, the Amazon, the very large Amazon facility that exists on the west side. That's about 90 feet. Does that? Does that clarify things?

Jonathan Hollinger: Thank you, Mr. Manriquez. Mr. Voorhees. Do you...

Mike Voorhees: It partly clarifies things, but it... my question, Chair Hollinger, is when we met with the Aviation Department and we're assured that that 3 stories was really all it was needed, and that that was approximately 36 feet and we knew that to be building heights, not setbacks, or any other restrictions, why, that is, all of a sudden change. There was no confusion on this point. I can attest to what was said at the meeting at the Sunport, and Ms. Horvath was not mistaken, and what she heard this was explained to all of us. And now it's like, Oh, well, that wasn't what was meant. And instead, we're... we just want to go with this higher level. And that wasn't even supported by the original staff recommendations. And I've seen no justification in any of this testimony why, it needs to go higher than 36 feet for any of those buildings. So if anybody could speak to what the yeah, the justification is, that would be helpful.

Jonathan Hollinger: Mr. Gleason, I remember you had some input on that during your testimony.

Dekker: Yes, thank you, Mr. Chair. I first want to make the point that this site is zoned NR Sensitive Use, and that requires that there are design standards associated with the development, that that by which the development can be judged. So we need to, we need to establish, because there are not design standards currently adopted for Double Eagle. So in order for any new development to go forward, we need to have in place a set of design standards. And so, and, as Mikaela said earlier, these would apply to any development. It's not like these are just around the runway. These are for anything on the 4,200 acres out there. I may be confused, muddled the waters a little bit, because there's been the issue about when the non-aeronautical uses were approved by the City. But, in fact, the design standards that we're talking about today will move forward and will be the metrics by which any future development would be judged. And the reason, and it's not an arbitrary number, it's not just like, well, let's make it as high as we can. The number, the 85 feet was first proposed because of the potential height of a film studio, and that's pretty close to some others that that we've seen. We think that we can work with a lower number. But film studios are very large clear height structures that... And that's what's required. It doesn't work with 36 feet. If you put 36 feet in there, there's no film studio period, and if I may just add on to that, the standards that currently govern this site are the FAA requirements which, as Manny mentioned, are much higher than 36 feet. They currently allow up to 150 feet. Manny, correct me if I'm incorrect there. So the standards that are actually being proposed within these design guidelines, at which are now capped at 65 per the revised conditions, are substantially lower than what the current height allowances for the airport are, so there's already been a concession in terms of understanding the importance of the mitigation to adjacent open spaces and hearing neighborhood concerns and trying to balance that with what is needed for future uses.

Jonathan Hollinger: Thank you for their response. Mr. Voorhees, additional questions?

Mike Voorhees: Yeah. Yeah. Well, to follow up, we had asked about the height of other film studios in town and at Mesa del Sol and my understanding was, none of them were that high, and since it has been expressed that when Mesa Studio presents their site plan, they can ask for a specific high you know, greater height if needed and justified for that particular project. But the FAA is only establishing a maximum so that it doesn't interfere with Aviation. Not that a lower one isn't, in fact, more desirable for visibility of approaching aircraft and taxiing aircraft and things of that nature. So going to a blanket lower, more sensitive, height of 36 feet to govern, you know, if we're... if this is supposed to be precedent setting, seems to be the prudent approach. And if a particular project of economic significance in the future has specifics and a justification, then, that, you know, could come back to the EPC for a review and see if it conforms with the cultural sensitivity and true public engagement prior to that process. So is, I mean, is there anything that would preclude that from occurring, going forward?

Jonathan Hollinger: Thank you, Mr. Voorhees. I'm gonna say, Mr. Gleason, that might be in your book.

Dekker: Thank you, Mr. Chair. I worked on the studios out at Mesa del Sol, Netflix, and those are over 80 feet tall. So that was really, they are much taller than what we're even proposing here. And I think the idea that we would have a lower maximum height, and then have an exception to that, I don't think that's how design sims [?] work. I mean, we.... this is a project that is coming into the, you know, it is on the horizon. And we're trying to anticipate what the needs would be to make that project successful, and to accommodate that within the design settings. If there's no need for a building to be taller, no one's going to build to a taller height. I mean, people are gonna, if there's a restaurant out there, that restaurant's gonna be probably 24 feet tall at the most. So it's not because we have higher maximum heights that everything's going to go higher. You want the design standards to reflect the type of development that is anticipated in this case. It is of a light industrial nature, and the IDO has heights of up to 65 feet for those uses, and that's what we're going with.

Jonathan Hollinger: Mr. Gleason, Mr. Voorhees.

Mike Voorhees: One final question. Is it not possible to set a separate standard for the film studio site? And keep everything and set a lower, more sensitive one for the rest of the property, rather than just going to the maximum that's anticipated at one project? Because we had extensive discussions with the Aviation Department about hangar height and other aeronautical uses. And they said, Yeah, it's not necessary. So it seems strange that they're now advocating for a higher height than they discussed in in meetings with us.

Jonathan Hollinger: Great question. Thank you for that. Mr. Manriquez.

Manny Manriquez: Yes, thank you, Mr. Chair. Mr. Voorhees. We're actually offering to establish a maximum height that's lower than the previous height that was included in the Dekker information on, or rather the design standards information that was present at the open house. So sounds like we have some misunderstandings about what was said in our meeting with you. You know I certainly wish to address that offline at a later time, so

we can come to a shared understanding. But for now, you know, we've answered all the questions, and I think all the clear information is on the table. So I'll conclude my remarks with that.

Jonathan Hollinger: Thank you. Mr. Voorhees. You said that was your last question. Does that?

Mike Voorhees: Yeah, I have nothing further. Thank you very much, Chair.

Jonathan Hollinger: Thank you. Ms. Chavez, who's next?

Christina Chavez- Gonzales: Yes, Chair Hollinger, our next speaker, is going to be Elizabeth Haley.

Jonathan Hollinger: Ms. Haley, Welcome back.

Elizabeth Haley: Thank you, and I'm sworn.

Jonathan Hollinger: Right. Yep.

Elizabeth Haley: Yes, this is this is my question, and I should preface it by saying, I am a retired... the retired owner of a consulting firm for architecture and planning, and done a lot of planning studies in my career. One of the things I think that is a problem is that usually you have a paper document that's fairly extensive. That gives the rationale for why you are making the decisions that that you are, so you don't have to rely on word of mouth issues. My question is this, when we had the July 23rd meeting, one of the questions that I addressed to Mr. Manriquez was, have you done a traffic study or an access study that would be able to serve your intended uses on a Atrisco Vista or alternate forms of access to the site? And have you done a facility, infrastructure for capacity study? Because anytime you have design standards that allow a certain mass of buildings, you have to have the infrastructure in order to serve those. And it is unusual in this case that design standards are happening after that kind of information. And so at that time those studies had not been conducted, and we were talking in respect to the Petroglyph National Monument, but I think it applies to all of the design standards and the impacts to the Northwest quadrant, because we all share the same existing wells and water sources have any additional studies of that nature, infrastructure, water, traffic access being conducted prior to setting the design standards and the mass of those buildings.

Jonathan Hollinger: And my assumption is, you'd like to direct that towards Mr. Manriquez.

Elizabeth Haley: I think. I think probably the people who came up with the design standards. The Dekker firm.

Jonathan Hollinger: Okay, Mr. Gleason.

Elizabeth Haley: It goes to the rationale of what they described.

Jonathan Hollinger: And I also see Mr. Manriquez. So Mr. Manriquez.

Manny Manriquez: Yes, if I may preface the remarks from the Dekker team. With a little bit of clarification from Ms. Haley. So a traffic study has to be conducted. In relation to a project that, is, is, it is already envisioned, so that you can properly assess the nature of the traffic impact. So that will be done in relation to the Mesa Film Studios project. That is the only concrete project we have on the table now. So yes, that will be done. As far as the infrastructure study, we do currently have AECOM engaging in an infrastructure study that maps and tracks existing conditions. From that point we will then determine what else we need to do to plan for future conditions for infrastructure, and we are in frequent and ongoing communication with the water authority, as well as PNM. To determine that what we are planning for meets the needs and requirements of Mesa Film Studio, but also that we're beginning to think through what kind of infrastructure investment needs to be made for the future.

Jonathan Hollinger: Ms. Haley, would that answer satisfy your question, or would you also like to hear from the Dekker?

Elizabeth Haley: I would like to hear from the Dekker team. Why, why, they have set design standards without knowing the capacities of the existing system, or having estimates of what additionally it would cost to serve the design standards that they've suggested. Yes.

Dekker: Thank you for the question, Ms. Haley. You know this is a very, and I appreciate your experience and your insight into this, it's a very large area, and as Manny... this is... we are following the process that the City of Albuquerque follows in terms of... we do those types of studies when a project becomes real. And that when a project is proposed, and I, my understanding is that the film studio is undergoing a traffic study right now and is working with the water utility authority to anticipate future demand for water for this project. And they need to prove that there's that capacity before that project will move forward. So it is... I think it is too speculative to do it on 4,000 acres, but it's a much more real and specific analysis, when there's a real project with real square footage that is on the table.

Jonathan Hollinger: Thank you, Mr. Gleason.

Elizabeth Haley: Yeah, yes, I do. Oh, I'm sorry. Chair.

Jonathan Hollinger: Any other additional questions?

Elizabeth Haley: Yes, yes, I have 1 additional question, and that is, and it gets back to Ms. Horvath's observation about entitlement, that question is that when you look back at prior master planning efforts for that site, you do have those kinds of studies and estimates included, and I guess the question to the group from Dekker is, that is, that if they consulted other public sources of information about the capacity of the systems there, the water authority and the Mid Region Council of Governments, and also both the County and the City traffic departments who did extensive research in their development of the design guidelines for Atrisco Vista, and I just wondered if any of those sources that they went to any of those sources before coming up with the mass of those buildings that they were suggesting in the design standards.

Jonathan Hollinger: So, Ms. Hayley, would you like to direct those towards staff or applicant?

Elizabeth Haley: I think, whoever can answer the question.

Jonathan Hollinger: Let's start with Staff and see where that goes. Ms. Heyne or Ms. Renz-Whitmore?

Catherine Heyne: I'll try to answer that question. Thank you, Chair, and then I'll let Ms. Renz-Whitmore follow up, if I've missed anything. As I understand it, as you develop this NR-SU site plan, that you can make those suggestions that up, you know, as fits the site so that there is no... you don't necessarily need to review surrounding site plans or other plans as long as it stays within the current site, master development or master plan that is there at the airport, the double eagle airport. So it's not a requirement by the IDO. Thank you. Does that answer the question?

Elizabeth Haley: Not really.

Jonathan Hollinger: I. Maybe.

Mikaela Renz-Whitmore | CABQ Planning: Yeah, I sure... I think that's actually a question for the Dekker team. What studies, if any, did you look at as you put together the design standards.

Jonathan Hollinger: Thank you, Ms. Renz-Whitmore. Just want to get everybody's input so we get a thorough answer. Mr. Gleason.

Dekker: Thank you, Mr. Chair. We are familiar with the larger studies that are ongoing. But frankly, we don't... It's not like we have an estimate of the amount of square footage that is going to be developed across this entire land that is known as Double Eagle II. I think part of the confusion here is that most site plans are for very specific sites with a much smaller footprint, and in this case, this is really, even though we're calling it a site plan, it is a huge parcel. It is of a master plan scale, and we are documenting existing conditions and proposing heights and dimensional standards, but to extrapolate from that how much water is going to be used out here is speculative at best, I would say, and the same with traffic studies. It is just way too early to do any kind of studies that are going to yield meaningful results.

Elizabeth Haley: I guess my answer to that is you've just described why we believe that perhaps these design standards, you know, we could understand trying to develop specific design standards for the one project that you have ongoing. But we're wondering why these other design standards for the whole entire site are being done now, instead of after the master planning process has completed, and those kinds of... that that kind of information is available to the design team.

Jonathan Hollinger: Thank you, Ms. Haley, so as I understand it, and this may be simplified, I think we're trying to create a foundation to work with to be able to move forward. I hear your question, though. I'm curious. Mr. Manriquez, would you like to add to that?

Manny Manriquez: Mr. Chair, I'm not sure what more I can add that I haven't already said other than, you know, our strategy is to establish standards that projects down the line can adhere to. But you know it is incredibly difficult for us to forecast what kinds of facilities or industries may or may not be present in the future. And that's

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really the reason for us being very careful about development and then taking the next step: to do the full Master Plan update. I'm not sure if that's a specific enough answer. But that's what I have for you today.

Jonathan Hollinger: Thanks, sir. Ms. Haley.

Elizabeth Haley: I again I would say that that it isn't understandable to anyone who's been in the industry or to the... most of the commenters. Why the appropriate time to set those design standards wouldn't be, with the exception of the film studio, which is an ongoing project, wouldn't be after the master planning document discloses all of that information about the site that's certainly relevant when you're talking about assigning building mass to an area that basically, you don't have much information on it. It doesn't seem to make a whole lot of sense. And that's... thank you for... thank you for facilitating these questions, Chair. I sometimes have to facilitate them myself. I'm a zoning commissioner, and you're doing an excellent job. Thank you.

Jonathan Hollinger: Thank you for the feedback. I appreciate that. I assume that concludes your questions at this time.

Elizabeth Haley: Yes. Okay.

Jonathan Hollinger: Thank you, Ms. Chavez. Is anyone else signed up?

Christina Chavez- Gonzales: Yes, Chair Hollinger, our next speaker, is going to be Kenneth Bobroff.

Kenneth Bobroff-Laguna Pueblo: Good afternoon. Again.

Jonathan Hollinger: Back. Let's see, you're not driving.

Kenneth Bobroff-Laguna Pueblo: I am not driving anymore. Or by the side of the road.

Jonathan Hollinger: Did we get your full name and address last time?

Kenneth Bobroff-Laguna Pueblo: I believe I provided that to Ms. Chavez in the chat.

Jonathan Hollinger: Okay. Let's just for...

Kenneth Bobroff-Laguna Pueblo: Sure.

Jonathan Hollinger: Check the box. Let's go ahead and do that. Answer your name and address for the record.

Kenneth Bobroff-Laguna Pueblo: Kenneth Bobroff, Acting Director of Government Affairs for the Pueblo Laguna, Post Office, box 194, Laguna, New Mexico, 87023, I believe, is the zip code.

Jonathan Hollinger: And I think we swore in last time. So you're good to go. What questions do you have?

Kenneth Bobroff-Laguna Pueblo: So thank you, chairman, and most of my questions relate to the consultation process. So my first question is, what is the date of the request? And the reason I want that is so, I can understand when the consultation process is supposed to begin.

Jonathan Hollinger: Okay, thank you for that. I believe Commissioner Coppola asked the same question. Staff answered.

Mikaela Renz-Whitmore | CABQ Planning: Mr. Chair, Commissioners, the deadline for applications for this hearing fell before the new tribal engagement requirements kicked in on August 3rd, so everything on the agenda for today was not subject to the new tribal engagement requirements. They can...

Kenneth Bobroff-Laguna Pueblo: And so that that date would have been sometime in 2024.

Mikaela Renz-Whitmore | CABQ Planning: Mr. Chair, Commissioners, Catherine, I don't know if you found the application date itself, but the 2023 Annual Update of the IDO of the Integrated Development Ordinance that put those standards into place went into effect on August 3rd, 2024. Catherine, do you want to weigh in on the application date.

Catherine Heyne: Yes, and the application deadline for the August hearing was July 5th, so this application was submitted prior to July 5th.

Kenneth Bobroff-Laguna Pueblo: Okay, thank you, and I guess, is it accurate to say that therefore there was not a legal requirement to have any consultation with tribes or pueblos for this request?

Mikaela Renz-Whitmore | CABQ Planning: Mr. Chair Commissioners, there was certainly no zoning requirement. I believe there was some testimony about tribal consultation done under the auspices of other... the NEPA process, for example. I'll let Mr. Manriquez answer about other efforts.

Jonathan Hollinger: Mr. Manriquez, would you like to address that.

Manny Manriquez: Yes, I mean I have addressed it before, Mr. Chair. The intention is to engage in extensive tribal engagement consultation above and beyond the requirement when we are doing the Master Plan update. So that that is certainly our intention.

Kenneth Bobroff-Laguna Pueblo: But not to the, Mr. Chair, not to the Site Plan that's before the Commission now.

Jonathan Hollinger: Mr. Manriquez.

Manny Manriquez: Sorry, Mr. Bobroff. You're referring to the present effort in advance of this current meeting and the application that preceded it.

Kenneth Bobroff-Laguna Pueblo: I'm asking about the request that is before the EPC.

Manny Manriquez: Yeah, currently. Okay. I'm not sure if you heard Mr. Sloan's comment.

Kenneth Bobroff-Laguna Pueblo: I did, and I... Mr. Chair, if I may. I understood him to say that there was a communication on, I think it was December 13th, and then there was a another communication on August 3rd, and I'm wondering, did the City Aviation Department as the Aviation Department make any attempts to communicate with any of the tribes or pueblos, and if so, what attempts? When? Which ones?

Manny Manriquez: So, Mr. Bobroff, thank you. So OEI, as our colleagues, as our sister department, is really the lead, because they have the tribal liaison function. So they've been the lead in that process. So really, that's, I think, you know, if more discussion needs to happen with OEI as to their process, I think that would be appropriate. That said, we will coordinate very closely with the Office of Equity and Inclusion in future discussions, but it is our intention that those discussions would happen directly with us, because we are obviously the property owners and the operators of Double Eagle II.

Jonathan Hollinger: Thank you. So, Mr...

Tim MacEachen: Okay. Chair.

Adrian N. Carver: Mr. Chair.

Tim MacEachen: Can we restrict the comments in the chat room? That's not what that's for, and that's not what we should be doing.

Jonathan Hollinger: Didn't see those. But yes, thank you. So as a reminder, let's keep the chats strictly for reminders from staff about the ability to comment and wishing to speak. Thank you, Commissioner MacEachen.

Kenneth Bobroff-Laguna Pueblo: Mr. Chair, I haven't...

Jonathan Hollinger: Second question, go ahead. What's your question?

Kenneth Bobroff-Laguna Pueblo: Just to clarify, is it the City Aviation Department's understanding that the new requirements for tribal consultation apply directly to them as the applicant and can't be outsourced to a sister agency?

Jonathan Hollinger: Are you, Mr. Bobroff? Are you directing that to Mr. Manriquez, or staff?

Kenneth Bobroff-Laguna Pueblo: I think it would be probably appropriate to... for both to make sure everybody's on the same page. I recognize that these requirements are new, and that we're trying to work out the best way to make sure that they're met. Yep.

Jonathan Hollinger: So let's start with you, Mr. Manriquez.

Manny Manriquez: Sure my understanding that when our application was put in for this session the requirements were not yet in place, and so we back in late 2024 leaned on our colleagues at OEI, who have the contacts. And again, that was not required. We did that because we thought it was, you know, advisable to reach out. That said, moving forward, we'll follow the rules. We always follow the rules. But we do pledge to make sure that those conversations are thorough, and we will work in tandem with our sister departments, not farm out the discussion. So that they can be well informed discussions. And we can hear all the input and really have a healthy back and forth. So that's our intention.

Jonathan Hollinger: Mr. Manriquez. Mr. Bobroff, would you still like to hear from staff?

Kenneth Bobroff-Laguna Pueblo: I think it'd be useful, Mr. Chair.

Jonathan Hollinger: Very well. Let's see, Ms. Heyne.

Catherine Heyne: I think I want I will defer to make Ms. Renz-Whitmore on that one. Thank you.

Jonathan Hollinger: No worries. Ms. Renz-Whitmore.

Mikaela Renz-Whitmore | CABQ Planning: I want to make sure I understand the question. The question is, can a department rely on another department to do tribal Engagement requirements going forward?

Kenneth Bobroff-Laguna Pueblo: That's not quite it. But what seems to be happening here is that the Aviation Department seems to be suggesting that, well, DEI made some outreach, and that was enough. Given the executive order direction, I'm not sure it was, but I want to make sure, going forward that the understanding that, regardless of who the applicant asks for assistance, the requirement applies to the applicant. And they can't... It's the applicant's responsibility to make sure those consultations take place.

Mikaela Renz-Whitmore | CABQ Planning: Mr. Chair Commissioners, I would agree that it is the applicant's responsibility to make sure that it takes place. That can be directed to an agent to do, for example, or to another department to assist with. But ultimately the responsibility for making sure that it's happened does lie with the applicant. And I believe because it doesn't require, it isn't.... It wasn't required for this application, because this application was submitted prior to when those regulations kicked in. I don't know that it's relevant to this discussion.

Kenneth Bobroff-Laguna Pueblo: Mr. Chair, I have, I said that with my last question, but with your indulgence I have 2 more. If that's all right.

Jonathan Hollinger: Let's yeah. Let's get it all out so it's all on the record.

Kenneth Bobroff-Laguna Pueblo: The first is, it seems as though the analysis of this Site Plan was sort of based on the perspective of looking from the Monument over the Site Plan. Has there been any analysis at all of looking from the west where Laguna is, or from where Isleta is to the south, towards the Monument, and towards the view shed in that area? And if not, why not? And will there be?

Jonathan Hollinger: Thank you for that question. I think that Vice Chair Eyster also asked those questions, and Ms. Heyne tried to respond. It didn't look like there was that information at this time. Ms. Heyne, can you confirm or deny that?

Catherine Heyne: We do not have that information at this time. Thank you. Chair.

Jonathan Hollinger: Thank you for saying. Okay, Mr. Bobroff.

Kenneth Bobroff-Laguna Pueblo: My last question would be whether there's anything preventing conditions being added to this request that would provide for such an analysis that would provide for tribal consultations or, if it would require rejecting this request and instructing the applicant to do so to resubmit.

Jonathan Hollinger: Sounds like you're asking if we could add a condition. But let's ask Staff, possibly, Ms. Renz-Whitmore.

Mikaela Renz-Whitmore | CABQ Planning: Mr. Chair Commissioners, if you wanted to add a condition asking for future analysis or asking for a required analysis when future development comes in, that you could absolutely do that. You could add a condition requiring tribal consultations for future development, or, as what I think Mr. Bobroff is getting at, you could continue this case and add and give instructions to the applicant to provide that analysis and have those consultations before you're ready to make a decision.

Kenneth Bobroff-Laguna Pueblo: Thank you, Ms. Renz-Whitmore, and thank you, Mr. Chair.

Jonathan Hollinger: Thank you for your time. Ms. Renz-Whitmore, could you maybe see in the back of your mind that condition? Certainly. I like to hear from fellow Commissioners, but just so that doesn't get lost. Before we go any farther, I did want to make note for the record that Commissioner Likar had to step away for a family emergency. So he's no longer a part of this meeting. That being said, Ms. Chavez, is anyone else signed up to speak?

Christina Chavez- Gonzales: Chair Hollinger, Mike Voorhees has re-entered the queue for further questions, if that's okay.

Jonathan Hollinger: Is there anyone else?

Christina Chavez- Gonzales: No one further.

Jonathan Hollinger: Okay, Mr. Voorhees, we heard from you once. Do you have additional questions?

Mike Voorhees: Yeah, this only comes up based on testimony after I finish my last cross examination. It's about the tribal engagement. And at the last EPC, when the Ktacs [?] was cited and I hadn't heard about it, I submitted an IPRA request to the City to see what tribal engagement, in fact, had occurred. And when that came back, the information was that only 2 tribes were successfully emailed, a 3rd used a bounced email, and yet, FAA, order

50 50.4 B is quite explicit in its tribal engagement requirements. There's an entire chapter in there, and we know that there are at least 29 pueblos and tribes that have an interest in this land. And so that, that is a national policy for complying with Federal law, and compliance with Federal law is binding upon the EPC. Under its rules as well as the City Charter, and so stating that the IDO didn't require any tribal engagement prior to this is somewhat irrelevant to the duty to consult, to comply with those rules so based upon that. It seems like it would be prudent to continue this case and ask for consultation to occur before the applicant comes back. When I raised a similar point of noncompliance with the process before the City Council, they did insert a severability clause on the new uses to somewhat insulate, but lack of compliance there continues to jeopardize potential federal funds that could help the airport in meeting its goals moving forward. So it... so my question is, was there more than those 2 pueblos and one unsuccessful bounced email that occurred? And if that's all it was, wouldn't it make sense to go back and truly engage those interested parties before asking the EPC to move forward on this?

Jonathan Hollinger: So Mr...

Mike Voorhees: Thank you very much.

Jonathan Hollinger: I assume that was a question, not a comment?

Mike Voorhees: It was. It was a question. You know, was there more tribal engagement at that point than the record showed from the IPRA?

Jonathan Hollinger: My question to you is, would you like to direct that to the applicant?

Mike Voorhees: I think the applicant is the only one who knows what tribal engagement did occur.

Jonathan Hollinger: So. Mr. Manriquez, would you like to address that?

Manny Manriquez: I will, Mr. Chair. Thank you. It's not appropriate for me to comment on the FAA's own procedure. What I can tell you is that we follow all of the federal rules and regulations stipulated by the FAA. So I can't really speak on behalf of the FAA following their own federal rules. I do not believe there's any concern around threatening future deployment of federal resources, because the Aviation Department follows these rules to the letter. So, Mr. Voorhees, if you'd like to contact the FAA, you're welcome to do so. That said, there's 2 separate consultations we're talking about here, and I want to make sure they're not conflated. There's the FAA process as part of the 163 analysis. And then their NEPA review. We're talking a lot more in this discussion around the City's process of engagement with the tribes and pueblos. So I think a lot has already been said about that. I would... I can add more information if there's something new. But I've spoken to this matter several times, so I think I've provided what I can for the record already. Thank you.

Jonathan Hollinger: Appreciate you. Thank you for being thorough. Thank you, Mr. Voorhees. Ms. Chavez, has anyone else signed up for questions?

Christina Chavez- Gonzales: Chair, at this time, there's no further comment. If you'd like to cross-examine on this particular agenda item, please say so now. No further comment, Chair.

Jonathan Hollinger: Okay, thank you. Ms. Chavez. As a reminder, the public and my fellow commissioners, I do have a hard stop at one o'clock. My time shows 12:53. So it's just kind of a heads up. Mr. Myers, can you remind me where we are? I believe we've gone through staff and applicant closing as well as cross examination. Is that correct?

Matt Myers: Yes, I believe that's correct. So I believe you would now close the floor for Commission deliberation.

Jonathan Hollinger: Thank you. Just wanna make sure we're playing by the rules.

Gary Eyster: Eyster!

Jonathan Hollinger: Go ahead, sir!

Gary Eyster: Thanks. Thanks, Chair. I'm looking at the rules of procedure. I'm seeing cross-examination as item 5, Roman. 5. And then I'm seeing I'm not seeing that the staff or applicant have closed. Yet I could be. I could be very wrong.

Jonathan Hollinger: I think I went slightly out of order. We did, the applicant and staff closing, and I was given a friendly reminder that cross examination would probably be worthwhile. So that was my error. But I we did go through closing.

Gary Eyster: Oh, beautiful! So now, Chair, would we be doing questions to planning staff or applicant from commissioners?

Jonathan Hollinger: Mr. Myers.

Matt Myers: Yeah, yeah, I think that's I think that's fine. I... if there's still some of those, I thought maybe that had happened previously, but certainly I think if the Commissioners have questions, they can ask those of the applicant or staff.

Jonathan Hollinger: Okay, thank you. Yeah. We've heard a lot of information. Commissioner Carver, I saw your hand up.

Adrian N. Carver: Yeah, thank you, Mr. Chair. I have a proposal for how we might move forward once we're on that agenda item. May I do that?

Jonathan Hollinger: Okay, one more time. You have a proposal of how to move forward with this case. Is that what we're talking about?

Adrian N. Carver: Correct.

Jonathan Hollinger: Let's hear it, sure.

Adrian N. Carver: Okay? So I'm thinking about how to how to move forward on this and in the spirit of compromise, I think we do kind of 2 steps, the first step being to vote on the amendments that there seem to be consensus on. I have heard from both the applicant and staff that there is agreement on the lower building heights of 65, etc. I think there's a couple of more. If Staff could help us prepare a list of o those amendments that we... it seems to be... there's consensus and then from there take a second step out of respect for our relationships and to meet the request of the Pueblo of Laguna to move a continuance of 30 days on this agenda item, with the instruction that the applicant conduct additional tribal and public engagement.

Jonathan Hollinger: Making a couple of notes as you're saying that.

Tim MacEachen: Chair.

Jonathan Hollinger: Mr. MacEachen.

Tim MacEachen: So I don't know that we have consensus that we are going to lower the heights of the building. I have not heard anyone speak about that, as that's a Commissioner... I've heard concern, but no consensus. Secondly, I'd like to remind the Commission what's in front of them. There are going to be many, many more bites of the apple going forward with anything that gets built out there. So for us to just continue to kick the road... the can down the road... I don't care if the roads goes up or down. I think we've got job to do here. We've spent hours getting ready to do it, and we have try to do our job.

Jonathan Hollinger: Thank you. Vice Chair, did you have your hand up as well?

Gary Eyster: No, I did not, Chair, but I do... I can say that I like the idea that Commissioner Carver proposed, as far as continuance, because I think that the public engagement and tribal engagement is clearly incomplete, and as far as I don't like to kick cans a bit, believe me, but for myself today all I could vote would be for disapproval or continuance, because I just cannot see rushing through and create, and we are approving today. If we do these building heights until they're changed, and I... they're going to be either the 85 or the 65, whatever those are, and that's a lot. And if I saw this profile... I discussed where I was standing down at the Petroglyphs, looking up, and I didn't see much above those volcanoes. I would approve that... 85, but without that I couldn't possibly do it. So for me, it's either denial or continuance. And there's been discussions about the tribal engagement, public engagement, and then I'd want to see an instruction that involved showing us how 85, or 65, would look from the Petroglyphs.

Jonathan Hollinger: Vice Chair, so I'm gonna take a moment since I'll have to leave the meeting, and I'll hand the meeting over to Vice Chair. Ms. Chavez, I understand that you'll be leaving to a new position, so I wanted to say, thank you for all your time and effort. It's been a pleasure to work with you. Good luck with your next endeavor, and thank you for the public, all Commissioners for your engagement and your time. I'm very sorry I have to step away. This is an important case, so I leave it in good hands, and I'll see you guys next month.

Christina Chavez- Gonzales: Thank you, Chair.

Gary Eyster: Thank you, Chair. I add my commendation from Ms. Chavez. She's done a beautiful job for us. I wish her all good things. So Commission, we've heard from about 3 commissioners on how they might want to move on the case. Would any Commissioner like to comment at this point?

Daniel Aragon: Chair Aragon here. I'm not sure if it's...

Gary Eyster: Please, please, Commissioner.

Daniel Aragon: Relevant to ask this question, but can we ask Staff? Does this... did we meet the requirement? The notification requirements? Just plain out. Does it meet it, or does it not meet it?

Gary Eyster: Yes, yes, I think we could go ahead. Ms. Heyne.

Mikaela Renz-Whitmore | CABQ Planning: Catherine, do you want to take it?

Gary Eyster: Oh, I'm sorry. I'm sorry. That was Ms. Renz-Whitmore. You guys do what you like.

Catherine Heyne: As per the IDO notification, requirements were met.

Daniel Aragon: Thank you.

Gary Eyster: Any Commissioner?

Adrian N. Carver: Mr. Vice Chair, I might just clarify that I... when I said that the consensus on the changes, the amendments that.. I meant agreement between the applicant and staff. And just to say that I think that out of an abundance of respect and caution to our neighbors and relationships with the tribal communities that although the requirements have been met that and that we were not held to the new tribal consultation amendment that, we provide the applicant the opportunity to conduct that that outreach.

Gary Eyster: Thank you, Commissioner.

Renn Halstead: Halstead.

Gary Eyster: Yes, Commissioner, go ahead.

Renn Halstead: Thanks, Vice Chair. I think I definitely don't want to push this in the direction of disapproval. I think the applicant has met all the requirements. And so, if I would prefer continuance over this this being outright denied, because I think they did meet all the requirements to justify their request.

Gary Eyster: Thank you, Commissioner.

Giovanni Coppola: Commissioner Coppola.

Gary Eyster: Yes, Commissioner Coppola.

Giovanni Coppola: In the... I feel like my question would be, has the applicant met all the requirements? If first... if we decided to do a continuance, and there was more community outreach, I think some of the opposition to it is probably going to remain. We find ourselves back at the issue whenever we find it. Wouldn't we be left with the same decision we're looking to face now where they've met all the requirements and a recommendation for approval would be in front of us again? Thank you.

Adrian N. Carver: Mr. Chair.

Gary Eyster: Go ahead, Commissioner Carver.

Adrian N. Carver: I think that this is an opportunity for the applicant to work with tribal communities and negotiate any other things that they'd like to see. So I do think that there could.... there is likelihood that there is possible... Let me say there is possible... that there would be substantive changes to the plan based on that. And so it would be a different consideration for this Commission.

Gary Eyster: Thank you.

Tim MacEachen: Chair.

Gary Eyster: Was that Commissioner MacEachen?

Tim MacEachen: It is.

Gary Eyster: Go ahead!

Tim MacEachen: So. And I and I get where Commissioner Carver's trying to go. But that's not gonna happen. I mean, they're gonna... they're gonna... it's... there's very little you can do. You've got to put in scope, for there's a couple of things going on in my mind. First of all, Albuquerque always has the reputation that we're not open for business. We're not open for business because we can never get anything passed. We can never get anything approved, and that's a bad way to be, and that's a bad reputation to have. Here's an applicant that has spent a ton of time arguably reached out several meetings with several groups of people. Obviously a lot of the Pueblos knew about it. Maybe not all. But it's not going to change anything in 30 days. How anybody's stance is, we've heard it. They've met their requirements, and I think we need to at least take a straw poll on whether we want to vote.

Gary Eyster: That's reasonable. Thank you. So there's discussion of a continuance to engage in further public engagement with the community and with the tribes. There is a the point I've made where I want to see what higher buildings look like from the from the Monument. I don't hear anyone else too worried about that. But I am. And then there is a sentiment about approval today, as requested. Let's look at... Let's look at a straw poll then. And let's ask the first one, where would you stand on approving this today with the condition that we saw, which was not 85 and 60, but 65 and 48. That's what I'm gonna ask. I believe that...

Tim MacEachen: Can we ask that? Can we see if the applicant can live with those sizes?

Gary Eyster: I understood that they could. But that's a perfectly good question. Dekker, Mr. Manriquez. Would you live with 65, and 48, which Ms. Heyne had showed us was a proposed condition.

Tim MacEachen: I'd like to reword that as, could they, not would they? I mean, there's certain requirements for things they need to do, and so "could we" question not "would we" question.

Gary Eyster: I won't quibble with that. Go ahead.

Manny Manriquez: Yes, thank you, Mr. Chair, Commissioner MacEachen. Appreciate it. Yes. So we've had some extensive discussion with Planning and 65 feet is the height restriction that we arrived at, given our consideration of likely development, as well as the Mesa Film Studio Project at Double Eagle II.

Gary Eyster: Thank you, Mr. Manriquez. Ms. Heyne, is that a condition? Did you call that 6a?

Catherine Heyne: Yes, I can pull up some of the conditions of approval that we had written up since.

Dekker: And 20 min...

Catherine Heyne: The...

Dekker: I don't be...

Catherine Heyne: Just for the last...

Dekker: For...

Gary Eyster: I don't think you'd have to do that.

Catherine Heyne: See it...

Gary Eyster: I don't think you'd have to do it now, because we're looking at a straw poll. But please do get them ready. And so as far as straw poll, Commissioners, would you support approval with the condition about the 65 feet? Commissioner Coppola.

Giovanni Coppola: I would support it. Yes.

Gary Eyster: Thank you. Commissioner Halsted.

Renn Halstead: I would also support it.

Gary Eyster: Yes, Commissioner Carver.

Adrian N. Carver: I'd like to see a continuance.

Gary Eyster: That's a no. Commissioner MacEachen.

Tim MacEachen: I would support it.

Gary Eyster: That's a yes, and Commissioner Aragon.

Daniel Aragon: I would support it today. Yes.

Gary Eyster: Thank you. A chair would not support it. but the straw poll says 4 yeses. There are 6 Commissioners. So 4 is a majority. That being the case, I would move ahead and ask Ms. Heyne to show us the amended findings or conditions.

Catherine Heyne: Thank you. Vice Chair.

Renn Halstead: This is Halstead.

Gary Eyster: Yes, Commissioner.

Renn Halstead: Yeah, just while Ms. Heyne is bringing that up, I just wanted to make a comment that I think there's a lot of confusion over what is being determined in the Site Plan versus what will be determined via the Master Plan. There's a lot of discussion about building mass, and the reason that, and I think the applicant touched on this, that a lot of those studies can't be performed is that nothing about the Site Plan has any determination on the size of the buildings. It's just setting a maximum height. So we really need to divorce that part because that is coming later. All we're determining is that that max height, so I think that's an important clarification that there is... that information will be provided as part of a Master Plan effort. But that's not appropriate to be a part of the consideration here.

Gary Eyster: Thank you, Commissioner Halsted, and before we proceed, are all the Commissioners satisfied with the process that we're pursuing here? If anybody's got a problem, let me know. Counsel Myers, any concerns? Please let us know.

Matt Myers: No concerns from me. It seems like you guys are moving forward.

Gary Eyster: Thank you. Thank you very much, and I urge any Commissioner at any time to sound off, if you want to see this move a little bit differently. So, Ms. Heyne. How many conditions? I have 10 conditions.

Catherine Heyne: We have condition 6 a., 6 c., 6 d., 7 e., 7 g., Condition 8, and then a finding, and then you had...

Gary Eyster: Okay.

Catherine Heyne: Talked about 2 additional conditions.

Gary Eyster: Thank you. Thank you. Although... Okay, well, let's... so we have 10 conditions. And you're proposing revised conditions. Am I right?

Catherine Heyne: That's correct.

Gary Eyster: All right. Why don't you go through the revised conditions? Or maybe I'll... maybe I'll even... You go ahead and take a... you take a shot.

Catherine Heyne: So originally condition 6.a was building and structure height shall be limited to 36 feet at the Double Eagle Airport., except for critical aeronautical support infrastructure as such as air traffic control towers, and for these we've separated them into the eastern portion of the site and western portion of the site that is adjacent to Major Public Open Space. So for the western and northwestern boundaries with the Major Public Open Space, no development shall be allowed within 150 feet, except view fencing on the perimeter and seeding with native grass mix appropriate for the area. Between 150 and 250 feet from the western and northwestern bounding boundaries with Major Public Open Space, the maximum building height shall be 36 feet.

Gary Eyster: So the condition 6 a that you've written there in in bold, that's what's in the staff report?

Catherine Heyne: Yeah, this is in the staff report.

Gary Eyster: And now the small type is the...

Catherine Heyne: Revised.

Gary Eyster: Yes, ma'am, and go ahead on the next part, please.

Catherine Heyne: So the next part is for the eastern boundary. Along Atrisco Visca Boulevard, a setback of 150 feet shall be required. View fencing shall be allowed on the perimeter. Parking lot shall be located at least 100 feet from the eastern perimeter lot line. Landscaping shall be located at least 50 feet from the eastern perimeter lot line, except for seeding with native grassland mixes appropriate for the area. Between 150 feet and 250 feet from the lot line abutting Atrisco Vista Boulevard, the maximum building height shall be 48 feet, and...

Gary Eyster: Go ahead!

Catherine Heyne: And then more than 250 feet from any perimeter lot line, the maximum building height shall be 65 feet.

Gary Eyster: Thank you. Commissioners, yeah, I was gonna ask for any comments on that.

Renn Halstead: Yeah, I'm okay with all that. Other than I'd like to discuss the 100 foot setback for parking. The applicant had requested, I think, 25 being the preference. I would be okay with the 25. I'm not sure what requiring the 100 foot setback really does outside of impinge on the potential layout opportunities for the Master Plan. So I'd be interested to get everyone's thoughts on that.

Gary Eyster: Ms. Heyne, what about the 100 feet? Why was that 100?

Catherine Heyne: The reason being that we propose this is that to maintain more open space along the Atrisco Vista Boulevard, that is in coordination with the Major Public Open Space to the east, so those views are better protected.

Gary Eyster: So you are seeing the volcano vista as a kind of a... almost like a scenic drive or something like that, and you don't want parking right up close to it?

Catherine Heyne: That is our recommendation.

Tim MacEachen: Chair. Have we heard what the what the applicant feels about this?

Gary Eyster: No, no, we haven't. I was gonna seek that. Anything else from, I think it's okay, Applicant? What's your take?

Dekker: You want to take that Manny, or you want me?

Manny Manriquez: Please go ahead, Will. Thank you.

Dekker: We would... we would prefer to have a smaller setback for parking. We feel like parking is not going to impact the viewshed there. And so... and that you're really... you're not gaining much by...

Tim MacEachen: [inaudible]

Dekker: By limiting the... by pushing the parking toward the back, so we'd rather have it where you have 25 foot buffer of nothing, and then from 25 feet on you can have landscape and parking. The same setback for buildings.

Renn Halstead: Vice Chair, can I ask a clarifying question?

Gary Eyster: Yes, Commissioner.

Renn Halstead: To Ms. Heyne, is the... what is the setback from the... or I guess the easement for the right-of-way from the road to the actual lot line, where that 100 feet would start?

Catherine Heyne: I'm sorry. Could you repeat that? I think I might may have to defer to...

Renn Halstead: Or yeah, it might be, maybe Will can answer that as well. But what is the setback from the... there... the west side of the roadway to the lot line. Where the 100 feet would start from the lot line?

Catherine Heyne: I do not. I'm sorry. I do not know. I will defer.

Dekker: I think it's adjacent to the right-of-way, Commissioner. If I'm understanding your question correctly. The western edge of the right-of-way for Atrisco Vista is adjacent to the property line for Double eagle. So the... from the... and the right-of-way itself is, I believe, 155 feet.

Tim MacEachen: What's the road width?

Dekker: Road width. Now I I'm going to guess it's a 2 Lane road, so it's somewhere around 24, maybe 30 feet. So there's a lot of right-of-way.

Gary Eyster: Huh.

Dekker: That's not built.

Renn Halstead: Yeah. And that's some of what I'm trying to drive at is that for people visualizing again, having that clear space, there's already 150 feet on the side of the road. That would remain. you know, native grassland effectively, even if it was only the 25 feet from the lot line over. So just to make sure that people are... it have that sort of image in their head correctly, it would be more like 175 from the side of the road to the actual parking, even with the 25 foot.

Gary Eyster: Thank you, Commissioner Halsted. Do I understand correctly that that from the edge of a Atrisco Vista... I used the wrong name... from the edge of a Atrisco Vista from the paving to the lot line is currently 150? Is that what you were saying, Commissioner?

Renn Halstead: Yeah, that's my understanding, based on Dekker's information.

Gary Eyster: That that road, of course, has a potential to be widened a fair amount going forward, I guess. Yeah, that's true. I can see the benefit of having a good bit of space between the edge of the road and the parking. I don't know about 25 versus 100. Is there any Commissioner with any point of view on that?

Tim MacEachen: Chair.

Gary Eyster: Yes, Commissioner MacEachen.

Tim MacEachen: I mean, if it were me and I was the only guy voting, I'd say 25 feet's fine. I mean, there is nothing when you have a road that wide and road East and 150 feet. That is not blocking any views, and then you have your 25 feet, and then you have your parking lot. There's no views.

Gary Eyster: Okay.

Tim MacEachen: However, you know, someone wants to compromise on maybe 50 feet versus 100 feet. That's something I could probably get on board with.

Gary Eyster: Thank you. That would certainly appeal to me. Are the Commissioners generally good with 50? Any problem with 50? Please say so.

Tim MacEachen: Manny's. Manny's got his hand up.

Gary Eyster: I would call on him, but I wanted to hear from the Commission 1st. Any commissioner have a problem with 50?

Giovanni Coppola: No.

Daniel Aragon: Chair. Aragon.

Gary Eyster: Yes, Commissioner.

Daniel Aragon: I just wanted to make just wanted to make a comment. The right-of-way is 150 feet wide, and the road is built approximately in the middle of it. So there's not a... there is not a large, I mean, there's a pretty good shoulder, and as this area develops, I'm sure they're probably going to expand into that. The other thing I wanted to point out is in looking at air... I'm sitting in my office. I have access to aerial photographs. A large portion of this right-of-way is next to the runway, and I can't imagine they're going to be parking cars next to the runway. So in that area, you're looking at, you know, hundreds of feet of open space to the runways, and it's only a small portion of area that is... would be available for even this area that we're discussing in relationship to the entire boundary. It's not that big of an area. So I don't know that 50 feet or 25 feet is really going to make that big of a difference over time.

Gary Eyster: Okay, okay. thank you, Commissioner Aragon. So 50 sounds okay to you?

Daniel Aragon: Yeah.

Gary Eyster: Any other Commissioner? Hearing none. Then, Mr. Manriquez, where are you on this?

Manny Manriquez: Yes, thank you, Co-Chair Eyster. I would. I would just like to submit the Aviation Department... that we would be comfortable with the 50 foot limitation.

Gary Eyster: Thank you. Thank you very much. Then we'll change that to 50. And Ms. Heyne, would you go to condition 6 d. again? So now we've got red...

Catherine Heyne: The red is the addition. So the black is what the original stated. So originally it was started at design standards...

Gary Eyster: Okay.

Catherine Heyne: So we're not in conflict with FAA safety regulations or aviation functionality design standards regarding clustered building, conservation design shade structures. Excuse me, shade structures and shading over parking shall be added to design standards with agreed upon regulations to promote integrated site design that is considerate to the natural features and topography.

Gary Eyster: Thank you. Any problem from a Commissioner or the applicant?

Renn Halstead: Halstead.

Gary Eyster: Commissioner Halsted.

Renn Halstead: It... Yeah, thank you, Vice Chair. Is this... would this be the condition that the applicant was addressing with the request to not have a shade shaded parking requirement everywhere on the site?

Catherine Heyne: From what I understand, they said that it would be impractical in areas near certain aeronautical facilities.

Giovanni Coppola: That's...

Renn Halstead: So can we can, we potentially massage the language so that and maybe it's required in general, non-aviation parking areas or something along those lines.

Gary Eyster: Ms. Heyne., was there a place where you talked where we're talking about shade structures otherwise, or is this the only one?

Catherine Heyne: This is the only one.

Gary Eyster: So applicant has said that the shading over parking applicant has said that that's impractical for how to have shading over parking in the Aviation area.

Catherine Heyne: That is correct. We could add and shading parking shall be added... Well, this would be something that would be negotiated as well. Those particular design standards.

Gary Eyster: When?

Catherine Heyne: When something comes up to be built.

Gary Eyster: Okay.

Catherine Heyne: And if I am misstating that, please, Ms. Renz-Whitmore correct that statement.

Mikaela Renz-Whitmore | CABQ Planning: I think that's a correct statement. You could add, "with regulations agreed upon in future amendments, or as future amendments come forward" to make it clear that it's future facing. And just to say I... the addition of the red language is meant to exempt Aviation... from the aviation uses from this standard. That's why it's upfront.

Gary Eyster: So would you repeat what you said about with regulations.

Mikaela Renz-Whitmore | CABQ Planning: Sure with regular... So, instead of agreed upon regulations, just say, "with regulations to be agreed upon in future amendments, or as future amendments come forward."

Gary Eyster: As future amendments come forward and then get rid of upon regulations. Yeah.

Catherine Heyne: Should I read that again?

Gary Eyster: No, not for me. Any Commissioners, applicant, any problems there? I don't hear any. Let's go to condition 6 d. And I would like to ask Commissioners to keep track of the numbers of these revised conditions so that you could... Commissioner Carver's doing that.

Adrian N. Carver: No, no, Mr. Vice, I have a question about...

Gary Eyster: Oh.

Adrian N. Carver: 6 B

Gary Eyster: Okay, let's go back to 6 B. Thank you. Go ahead, Commissioner.

Adrian N. Carver: Does anybody have heartburn around the vagueness around the term aviation functionality.

Gary Eyster: Oh. oh, 6 c. Excuse me!

Adrian N. Carver: It... like I don't. I don't know what that means.

Gary Eyster: Is there a better way to state that Ms. Heyne, Ms. Renz-Whitmore?

Catherine Heyne: We could potentially delete it.

Giovanni Coppola: Could we just put FAA safety rules and regulations?

Adrian N. Carver: I'm not sure I'm making a recommendation. I'm just I don't exactly know what that means, and I'm trying to help the applicant accomplish what they are trying to accomplish with this change. Yeah. And but that just seems overly broad to me.

Mikaela Renz-Whitmore | CABQ Planning: Mr. Chair. Commissioners, can I just say that it's up to the applicant to propose what comes forward. So they have the 1st bite at the apple of saying this does impact aviation functionality. So we're not meeting the condition on the site plan. And 2, you have the second bite at that apple as they come forward with what's proposed. And you say, actually, no, that doesn't impact your functionality, you have to meet. So from our perspective, it's okay for it to be broad, because again, anything that's proposed in the future comes back to you, and you'll have the chance to determine whether that meets aviation functionality or not.

Adrian N. Carver: Okay, thanks. Let's leave it.

Gary Eyster: Thank you, Commissioner, and that could also provide an opportunity for the aviation community to comment on ideas that might impact aviation functionality. Then conditions 6 d. Used to say, solar hook up shall be required for all new development. And we're going to remove 6 d. And.

Catherine Heyne: Added.

Gary Eyster: Add a finding, add a finding solar power. Other forms of alternate energy shall be encouraged for all new development. Any problem there? Hearing none, then, so condition 6 d is removed, or can we just leave it as revised? We'll say revised. And the revision is to remove it. 7 E. Says, now... A problem there, Commissioner Carver?

Adrian N. Carver: Yeah. Sorry. Vice Chair. the word alternate energy. I...

Catherine Heyne: Alternative. Sorry. That's a typo.

Gary Eyster: Thank you. Commissioner.

Adrian N. Carver: Even if it's alternative. I think the intention here is that it is... It's a renewable energy or...?

Catherine Heyne: Renewable energy.

Gary Eyster: So, Ms. Heyne? Is it renewable? Is that....

Catherine Heyne: Renewable. Yes.

Gary Eyster: Good.

Catherine Heyne: Thank you, Commissioner Carver.

Gary Eyster: Applicants good with renewable shall be encouraged?

Manny Manriquez: Yes.

Gary Eyster: Thank you. Thank you. If we're ready to move on, then Condition 7 d now says wall mounted signage dimensions shall be provided, and condition 7 G says height dimensions to freestanding signs shall be added to a certain sheet. But we want to say... staff wants us to say will be condition 7 e., and 7 g. will be removed, and...

Catherine Heyne: We put...

Gary Eyster: With a replaced... with a finding saying that existing wall mounted and freestanding signs may not be in compliance with current IDO regulations. If nonconforming all future changes shall comply with IDO regulations pursuant to those citations, nonconforming signs. New development will comply with IDO or DEII design standards. Does that look okay to Commissioners and applicant?

Tim MacEachen: Chair. 1 item I would like to see Ms. Heyne keep track of what... when we make a motion for this... what the conditions and findings are, because none of us are going to get it right.

Gary Eyster: Thank you.

Tim MacEachen: Conditions and new findings are so you can just kind of keep score at home and give us guidance when we get to that point, Ms. Heyne. I think that'd be very helpful.

Catherine Heyne: Okay. I'll do my best.

Gary Eyster: Thank you. Thank you. I've been jotting them down as we go, but I'm not the one to make the motion. I don't hear any objections to removing condition 7 e. and 7 g. And providing a finding instead. Condition 8 t this time says, development shall not occur east of the right-of-way of Atrisco Vista Boulevard. But we want we want to remove the red part. Ms. Heyne?

Catherine Heyne: That is correct.

Gary Eyster: So development shall not occur east of the right-of-way of Atrisco Boulevard. But we're going to remove and adjacent to Major Public Open Space or within 330. We're just say we just wanted to say, not east of the right-of-way of Atrisco Vista.

Catherine Heyne: That is...

Gary Eyster: Check the other.

Catherine Heyne: 2 were taken care of up above.

Gary Eyster: So when we say not occur east of the right-of-way, do we mean the entire area within the right-of-way?

Catherine Heyne: Yes, yes, sir, that, yes, Vice Chair. That's there's gonna be no development to the East in any portion of the Double Eagle Airport to property line east of the Atrisco Vista Boulevard.

Gary Eyster: Okay, okay. so it refers to the east side of a Atrisco Vista?

Catherine Heyne: Correct.

Gary Eyster: Does. Is that clear to everybody? Is that satisfactory? Now we move to findings. Finding 15 used to say because the addition of fencing opaque, you use just opaque, you changed to view fences. Opaque fencing should retain the feeling that should say view fencing?

Catherine Heyne: Correct.

Gary Eyster: All good. No objections from Commission or applicant. Okay, so we're down to the end of the revised conditions and findings, findings and conditions. There was a condition that Commissioner Carver talked about, and I talked about it a bit around consultation with the community and with tribes, but that was going to be more if we were going to continue. Am I right?

Matt Myers: It's...

Gary Eyster: So we're not putting a condition on that. If we're going to approve this today.

Giovanni Coppola: Correct.

Gary Eyster: And then this other one is kind of the same. If we approve today, we don't need to see an analysis of the view plane from the pueblos, or from the volcanoes or from the monument, which was what I was asking for. But I... so I don't think we need that today, either, do we?

Giovanni Coppola: Nope.

Gary Eyster: But, I would say that when the applicant comes in with modifications or with actual new facilities. As a Commissioner, I would sure like to see that that profile of what that view looks like from various places in the in the monument. So there's nothing further for today. And Ms Heyne, I'm... what I'm saying, I think is just for the ears of the applicant and any app... any of his consultants going forward. It doesn't have to be written here for me. Yes, Commissioner.

Renn Halstead: Thank you, Mr. Chair. Might that work as a finding? Just so it's documented.

Gary Eyster: Well, thank you, I think it... There could be a an additional finding that says that the Commission indicated a desire to... let's see... a graphic representation of how oh, building heights would affect views from the monument.

Renn Halstead: Yeah, I think that would be a great addition to any master plan. Our future amendment submittals definitely.

Gary Eyster: You. Thank you. It would certainly help me, or people who think like me. So it's really future site plan, future site plan applications: Commission expressed a desire to have profiles showing the impacts of buildings on views from the Petroglyph National Monument.

Catherine Heyne: So profiles showing building height impacts.

Gary Eyster: From... and that side from Pueblos. I'm not clear on if pueblos are close enough to be impacted by building heights.

Catherine Heyne: So are you wanting height impacts from Petroglyph National Monument?

Gary Eyster: That was what I was wanting. But I don't know if there's things that others wanted. If it looks okay to everyone, yeah, I would just put the S on applications. And that would be an added finding. Did we have 21 findings?

Catherine Heyne: We had, I think it was, 21 findings.

Gary Eyster: So that would be an additional finding. 22.

Catherine Heyne: And I can double...

Mikaela Renz-Whitmore | CABQ Planning: Catherine, I think that would be actually a new 25 because we added 2 above when we removed conditions. So I've been keeping track, and we've got 22, 23 above, and this would be 24.

Catherine Heyne: Would... because the conditions E and G. Those would have been under 7, as a single condition.

Mikaela Renz-Whitmore | CABQ Planning: But we replace them with a finding, so that would be a new finding number. So if you go above just real quick. The 1st one that we did was solar becomes a new finding. So that's going to be 23. And then there's... so that was for 60, and then 7 G, that would be a new sorry that one's gonna be 22, Catherine.

Catherine Heyne: Which one, this one up here? Yeah. Sorry.

Mikaela Renz-Whitmore | CABQ Planning: Because you had 21 in the original staff report. So that's 22, the next one under 7 E. And 7 g. Oops go back up is going to be 23, and then the one you've just written would be 24, and for the Commissioners I am putting together a slide with all of these revisions that we can show when you're ready to make a motion.

Gary Eyster: Thank you, Ms. Renz-Whitmore. Is there any further discussion from the Commission about any finding or condition?

Giovanni Coppola: Oh!

Gary Eyster: More. Ms. Heyne, Ms. Renz-Whitmore, would you like a 5 min, 10 min break to assemble that and get that where you're comfortable?

Gary Eyster: We'll break. You won't break.

Mikaela Renz-Whitmore | CABQ Planning: Catherine, do you need it? I'm ready if you.

Catherine Heyne: Are you ready? I am ready.

Mikaela Renz-Whitmore | CABQ Planning: Okay, let me share my screen for the Commissioners.

Catherine Heyne: Let me stop sharing.

Mikaela Renz-Whitmore | CABQ Planning: So, everybody ready. So this would be approval of the case with findings 1 through 24, with a new find... with new findings 22, 23, 24 as read into the record, revised finding number 15 as read into the record, revised conditions 6a., 6c., 6d., and 9 removed conditions 7e. 7g.

Giovanni Coppola: I'm prepared to make the motion.

Mikaela Renz-Whitmore | CABQ Planning: One second, Catherine.

Catherine Heyne: I think that's condition 8 was revised not 9.

Gary Eyster: That is great. We need you to join the commission.

Mikaela Renz-Whitmore | CABQ Planning: You have. You have me til February.

Gary Eyster: What? Which district do you live in.

Tim MacEachen: Seat.

Mikaela Renz-Whitmore | CABQ Planning: I'm Megan. I'm Megan. I'm Megan.

Gary Eyster: Yeah, yeah, you. Oh, that's that sounds like a good novel or some TV show. Well, would any Commissioner care to move?

Giovanni Coppola: Mr. Vice Chair, Acting Chair. I will give this my best shot if we're ready.

Gary Eyster: Please go ahead. Commissioner Coppola.

Giovanni Coppola: Okay in the matter today, I move motion for approval of project number 2018-001577, case number, SI- 2024-00994, with findings, 1 through 24, with new findings. Number 22, 23, and 24 as read into the record and revised. Finding number 15 as read into the record along with revised conditions. 6a, 6c, 6d And 8, removing conditions, 7e, And 7g.

Tim MacEachen: Wow! I'm impressed.

Gary Eyster: Good job.

Tim MacEachen: Yeah.

Gary Eyster: Okay, so thank you. Commissioner Coppola. Commissioner MacEachen seconds of the motion. Is there any discussion? Yes, go ahead, Commissioner Halsted.

Renn Halstead: Thank you, Vice Chair. I just wanted to make sure that we were okay with that, because we didn't mention conditions 1 through 10 with the revised conditions.

Gary Eyster: I think Mr. Myers will say that if. since you've made that point that we're.

Giovanni Coppola: If not, I can... I can rerun it real fast.

Gary Eyster: Perhaps, Commissioner Coppola, you could just state the findings that we already had, or conditions that we already had.

Giovanni Coppola: In addition to my initial motion. a motion for approval at adding conditions, 1 through 10, along with the revised conditions and remove conditions that I mentioned in my previous motion.

Gary Eyster: Does that sound good, Mr. Myers?

Matt Myers: Sounds good to me. I recently...

Tim MacEachen: Second.

Gary Eyster: Okay, thank you. Commissioners. Any discussion of the motion?

Adrian N. Carver: Mr. Vice Chair.

Gary Eyster: Yes, Commissioner Carver.

Adrian N. Carver: I'd just say that. I'm while I'm generally supportive of the application, I'll be voting against you to the tribal notification. Our tribal engagement piece that we spoke about earlier.

Gary Eyster: Thank you. Thank you. I'm going to see how the vote goes. I think that the coordination communication with the community that I've seen from the Aviation Department. That leaves a lot to be desired. I think it was is so poor that it would warrant a continuation. But let's go ahead and call a vote if there's no more discussion. Commissioner Coppola.

Giovanni Coppola: Coppola, aye.

Gary Eyster: Commissioner Halsted.

Renn Halstead: Halstead, aye.

Gary Eyster: Commissioner, Carver.

Adrian N. Carver: Carver, no.

Gary Eyster: Commissioner MacEachen.

Tim MacEachen: MacEachen, aye.

Gary Eyster: Commissioner, Aragon.

Daniel Aragon: Aragon, aye.

Gary Eyster: Chair votes no. Motion passes 4 to 2. That completes that request today. I'd like to thank the applicant, the staff. the members of the community who all participated. You all did a beautiful job of making yourselves heard, and your contributions made a big difference. Thank you for being here. That concludes project number 3, agenda item 3.



ENVIRONMENTAL PLANNING COMMISSION AGENDA

THURSDAY, AUGUST 15, 2024
8:40 A.M.

Location: ZOOM

Members of the public may attend via the web at this address: <https://cabq.zoom.us/j/88163635817> or by calling the following number: (719) 359-4580 and entering Meeting ID: 881 6363 5817

MEMBERS

Jonathan R. Hollinger, Chair
Gary L. Eyster P.E. (Ret.), Vice Chair

Giovanni Coppola
Joseph Cruz
Renn Halstead
Adrian Carver

Tim MacEachen
Jarrod Likar
Daniel Aragon

NOTE: A LUNCH BREAK AND/OR DINNER BREAK WILL BE ANNOUNCED AS NECESSARY

Agenda items will be heard in the order specified unless EPC approves changes at the beginning of the hearing; requests for deferral or withdrawal are reviewed at the beginning of the hearing.

TESTIMONY: Please be prepared to provide brief and concise testimony to the Commission if you intend to give comments. **In the interest of time, presentation times are limited as follows, unless otherwise granted by the Commission Chair: Staff – 5 minutes; Applicant – 10 minutes; Public speakers – 2 minutes each, City-recognized neighborhood association or coalition representatives – 5 minutes.**

CROSS-EXAMINATION: For quasi-judicial decisions, applicants and members of the public with legal standing have a right to cross-examine other persons speaking pursuant to Article 3, Section 2D, of the EPC Rules of Practice & Procedure.

48-HOUR MATERIAL: The EPC will only consider very limited, clarifying written material if it has been submitted to the EPC at least 48 hours prior to the public hearing and posted for public review. Except in extraordinary circumstances, the EPC will not consider written materials submitted at the hearing. In the event the EPC believes that newly submitted material may influence its final decision, the application may be deferred or continued to a subsequent hearing.

PUBLIC COMMENT: All public comments should be submitted via an online survey, which can be accessed at: <https://cabq.gov/epc-public-comment>. All comments received prior to the 48-hour deadline will be included with the record. All other comments should be given verbally during the hearing.

NOTE: ANY AGENDA ITEMS NOT HEARD BY 8:30 P.M. MAY BE DEFERRED TO ANOTHER HEARING DATE AS DETERMINED BY THE PLANNING COMMISSION

Call to Order:

- A. Pledge of Allegiance
- B. Roll Call of Planning Commissioners
- C. Zoom Overview
- D. Announcement of Changes and/or Additions to the Agenda
- E. Swearing in of City Staff

1. Project # PR-2024-010612

RZ-2024-00035– Zoning Map Amendment
(Zone Change)

JAG Planning & Zoning LLC, agent for Lolo Chavez, requests a Zoning Map Amendment from R-A to R-1C, for all of or a portion of Lot 14, Quaker Heights Addition, located at 4815 Northern Trail NW, at the SW intersection of Westward Lane NW and Northern Trail, approximately 1 acre.

(F-11)

Staff Planner: Jude Miller

2. Project # PR-2024-010617

RZ-2024-00037– Zoning Map Amendment
(Zone Change)

YES Housing Inc., requests a Zoning Map Amendment from MX-L to MX-M for all or a portion of the southerly portion of Tract 162A, Airport Unit of Atrisco Grant, Tract 163, excluding the Northeasterly portion & the southeasterly portion to the right-of-way, Tract 164, 10N 2E SEC15, and Tract 165, excluding the Southwesterly Portion to the right-of-way, the Airport Unit of Town of Atrisco Grant, located at 701 Coors Blvd. NW, between Fortuna Rd NW and Glenrio Rd. NW, approximately 15 acres.

(J-10, J-11)

Staff Planners: Leslie Naji & Nasima Hadi

3. Project # PR-2018-001577

SI-2024-00994 – Site Plan – EPC

Dekker, agent for the City of Albuquerque Aviation Dept., requests an NR-SU Site Plan – EPC for the DEII Airport for all or a portion of Tract A-1, Plat of Tract A-1 & Tract L-1, Parcels 1-5, Double Eagle II Airport; Tract C and Parcel-1 Tract M Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; the Southeasterly Portion of Tract D-1-A-1, Tract D-1-A-2, and Tract S-1-A Plat of Tracts D-1-A-1, D-1-A-2, & S-1-A of Amended Bulk Land Plat for Aerospace Technology Park; Tract E, F, and K Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; Tract N-1 and Tract N-2 AKA Parcel 4 Bulk Land Plat of Tracts N-1, O-1, & N-2 Parcels 1, 2, 3, & 4 Double Eagle II Airport & Adjacent Lands; and Tract S-2 Bulk Land Plat for Aerospace Technology Park, Tracts D & S of Double Eagle II Airport & Adjacent Lands, located at 7401 Paseo Del Volcan NW, NW of the Atrisco Vista Blvd NW and Shooting Range Access Rd. intersection, approximately 4,200 acres.

(C-4, C-5, C-6, D-4, D-5, D-6, E-4, E-5, E-6, F-4, F-5, F-6, G-4, G-5, G-6)

Staff Planners: Catherine Heyne and Megan Jones

4. OTHER MATTERS:

A. Approval of Action Summary Minutes for:

- June 20, 2024
- July 18, 2024

B. Announcements

5. ADJOURNMENT



ENVIRONMENTAL PLANNING COMMISSION ACTION SUMMARY

Thursday, August 15, 2024
8:40 a.m.

Members of the public may attend via the web at this address: <https://cabq.zoom.us/j/88163635817> or by calling the following number: 1 301 715 8592 and entering Meeting ID: 881 6363 5817

COMMISSIONER MEMBERS PRESENT:

Jonathan R. Hollinger Chair
Gary L. Eyster, P.E. (Ret.) Vice Chair
Tim MacEachen
Renn Halstead
Daniel Aragon
Adrian Carver
Jarod Likar
Giovanni Coppola

COMMISSIONER MEMBERS ABSENT:

Joseph Cruz

Call to Order: 8:41 am

- A. Pledge of Allegiance
- B. Roll Call of Planning Commissioners
- C. Zoom Overview
- D. Announcement of Changes and/or Additions to the Agenda
- E. Swearing in of City Staff

1. Project # PR-2024-010612

RZ-2024-00035– Zoning Map Amendment (Zone Change)

JAG Planning & Zoning LLC, agent for Lolo Chavez, requests a Zoning Map Amendment from R-A to R-1C, for all of or a portion of Lot 14, Quaker Heights Addition, located at 4815 Northern Trail NW, at the SW intersection of Westward Lane NW and Northern Trail, approximately 1 acre.

(F-11)

Staff Planner: Jude Miller

A motion was made by Commissioner Halstead and Seconded by Commissioner Aragon that this matter be Approved. The motion carried by the following vote:

For 8: Aragon, Carver, Likar, Coppola, Eyster, Halstead, MacEachen, Hollinger

Absent 1: Cruz

2. Project # PR-2024-010617

RZ-2024-00037– Zoning Map Amendment (Zone Change)

YES Housing Inc., requests a Zoning Map Amendment from MX-L to MX-M for all or a portion of the southerly portion of Tract 162A, Airport Unit of Atrisco Grant, Tract 163, excluding the Northeasterly portion & the southeasterly portion to the right-of-way, Tract 164, 10N 2E SEC15, and Tract 165, excluding the Southwesterly Portion to the right-of-way, the Airport Unit of Town of Atrisco Grant, located at 701 Coors Blvd. NW, between Fortuna Rd. NW and Glenrio Rd. NW, approximately 15 acres.

(J-10, J-11)

Staff Planners: Leslie Naji & Nasima Hadi

A motion was made by Commissioner Coppola and Seconded by Commissioner MacEachen that this matter be Approved. The motion carried by the following vote:

For 8: Aragon, Carver, Liker, Coppola, Eyster, Halstead, MacEachen, Hollinger

Absent 1: Cruz

3. Project # PR-2018-001577

SI-2024-00994 – Site Plan – EPC

Dekker, agent for the City of Albuquerque Aviation Dept., requests an NR-SU Site Plan – EPC for the DEII Airport for all or a portion of Tract A-1, Plat of Tract A-1 & Tract L-1, Parcels 1-5, Double Eagle II Airport; Tract C and Parcel-1 Tract M Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; the Southeasterly Portion of Tract D-1-A-1, Tract D-1-A-2, and Tract S-1-A Plat of Tracts D-1-A-1, D-1-A-2, & S-1-A of Amended Bulk Land Plat for Aerospace Technology Park; Tract E, F, and K Bulk Land Plat of Double Eagle II Airport & Adjacent Lands; Tract N-1 and Tract N-2 AKA Parcel 4 Bulk Land Plat of Tracts N-1, O-1, & N-2 Parcels 1, 2, 3, & 4 Double Eagle II Airport & Adjacent Lands; and Tract S-2 Bulk Land Plat for Aerospace Technology Park, Tracts D & S of Double Eagle II Airport & Adjacent Lands, located at 7401 Paseo Del Volcan NW, NW of the Atrisco Vista Blvd. NW and Shooting Range Access Rd. intersection, approximately 4,200 acres.

(C-4, C-5, C-6, D-4, D-5, D-6, E-4, E-5, E-6, F-4, F-5, F-6, G-4, G-5, G-6)

Staff Planners: Catherine Heyne and Megan Jones

A motion was made by Commissioner Aragon and seconded by Commissioner MacEachen that this matter be Approved. The motion carried by the following vote:

**For 4: Aragon, Coppola Halstead, MacEachen
Against 2: Eyster, Carver
Absent 3: Cruz, Liker, Hollinger**

7. OTHER MATTERS

A. Approval of Action Summary Minutes

- June 20, 2024

A motion was made by Commissioner Carver and seconded by Commissioner Halstead that this matter be Approved. The motion carried by the following vote:

**For 6: Coppola, Halstead, Carver, MacEachen, Aragon, Eyster
Absent 3: Hollinger, Liker, Cruz**

- July 18, 2024

A motion was made by Commissioner Carver and seconded by Commissioner Halstead that this matter be Approved. The motion carried by the following vote:

**For 6: Coppola, Halstead, Carver, MacEachen, Aragon, Eyster
Absent 3: Hollinger, Liker, Cruz**

B. Announcements

- EPC
 - **Establishing an EPC Task Force to review and recommend updates to EPC Rules of Conduct was discussed. The Commission will meet on this again at next month's hearing.**
 - **Ms. Renz-Whitmore announced EPC Senior Administrative Assistant Nichole Maher will be taking over as EPC Hearing Monitor for Christina Chavez.**

8. ADJOURNMENT: 1:51 P.M.



NOTICE OF APPEAL

September 3, 2024

TO WHOM IT MAY CONCERN:

The Planning Department received an appeal on August 29, 2024. You will receive a Notice of Hearing as to when the appeal will be heard by the **Land Use Hearing Officer**. If you have any questions regarding the appeal, please contact Nichole Maher, Planning Sr. Administrative Assistant, (505) 924-3845 or nmaher@cabq.gov.

Please refer to the enclosed excerpt from the Land Use Hearing Officer Rules of Procedure and Qualifications for any questions you may have about this procedure.

Any questions you might have regarding Land Use Hearing Officer policy or procedures that are not answered in the enclosed rules can be answered by Michelle Montoya, Clerk to the City Council, (505) 768-3100 or mmmontoya@cabq.gov.

CITY COUNCIL APPEAL NUMBER: AC-24-26

PLANNING DEPARTMENT CASE FILE NUMBER:

PR-2018-001577, SI-2024-00994 (Site Plan – EPC), VA-2024-00260 (Appeal)

APPLICANT: NAEVA and All Applicants listed on attendance list

**Hessel E. Yntema, III
Yntema Law Firm, P.A.
215 Gold Ave. SW, Suite 201
Albuquerque, NM 87102**

CC: City Aviation Department, Manny Manriquez, mmanriquez@cabq.gov
Dekker, Will Gleason, WillG@dpsdesign.org
Dekker, Jessica Lawlis, JessicaL@dpsdesign.org
Dekker, Rebecca Shank, RebeccaS@dpsdesign.org
City Aviation Department, Christina Sandoval, cmsandoval@cabq.gov
Bernadette Hardy, Bernadette@naeva.org
Alysia Coriz, alysia@naeva.org
Baxter Sosebee, mb422@comcast.net
Elizabeth Haley, elizabethkayhaley@gmail.com
Evelyn Rivera, rioreal@earthlink.net
Jane Baechle, jane.baechle@gmail.com
Santa Fe Village Neighborhood Association, SfVNA2014@gmail.com
Lan Sena, lan@civicpolicy.com
René Horvath, aboard111@gmail.com

Mike Vorhees, mike@cyonic.com
National Park Service, Nancy Hendricks, nancy_hendricks@nps.gov
Pueblo of Laguna, Kenneth Bobroff, kbobroff@pol-nsn.org
City Office of Equity and Inclusion, Terry Sloan, terrysloan@cabq.gov
Alan Varela, avarela@cabq.gov
Andrew Coon, acoon@cabq.gov
Crystal Ortega, cortega@cabq.gov
Isaac Padilla, iepadilla@cabq.gov
James Aranda, jmaranda@cabq.gov
Jessica Enriquez, jenriquez@cabq.gov
Julia Ronquillo, julia@cabq.gov
Julian Moya, julianmoya@cabq.gov
Kevin Morrow, kmorrow@cabq.gov
Matthew Cox, mcox@cabq.gov
Michelle Montoya, mmmontoya@cabq.gov
Mikaela Renz-Whitmore, mrenz-whitmore@cabq.gov
Michael Vos, mvos@cabq.gov
Omega Delgado, odelgado@cabq.gov
Steven Chavez, steven@stevenchavezlawfirm.com
Vincent Higgins, vhiggins@cabq.gov
EPC File