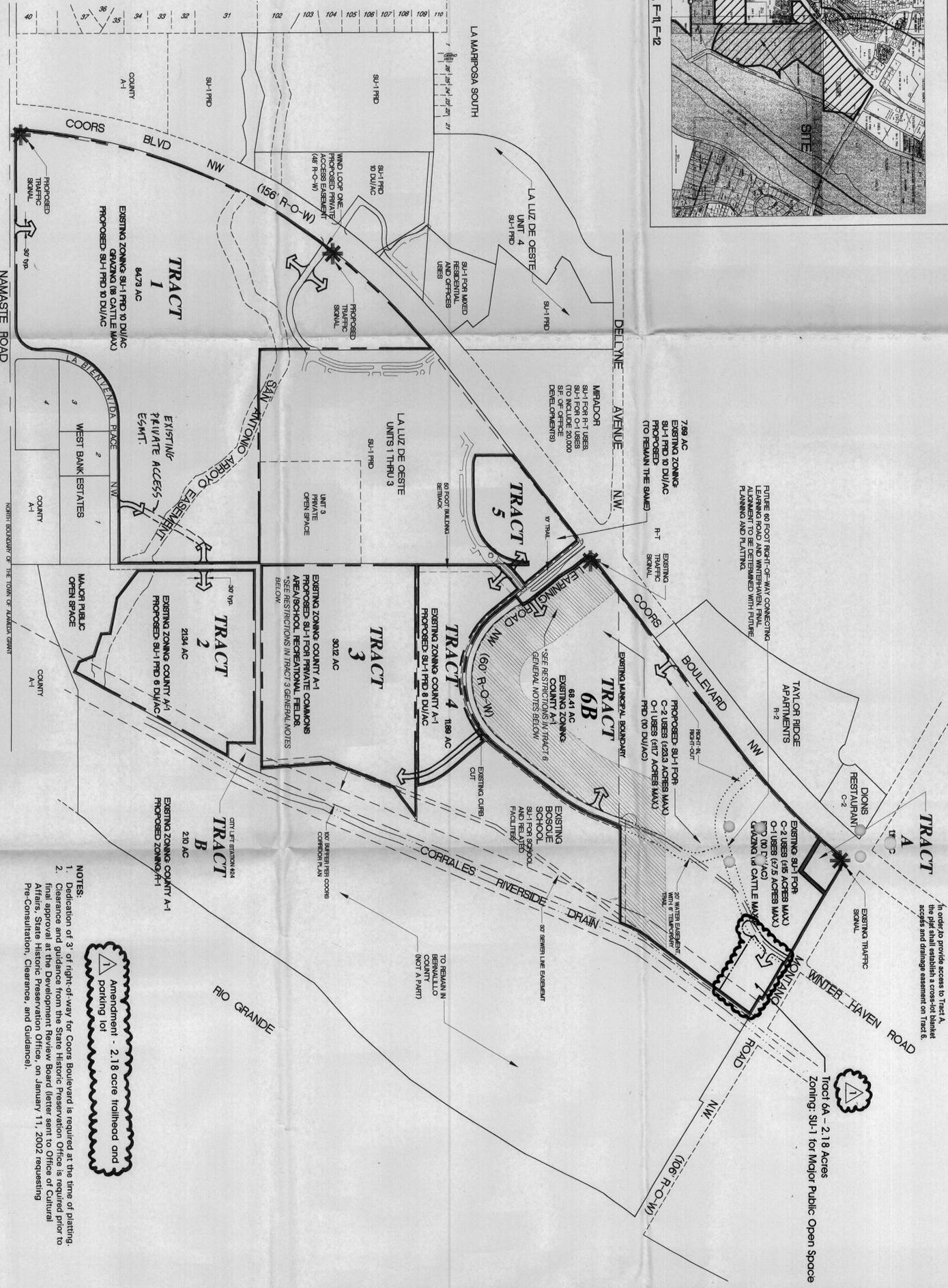
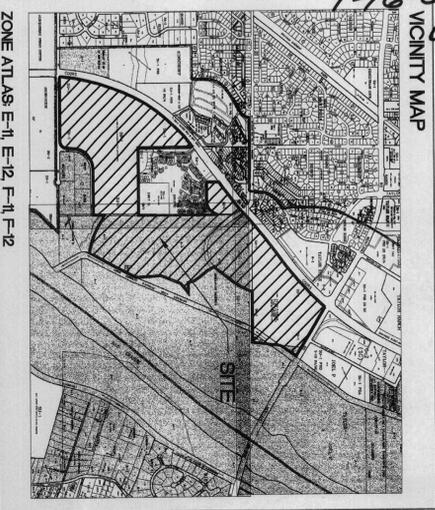


1200965
1-16-03
E-12



GENERAL NOTES:

Currently, all of the land covered by this Site Plan for Subdivision is unplatted. The Tract boundaries were created through actions by others. The goal is to: (1) annex Bernillo County land to the City of Albuquerque and to establish zoning consistent with City goals and policies contained in the Coors Corridor Plan, West Side Strategic Plan, and the Comprehensive Plan; and (2) plat all of these tracts. Details regarding each tract are provided below:

- TRACT 1:** This tract contains approximately 84.7 acres that was annexed to the City of Albuquerque by the State Boundary Commission (AX-85-1). It is bound by La Luz de Oeste to the north and east, Coors Boulevard to the west, and Namaste Road and La Bienvenida Place rights-of-way to the south and east. The San Antonio Arroyo runs through the tract from west to east. A proposed 48' private access easement (Wind Loop One) also runs through the tract from Coors Boulevard to La Luz to provide traffic signal access per the Coors Corridor Plan. The existing zoning is SU-1 for PFD 10 du/ac max. and no change is proposed.
- TRACT 2:** This tract contains approximately 21.3 acres and is currently in Bernillo County. It is bound by the City lift station right-of-way to the north and west, Corrales Riverside Drain to the east and the San Antonio Arroyo Easement to the south. The existing zoning is County A-1 and the proposal is to annex it to the City and establish SU-1 PFD 8 du/ac. Access to Tract 2 shall be provided at the time the Development Review Board approves the plat and this Site Development Plan for Subdivision.
- TRACT 3:** This tract contains approximately 30.1 acres and is currently in Bernillo County. It is bound on the north by vacant County A-1 land (proposed SU-1 PFD 8 du/ac), Bosque School to the east, City-owned road to Learning Road to the south and La Luz to the west. The existing zoning is County A-1. The property is proposed for annexation and establishment of SU-1 for Private Commons Area and School Recreational Fields. School Recreational Fields shall be limited to an area of 240' X 390' in the northeastern portion of Tract 3. No parking or field lighting is permitted in the vicinity of the Recreational Fields.

TRACT 4: This tract contains approximately 11.9 acres within Bernillo County. It is bound by Learning Road to the north, La Luz to the west, Bosque School to the east, and vacant County A-1 land (proposed for SU-1 for Open Space/School Recreational Fields) to the south. The existing zoning is County A-1 and the proposal is to annex it to the City and establish SU-1 PFD 8 du/ac zoning.

- TRACT 5:** This tract contains approximately 7.7 acres and was annexed to the City of Albuquerque by the State Boundary Commission (AX-85-1). It is bound by Coors Boulevard to the west, Learning Road to the north, La Luz de Oeste Units 1-3 to the south, and the La Luz Connection Road to the east. Existing zoning is SU-1 PFD 10 du/ac max. and no change is proposed.
- TRACT 6:** This tract contains approximately 70.6 acres with a portion within Bernillo County and another portion within the City. It is bound by Learning Road and Bosque School to the south, Coors and Montana Roads to the north and west, and Bosque School and the Rio Grande Bosque to the east. Approximately 23.3 acres are zoned County A-1. The proposal is to complete the annexation of the property to the City and establish SU-1 for C-2 Uses (23.3 acres max.); O-1 Uses (11.7 acres max.); and PFD (10 du/ac). The area of Tract 6 adjacent to Learning Road and the northern boundary of Bosque School (a minimum of 300 feet) shall be restricted to office and residential uses (PFD & O-1).
- TRACT 7:** This tract contains approximately 1.2 acres and was annexed to the City of Albuquerque by the State Boundary Commission (AX-85-1). It is located at the southeast corner of Coors Boulevard and Montana Road. Existing zoning is SU-1 for C-2 Uses (± 23.3 acres max.), O-1 Uses (11.7 acres max.) and PFD (10 du/ac), consistent with zoning in Tract 6.
- TRACT 8:** This tract contains approximately 2.1 acres and is owned by the City of Albuquerque, Lift Station #24. It is existing A-32 private access easement provides access to the City and establish R-1 zoning.

- NOTES:**
- Dedication of 3' of right-of-way for Coors Boulevard is required at the time of platting.
 - Elevation and guidance from the State Historic Preservation Office is required prior to platting. All platting shall be done in accordance with the State Historic Preservation Office, on January 11, 2002 requesting Pre-Consultation, Clearance, and Guidance.

Amendment - 2.18 acre toolhead and pointing lot

SITE PLAN FOR SUBDIVISION - REQUIRED INFORMATION

THE SITE: The site consists of unplatted land, approximately 230.8 acres, with a portion in the City of Albuquerque and a portion in Bernillo County. The portion in Bernillo County is proposed for annexation and establishment of zoning.

PROPOSED USE: The proposed zoning for the annexation parcels is SU-1 for O-1, C-2, PFD (varying densities), and Private Commons Area/School Recreational Fields. The parcels in the City of Albuquerque are proposed to remain the same relative to zoning and land use.

PEDESTRIAN AND VEHICULAR INGRESS AND EGRESS: VEHICULAR ACCESS: Two new roads have been constructed (Learning Road and La Luz Connection Road) and one new 48' foot private access easement is planned (Wind Loop Road) to provide vehicular ingress and egress to these parcels, to increase safety to existing development, and to be consistent with City policies contained in the Coors Corridor Plan. Two additional right-in/right-out access points onto Coors Boulevard are proposed between Learning Road and Montana Road. Learning Road is a signalized intersection and Wind Loop Road is proposed to be a signalized intersection.

BICYCLE AND TRAIL ACCESS: Bicycle access is provided by 6 foot on-street bike lanes in Learning Road, Learning Road trail (a 10 foot trail within a 20 foot landscape easement), and a 6 foot temporary trail to connect the northern end of Learning Road to Montana Road.

TRANSIT ACCESS: Coordination with the City Transit Department shall be initiated at the Site Plan for Building Permit to provide access and service to this property. Coors Boulevard is a major transit route on the West Side.

INTERNAL CIRCULATION REQUIREMENTS: Conceptual access points (30 feet in width) have been identified on this Site Plan for each of the proposed parcels; final locations, width, and configuration shall be determined with future Site Plans for Building Permit with approval by the City Engineer. Internal sidewalks and/or trails shall be provided within each parcel with future Site Plans for Building Permit.

BUILDING HEIGHTS AND SETBACKS: See Sheet 2 of 2, Design Standards. Building height shall be consistent with the Coors Corridor Plan and should be kept to a minimum, with the majority of the buildings to be one-story.

MAXIMUM FAR: A maximum floor area ratio (FAR) shall be .35 for the SU-1 for C-2 and SU-1 for O-1 portions of Tract 6.

LANDSCAPE PLAN: The Design Standards (see Sheet 2 of 2) provide for preservation of significant cottonwoods, emphasis on native and naturalized plant species, landscape criteria, and landscape buffers. Subsequent landscape plans shall be consistent with City standards and policies regarding water conservation.

APPROVALS

Project #	1000965	DB#	02500 - 00116
EPC#	0028-0000-0743		

This Site Plan for Subdivision is consistent with the Site Plan approved by the Environmental Planning Commission on March 22, 2001 and Conditions of Approval have been met.

Utility Department	Date
Planning Director	2/22/02
Transportation Development	2-19-02
City Engineer/MANAR	2-6-02
Utility Department	2/6/02
Parks and Recreation Department	2/6/02

RAY A. GRAHAM III

Site Plan for Subdivision
LANDS OF

Prepared For: Ray A. Graham III
1741 1st Avenue NW
Albuquerque, New Mexico 87120

Prepared By: Consensus Planning, Inc.
924 Park Avenue SW
Albuquerque, New Mexico 87102

Bordenave Designs
P.O. Box 91194
Albuquerque, New Mexico 87199

Scale 1"=300'

PLANNING

CONSENSUS

150' 0 300' 600'

January 22, 2002

Sheet 1 of 2

Design Standards

The purpose of these Design Standards is to provide a framework to assist the architects, landscape architects, and designers in understanding the vision and development goals for the property. The primary goal for this property is to achieve a vibrant, mixed-use community that fosters pedestrian accessibility and maintains a Village-Type character.

The Design Standards should be used to facilitate the design of buildings which respect the natural conditions of the site, maintain and highlight the spectacular views of the Santa and Marzano Mountains, and to leave significant areas dedicated to open space. Innovative techniques such as cluster housing, water harvesting, and use of "green" building materials are strongly encouraged.

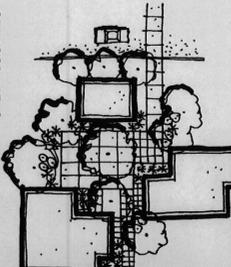
These standards address the issues of landscape, setbacks, pedestrian amenities, screening, lighting, signage, and architecture that will create the visual image desired for the Graham property. They are intended to be complementary to the City of Albuquerque's first cluster housing project, and the Bosque project, and to be consistent with the Design Intent and Design Purposes established by the City of Albuquerque. The Design Standards established by this Site Plan for Subdivision.

PEDESTRIAN AND SITE AMENITIES

The creation of a pedestrian-friendly environment will depend on creative site design and will be a primary design objective for the Graham property. Objectives to achieve this goal include maintaining a high quality and consistency in style for site amenities including benches, plazas, walkways, lighting, and providing shaded walkways, and creating separate vehicle and pedestrian circulation systems in order to support the creation of a village-type character. Access to the Bosque will be a key feature for the property.

The use of alternative paving materials (brick, colored concrete, etc.) for pedestrian pathways are encouraged. Public art is another site amenity that is strongly encouraged, and if proposed, should be part of the subsequent building plans.

- Pedestrian connections to buildings should be provided in a walkable, safe manner, and should connect to adjacent roadways, sidewalks, and pathways.
- All pedestrian paths shall be designed to be handicapped accessible (see Americans with Disabilities Act Criteria for Barrier-Free Design).
- The use of asphalt paving for pedestrian paths is discouraged.
- Pedestrian crossings shall be clearly demarcated with special paving treatment where they cross vehicular entrances and drive aisles.
- Restaurants, if proposed by subsequent Site Plans for Building Purposes, shall be designed to include outdoor seating and shall be shaded by trees and/or a shade structure that is architecturally integrated with building architecture.
- Non-residential and multi-family development, if proposed by subsequent Site Plans for Building Purposes, shall provide secure bicycle storage racks that are conveniently located near building entrances, consistent with the City Zoning Code.

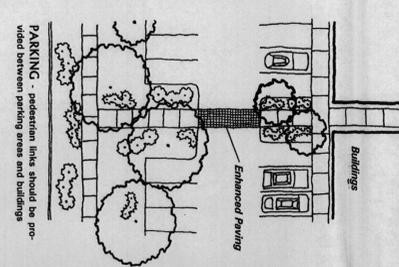


OUTDOOR AREAS - defined by building edges, differentiation of paving, variety of scales of landscaping

PARKING

In order to support the goals for the property regarding pedestrian accessibility, careful attention should be paid to the parking design. An effort should be made by site designers to lessen the impact of parking facilities on the land and to preserve views to the Bosque and the Santa and Marzano Mountains. In order to lessen the impact of parking areas, parking facilities should be broken up into a series of smaller areas.

- Handicapped parking spaces shall be provided adjacent to building entries.
- The total maximum amount of parking provided shall meet parking requirements in the City Zoning Code, plus 10 percent.
- Structures and on-site circulation systems should be located to minimize pedestrian/vehicle conflicts. Pedestrian access shall be provided to link structures to the public sidewalk.
- Parking areas shall be designed to include a pedestrian link to buildings.
- In cases where parking is adjacent to roadways, landscaping for screening purposes shall be required. The wall shall be architecturally compatible with the surrounding buildings.
- Parking is not allowed in the vicinity of the recreational fields in Tract 3.



PARKING - pedestrian links should be provided between parking areas and buildings

SETBACKS

The use of building and parking area setbacks is required to provide space for the creation of visually attractive streetscapes. Required within these setbacks will be pedestrian walkways and screening adjacent to roadways.

- Lot Setbacks**
- Front Yard: 20 feet
 - Side Yard: 5 feet
 - Rear Yard: 15 feet
- Setbacks on westerly edge of Tract 4 shall be 50 feet in order to preserve views from La Luz.

Parking Area Setbacks
To allow for an appropriately sized landscape buffer adjacent to roadways, parking areas shall be setback as follows:

- 15 feet

LANDSCAPE

The development of an overall landscape concept will establish a framework that unifies the property and is complementary to the Rio Grande Bosque. The landscape design should emphasize native and naturalized plant species.

All landscaped areas need to be coordinated and responsive to existing environmental conditions and local building policies. These standards are to be used as a supplement to the City requirements in the

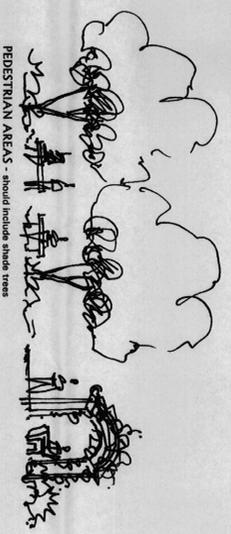


CONSENSUS PLANNING

Water Conservation Landscaping and Water Waste Ordinance, the Street Tree Ordinance, and landscape regulations included in the City of Albuquerque Comprehensive City Zoning Code.

The following are minimum standards for the development of specific landscape plans:

- Significant cottonwood specimens shall be identified and preserved, where feasible.
- Street trees shall be provided along roadways at a rate of one tree per 25 linear feet. They should be randomly placed.
- Individual lot owners will be responsible for the installation and maintenance of the landscaping on their property and within the adjacent public right-of-way, if any. All plant material, including trees, shrubs, groundcovers, turf, wildflowers, etc. shall be maintained by the lot owner in a living, attractive condition.



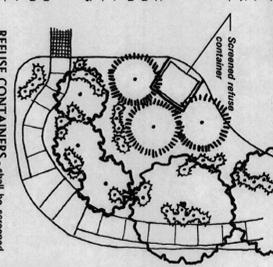
PEDESTRIAN AREAS - should include shade trees

- A minimum of 30 percent of the site area (minus the building square footage) shall be devoted to landscape materials.
- Landscaped areas shall be a minimum of 36 square feet and a minimum width of 6 feet. Living, vegetative materials shall cover a minimum of 75 percent of the landscaped areas. The area and percentage is calculated based on the mature canopy size of all plant materials.
- All planting areas not covered with turf shall have a ground topping of river rock, shredded bark, or similar material which extends completely under the plant material.
- Appropriate landscape headers shall be used to separate any turf and groundcover areas.
- To shade and mitigate the negative visual impact of large expanses of pavement, interior parking areas shall have one tree for each 10 parking spaces with no space being more than 100 feet from a tree.
- 75 percent of the required parking area trees shall be deciduous and have a mature height and canopy of at least 25 feet.
- An automatic underground irrigation system shall be provided to support all required landscaping. Irrigation components should be checked periodically to ensure maximum efficiency.
- All plant materials shall be chosen from the City of Albuquerque's Water Conservation Ordinance Plant List.
- Minimum plant sizes at time of installation shall be as follows:
 - Trees: 2 inch caliper, or 10 to 12 feet in height
 - Shrubs & Groundcovers: 1 gallon provide complete ground coverage within 1 growing season
 - Turf Grasses: Turf installation.

SCREENING / WALLS AND FENCES

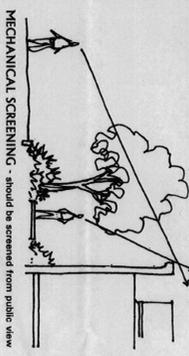
The effective use of screening devices for parking lots, loading areas, refuse collection, and delivery/storage areas is essential to limit their adverse visual impact on the property. The site orientation of these elements shall be away from any street or pedestrian area. The standards established in the landscape and setback sections will provide screening devices to screening unattractive elements and activities.

- Screening:
 - Parking areas shall be located away from adjacent streets and properties and shall be screened with plant materials, walls, or earth berming. Such screening shall have a minimum height of 3 feet.
 - Screening shall be designed to be aesthetically pleasing, graphically, the focus of the screening materials should be on trees.
 - All outdoor refuse containers shall be screened within a minimum 6 foot tall enclosure which is large enough to contain all trash generated between collections. Trash enclosures shall be screened with plant materials.



REFUSE CONTAINERS - shall be screened by a 6 foot tall enclosure and plant materials

- The design and materials for refuse collection enclosures shall be compatible with the architectural theme of the site.
- No refuse collection areas shall be allowed between streets and building fronts.
- All roof-mounted and/or ground-mounted equipment shall be screened from public view by materials of the same nature as the basic materials of the building.
- Unfinished block walls and barbed wire, chain link, concrete wire, and plastic/fiberglass fencing are prohibited.



MECHANICAL SCREENING - should be screened from public view

PERIMETER WALLS/FENCES:

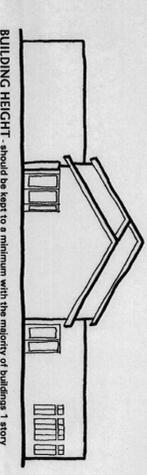
- Perimeter fencing is allowed on the property, however, an effort should be made by the site designer to lessen its visual impact through landscaping, meandering within a landscape area, and providing openings. Tensile wire fencing is encouraged for development adjacent to the Bosque to ensure visual access.
- Perimeter walls, if used, shall include pedestrian openings at key locations within the development to ensure convenient access to other areas within the property, as well as to the Bosque.
- Visual openings shall be provided in perimeter walls every 100 feet.

ARCHITECTURE

The design objective for architecture is to provide a series of buildings clustered together in order to create small interior plazas and leaving large areas dedicated to open space. Specific architectural style shall be determined at the time of the final architectural design. However, the architectural style shall be consistent with the overall design intent of the property and shall respond to climate, views, solar access, and aesthetic considerations.

Buildings and structures shall comply with all applicable City of Albuquerque zoning, building, and fire codes, as well as other local codes.

- Finished building materials shall be applied to all exterior sides of buildings and structures and shall be consistent with the architectural style of the main building, shall be of similar compatible design and materials.
- Generic franchise building elevations or canopies are prohibited.
- No plastic or vinyl building panels, awnings, or canopies are allowed. Awnings and canopies, if used, shall be integrated with building architecture.
- Building heights should be kept to a minimum, with the majority of the buildings to be 1 story. Building heights shall be limited to 45 feet for the ridge of the building to correspond with the Coors Corridor Plan.



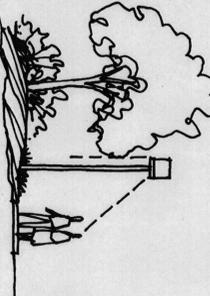
BUILDING HEIGHT - should be kept to a minimum with the majority of buildings 1 story

- Entry ways to non-residential and multi-family buildings shall be clearly defined.
- No freestanding cell towers or antennas are allowed; rather antenna shall be integrated with the building architecture.
- Highly reflective surfaces shall be screened from public view.

LIGHTING STANDARDS

In order to provide a safe, secure, and visually appealing environment, careful consideration must be given to both the daytime and nighttime appearance of the lighting design and fixtures. The primary design objective of the site lighting system shall be to maximize public safety while not affecting adjacent properties, buildings or roadways with unnecessary glare or reflection.

- Placement of fixtures and standards shall conform to applicable codes and local safety and illumination requirements.
- All lights shall be shielded source to prevent spillage onto adjoining properties or light pollution of the existing "dark sky". Cobra lights are prohibited.
- The height of street lights and parking area lights shall be kept to a minimum necessary to meet safety requirements.
- Individual site lighting standards shall blend with the architectural character of the buildings and other site fixtures.
- The location of light fixtures shall be identified on the Site Plan for Building Purposes. They shall be a maximum of 20 feet in height, consistent with the Coors Corridor Plan.



LIGHTING - should be shielded source, with the height kept to a minimum necessary to meet safety standards

- No parking or field lighting is allowed in the vicinity of the recreational fields in Tract 3.
- Neon lights are prohibited in the areas of Tract 6, adjacent to Tracts 3 and 4, and lying 300 feet north of Learning Road.

SIGNAGE STANDARDS

The following signage standards were developed to regulate the size, location, type, and quality of sign elements within the Graham property. The goal is to provide a signage program that is of high quality, maintaining a consistent style, creates a sense of arrival, and complements the visual character of the property.

- Entry signs shall be monument type and shall complement the materials, color, and architectural character of the buildings.
- Free-standing signs shall be designed that do not require any external bracing, angle-iron supports, guy wires or similar devices.
- No signage is allowed that uses moving parts, makes audible sounds, or has blinking or flashing lights.
- Signs shall not overhang into the public right-of-way or extend above the building roof line.
- Off-premise signs are prohibited.
- Building-mounted signs shall not exceed 5 percent of the facade area and the lettering shall not exceed 1 foot in height.
- No illuminated plastic panel signs are allowed except business logos within the non-residential areas of the property.

UTILITIES

To ensure the overall aesthetic quality of the property and the natural environment, the visual impact of utilities and equipment should be minimized by the following:

- All electric distribution lines shall be placed underground.
- Transformers, utility poles, and telephone boxes shall be appropriately screened with walls and/or vegetation when viewed from the public right-of-way.
- When an above-ground backflow prevention device is required by the City of Albuquerque, the head enclosure shall be constructed of materials compatible with the architectural materials used as the main elements of the building. If pre-fabricated fiberglass enclosures are used, they shall be appropriately screened from view by walls and/or landscaping.

RAY A. GRAHAM III

Site Plan for Subdivision

LANDS OF

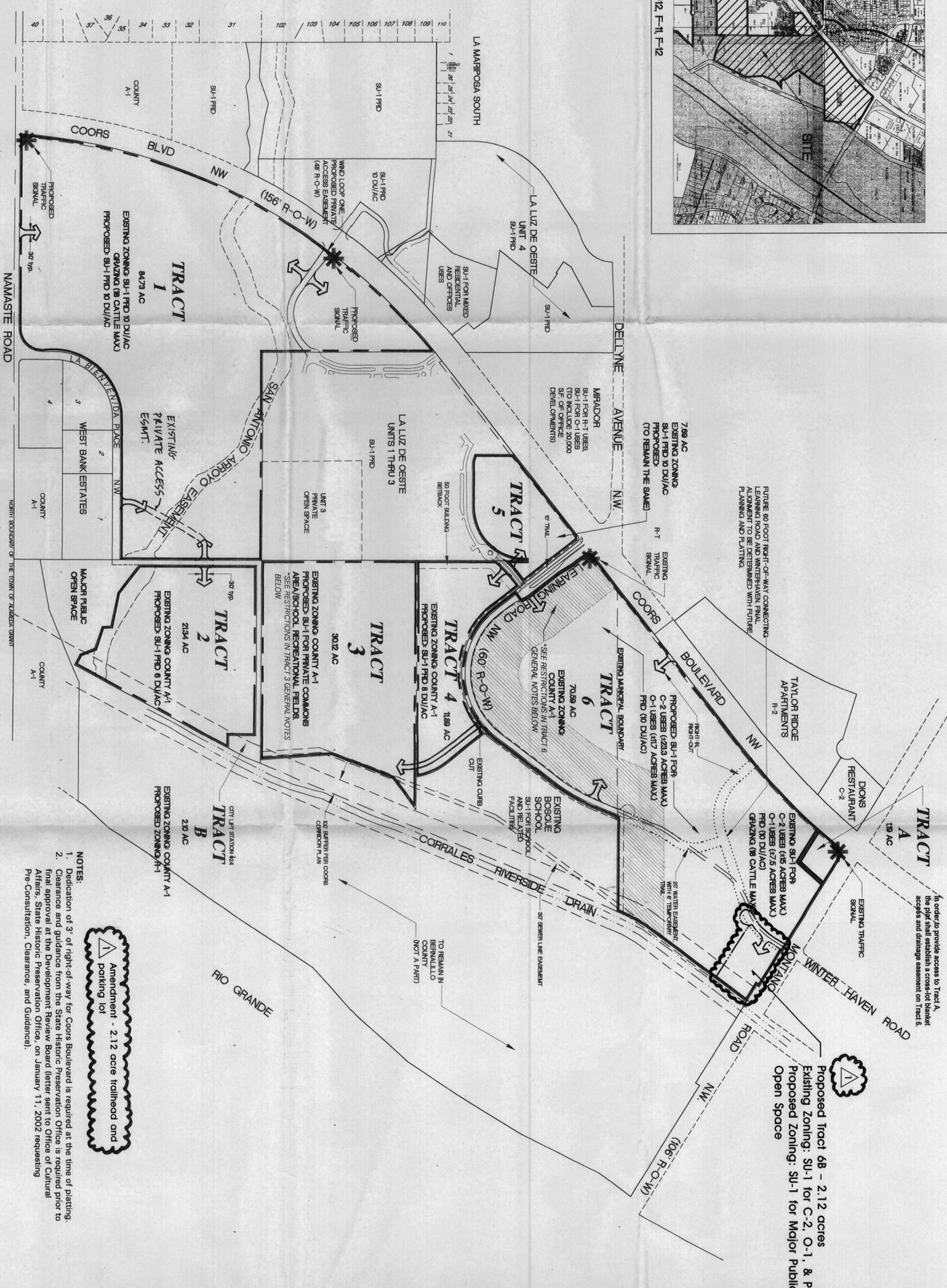
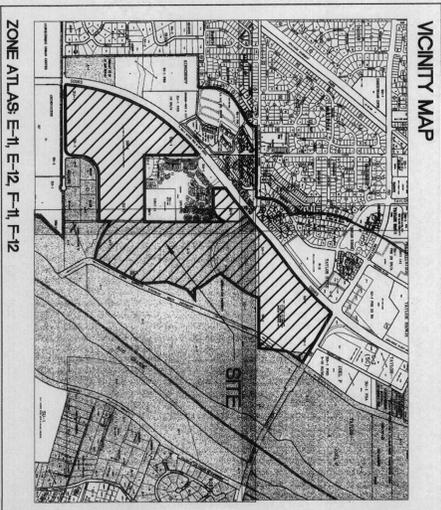
Prepared For:

Ray A. Graham III
1 Wind Road NW
Albuquerque, NM 87120

Prepared By:

Consensus Planning, Inc.
924 Park Avenue SW
Albuquerque, NM 87102

January 29, 2002



GENERAL NOTES:

Currently, all of the land covered by this Site Plan for Subdivision is unplatted. The Tract boundaries were created through actions by others. The goal is to: (1) annex Bernalillo County land to the City of Albuquerque and to establish zoning consistent with City goals and policies contained in the Coors Corridor Plan, West Side Strategic Plan, and the Comprehensive Plan; and (2) plat all of these tracts. Details regarding each tract are provided below:

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- TRACT 2:** This tract contains approximately 21.3 acres and is currently in Bernalillo County. It is bound by the City lift station right-of-way to the north and west, Corrales Riverside Drain to the east and the San Antonio Arroyo Easement to the south. The existing zoning is County A-1 and the proposal is to annex it to the City and establish SU-1 PRD 6 du/ac. Access to Tract 2 shall be provided at the time the Development Review Board approves the plat and this Site Development Plan for Subdivision.
- TRACT 3:** This tract contains approximately 30.1 acres and is currently in Bernalillo County. It is bound on the north by vacant County A-1 land (proposed SU-1 PRD 8 du/ac), Bosque School to the east, City-owned road to Lift Station 24 to the south and La Luz to the west. The existing zoning is County A-1. The property is proposed for annexation and establishment of SU-1 for Private Commons Area and School Recreational Fields. School Recreational Fields shall be limited to an area of 240' X 350' in the northeastern portion of Tract 3. No parking or field lighting is permitted in the vicinity of the Recreational Fields.
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NOTES:

- Dedication of 3' of right-of-way for Coors Boulevard is required at the time of platting.
- Clearance and guides from the State Historic Preservation Office is required prior to platting. State Historic Preservation Office, on January 11, 2002 requesting Pre-Consultation, Clearance, and Guidance.

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APPROVAL

Project # 1000065
EPC #
Date #

Planning Department	Date
Transportation Department	Date
City Engineer	Date
Utility Department	Date
Parks and Recreation Department	Date

SITE PLAN FOR SUBDIVISION - REQUIRED INFORMATION

THE SITE: The site consists of unplatted land, approximately 230.8 acres, with a portion in the City of Albuquerque and a portion in Bernalillo County. The portion in Bernalillo County is proposed for annexation and establishment of zoning.

PROPOSED USE: The proposed zoning for the annexation parcels is SU-1 for O-1, C-2, PRD (varying densities), and Private Commons Area/School Recreational Fields. The parcels in the City of Albuquerque are proposed to remain the same relative to zoning and land use.

PEDESTRIAN AND VEHICULAR ACCESS AND EGRESS: VEHICULAR ACCESS: Two new roads have been constructed (Learning Road and La Luz Connection Road) and one new 48' foot private access easement is planned (Wind Loop Road) to provide vehicular ingress and egress to these parcels, to increase safety to existing development, and to be consistent with City policies contained in the Coors Corridor Plan. Two additional right-in/right-out access points onto Coors Boulevard are proposed between Learning Road and Montano Road. Learning Road is a signalized intersection and Wind Loop Road is proposed to be a signalized intersection.

BICYCLE AND TRAIL ACCESS: Bicycle access is provided by 6' foot on-street bike lanes in Learning Road, Learning Road trail (a 10' foot trail within a 20' foot landscape easement), and a 6' foot temporary trail to connect the northern end of Learning Road to Montano Road.

TRANSIT ACCESS: Coordination with the City Transit Department shall be initiated at the Site Plan for Building Permit to provide access and service to this property. Coors Boulevard is a major transit route on the West Side.

INTERNAL CIRCULATION REQUIREMENTS: Conceptual access points (30 feet in width) have been identified on this Site Plan for each of the proposed parcels, final locations, width, and configuration shall be determined with future Site Plans for Building Permit with approval by the City Engineer. Internal sidewalks and/or trails shall be provided within each parcel with future Site Plans for Building Permit.

BUILDING HEIGHTS AND SETBACKS: See Sheet 2 of 2. Design Standards. Building height shall be consistent with the Coors Corridor Plan and should be kept to a minimum, with the majority of the buildings to be one-story.

MAXIMUM FAR: A maximum floor area ratio (FAR) shall be .35 for the SU-1 for C-2 and SU-1 for O-1 portions of Tract 6.

LANDSCAPE PLAN: The Design Standards (see Sheet 2 of 2) provide for preservation of significant cottonwoods, emphasis on native and naturalized plant species, landscape criteria, and landscape buffers. Subsequent landscape plans shall be consistent with City standards and policies regarding water conservation.

APPROVALS

PROJECT # 100065 DB# 02500 - 00114
EPC# 0028-0000-0743

Planning Director: *[Signature]* Date: 2/2/02
Transportation Development: *[Signature]* Date: 2-6-02
City Engineer/AV/AV/AV: *[Signature]* Date: 2-6-02
Utility Department: *[Signature]* Date: 2/6/02
Parks and Recreation Department: *[Signature]* Date: 2/6/02

Prepared For: Ray A. Graham III
1 Wind Road NW
Albuquerque, New Mexico 87120

Prepared By: Consensus Planning, Inc.
924 Park Avenue SW
Albuquerque, New Mexico 87120
Bordenave Designs
P.O. Box 91196
Albuquerque, New Mexico 87199

Scale 1"=300'

PLANNING
CONSENSUS

150' 0 300 600 north

January 22, 2002

Sheet 1 of 2

Design Standards

The purpose of these Design Standards is to provide a framework to assist the architect, landscape architect, and designers in understanding the vision and development goals for the property. The primary goal for this property is to achieve a vibrant, mixed-use community that fosters pedestrian accessibility and maintains a village-type character.

The Design Standards should be used to facilitate the design of buildings which respect the natural context of the site. The standards are intended to provide a framework for the design of buildings, signage, and architecture that will create the visual image desired for the Graham property. They are intended to be used in conjunction with the City of Albuquerque Comprehensive City Zoning Code.

These standards address the issues of landscape, setbacks, pedestrian amenities, screening, lighting, signage, and architecture that will create the visual image desired for the Graham property. They are intended to be used in conjunction with the City of Albuquerque Comprehensive City Zoning Code. Subsequent Site Plans for Building Purpose shall be consistent with the Design Standards established by this Site Plan for Subdivision.

PEDESTRIAN AND SITE AMENITIES

The creation of a pedestrian-friendly environment will depend on creative site design and will be a primary design objective for the Graham property. Objectives to achieve this goal include maintaining a high quality and consistency in style for site amenities including benches, plazas, walkways, lighting, etc.; providing shaded walkways; and creating separate vehicular and pedestrian circulation systems in order to support the creation of a village-type character. Access to the Bosque will be a key feature for the property.

The use of alternative paving materials (brick, colored concrete, etc.) for pedestrian pathways are encouraged. Public art is another site amenity that is strongly encouraged, and if proposed, should be part of the subsequent building plans.

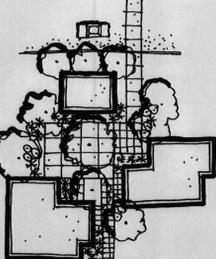
- Pedestrian connections to buildings should be provided in parking lots with greater than 50 spaces and should connect to adjacent roadways, sidewalks, and pathways.

- All pedestrian paths shall be designed to be handicapped accessible in accordance with the Americans with Disabilities Act (Criteria for Barrier-Free Design).
- The use of asphalt paving for pedestrian paths is discouraged.

- Pedestrian crossings shall be clearly demarcated with special paving treatment where they cross vehicular entrances and drive aisles.

- Restaurants, if proposed by subsequent Site Plans shall be designed to provide outdoor parking and shall be designed by an architecturally integrated building architecture.

- Non-residential and multi-family development, if proposed by subsequent Site Plans for Building Purpose, shall provide secure bicycle racks shall be determined by the number of parking spaces provided, consistent with the City Zoning Code.



OUTLOOK AREAS - defined by building edges, differentiation of paving, variety of scales of landscaping

PARKING

In order to support the goals for the property regarding the use of alternative paving materials, the design of the parking design effort should be made by site designers to lessen the impact of parking facilities on the land and to preserve views to the Bosque and the Serrada and Marzano Mountains. In order to lessen the visual impact of parking areas, parking facilities should be broken up into a series of smaller areas.

- Handicapped parking spaces shall be provided adjacent to building entries.

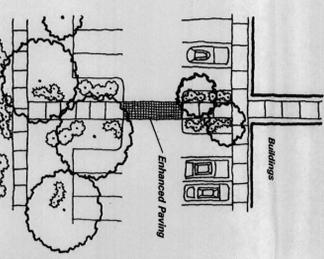
- The total maximum amount of parking provided shall be consistent with the City Zoning Code, plus 10 percent.

- Structures and on-site circulation systems should be located to minimize pedestrian/vehicle conflicts. Pedestrian access shall be provided to link structures to the public sidewalk.

- Parking areas shall be designed to include a pedestrian link to buildings.

- In cases where parking is adjacent to roadways, parking areas shall be architecturally compatible with the surrounding buildings.

- Parking is not allowed in the vicinity of the recreational fields in Tract 3.



PARKING - pedestrian links should be provided between parking areas and buildings

SETBACKS

The use of building and parking area setbacks is required to provide space for the creation of visually attractive streetscapes. Required within these setbacks will be pedestrian walkways and screening materials, including landscaping, earthen berms, and walls. Parking areas are discouraged from being adjacent to roadways.

Lot Setbacks

- Front Yard: 20 feet
 - Side Yard: 5 feet
 - Rear Yard: 15 feet
- Setbacks on westerly edge of Tract 4 shall be 50 feet in order to preserve views from La Luz

Parking Area Setbacks
To allow for an appropriately sized landscape buffer adjacent to roadways, parking areas shall be setback as follows:

- 15 feet

LANDSCAPE

The development of an overall landscape concept will establish a framework that unifies the property and is complementary to the Rio Grande Bosque. The landscape design should emphasize native and naturalized plant species.

All landscaped areas need to be coordinated and responsive to existing environmental conditions and local building policies. These standards are to be used as a supplement to the City requirements in the



Water Conservation Landscaping and Water Waste Ordinance, the Street Tree Ordinance, and landscape regulations included in the City of Albuquerque Comprehensive City Zoning Code.

The following are minimum standards for the development of specific landscape plans:

- Significant cottonwood specimens shall be identified and preserved, where feasible.
- Street trees shall be provided along roadways at a rate of one tree per 25 linear feet. They should be randomly placed.

- Individual lot owners will be responsible for the installation and maintenance of the landscape on their property and within the adjacent public right-of-way, if any. All plant material, including trees, shrubs, groundcovers, turf, wildflowers, etc. shall be maintained by the lot owner in a living, attractive condition.



PEDESTRIAN AREAS - should include shade trees

- A minimum of 30 percent of the site area (minus the building square footage) shall be devoted to landscape materials.

- Landscaped areas shall be a minimum of 36 square feet and a minimum width of 6 feet. Living, vegetative materials shall cover a minimum of 75 percent of the landscaped areas. The area and percentage is calculated based on the mature canopy size of all plant materials.

- All planting areas not covered with turf shall have a ground topping of river rock, shredded bark, or similar material which extends completely under the plant material.

- Appropriate landscape headers shall be used to separate any turf and groundcover areas.

- To shade and mitigate the negative visual impact of large expanses of pavement, interior parking areas shall have one tree for each 10 parking spaces with no space being more than 100 feet from a tree.

- 75 percent of the required parking area trees shall be deciduous and have a mature height and canopy of at least 25 feet.

- An automatic underground irrigation system shall be provided to support all required landscaping. Irrigation components should be checked periodically to ensure maximum efficiency.

- All plant materials shall be chosen from the City of Albuquerque's Water Conservation Ordinance Plant List:

- Minimum plant sizes at time of installation shall be as follows:
 - Trees: 2 inch caliper, or 10 to 12 feet in height
 - Shrubs & Groundcovers: 1 gallon
 - Turf Grasses: provide complete ground coverage within 1 growing season after installation.

SCREENING / WALLS AND FENCES

The effective use of screening devices for parking lots, loading areas, refuse collection, and delivery/storage areas is essential to limit their adverse visual impact on the property. The site orientation of these elements shall be away from any street or pedestrian area. Screening devices shall be designed to provide the main objectives to screening-unattractive elements and activities.

Screening:

- Parking areas shall be located away from adjacent streets and properties and shall be screened with plant materials, walls, or earthen berms. Such screening shall have a minimum height of 3 feet. Since the viewing public is from above site topographically, the focus of the screening materials should be on trees.
- All outdoor refuse containers shall be screened within a minimum 6 foot tall enclosure which is large enough to contain all trash generated between collections. Trash enclosures shall be screen with plant materials.

- The design and materials for refuse collection enclosures shall be compatible with the architectural theme of the site.
- No refuse collection areas shall be placed between streets and building fronts.

- All roof-mounted and/or ground-mounted equipment shall be screened by the walls of the same nature as the basic materials of the building.
- Unfinished block walls and barbed wire, chain link, concertina wire, and polyester fencing are prohibited.

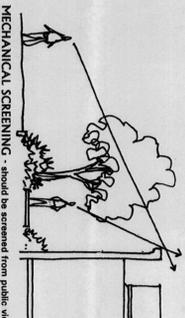
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- The design and materials for refuse collection enclosures shall be compatible with the architectural theme of the site.

MECHANICAL SCREENING



MECHANICAL SCREENING - should be screened from public view

Perimeter Walls/Fences:

Perimeter fencing is allowed on the property, however, an effort should be made by the site designer to create small interior plazas and leaving large areas dedicated to open spaces. Specific architectural style is not dictated at this time. However, the architectural design should demonstrate a high quality aesthetic that is complementary to the property and should respond to climate, views, solar access, and aesthetic considerations.

- Perimeter walls, if used, shall include pedestrian openings at key locations within the development to ensure convenient access to other areas within the property, as well as to the Bosque.
- Visual openings shall be provided in perimeter walls every 100 feet.

ARCHITECTURE

The design objective for architecture is to provide a series of buildings clustered together in order to create small interior plazas and leaving large areas dedicated to open spaces. Specific architectural style is not dictated at this time. However, the architectural design should demonstrate a high quality aesthetic that is complementary to the property and should respond to climate, views, solar access, and aesthetic considerations.

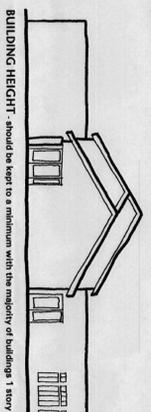
- Buildings and structures shall comply with all applicable City of Albuquerque zoning, building, and fire codes, as well as other local codes.

- Finished building materials shall be applied to all exterior sides of buildings and structures and shall be consistent on all sides. Any accessory buildings and enclosures, whether attached or detached from the main building, shall be of similar compatible design and materials.

- Generic franchise building elevations or canopies are prohibited.

- No plastic or vinyl building panels, awnings, or canopies are allowed. Awnings and canopies, if used, shall be integrated with building architecture.

- Building heights shall be kept to a minimum, with the majority of the buildings to be 1 story. Maximum height shall be limited to 45 feet for the ridge of the building to correspond with the Cors Corridor Plan.



BUILDING HEIGHT - should be kept to a minimum with the majority of buildings 1 story

- Entry ways to non-residential and multi-family buildings shall be clearly defined.
- No freestanding call towers or antennas are allowed; rather antenna shall be integrated with the building architecture.
- Highly reflective surfaces shall be screened from public view.

LIGHTING STANDARDS

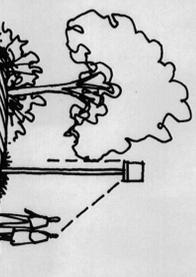
In order to enhance the safety, security, and visual aesthetics, careful consideration must be given to both the daytime and night-time appearance of the lighting design and fixtures. The primary design objective of the site lighting system shall be to maximize public safety while not affecting adjacent properties, buildings, or roadways with unnecessary glare or reflection.

In order to accomplish the lighting goals, the following guidelines shall be required for the design of the lighting system:

- Placement of fixtures and standards shall conform to state and local safety and illumination requirements.
- All lights shall be shielded source to prevent spillage onto adjoining properties or light pollution of the existing "dark sky". Color lights are prohibited.
- The height of street lights and parking area lights shall be kept to a minimum necessary to meet safety requirements.
- Individual site lighting standards shall blend with the architectural character of the buildings and other site fixtures.

- The location of light fixtures shall be identified on subsequent Site Plans for Building Purpose, consistent with the Cors Corridor Plan.

- No parking or field lighting is allowed in the vicinity of the recreational fields in Tract 3.
- Neon lights are prohibited in the area of Tract 6, adjacent to Tracts 3 and 4, and lying 300 feet north of Learning Road.



LIGHTING - should be shielded source, with the height kept to a minimum necessary to meet safety standards

SIGNAGE STANDARDS

The following signage standards were developed to regulate the size, location, type, and quality of sign elements within the Graham property. The goal is to provide a signage program that is of high quality, maintains a consistent style, creates a sense of arrival, and complements the visual character of the property.

- Entry signs shall be monument type and shall complement the materials, color, and architectural character of the buildings.
- Free-standing signs shall be designed that do not require any external bracing, angle-iron supports, guy wires or similar devices.
- No signage is allowed that uses moving parts, makes audible sounds, or has blinking or flashing lights.
- Signs shall not overhang into the public right-of-way or extend above the building roof line.
- Off-premise signs are prohibited.

- Building-mounted signs shall not exceed 5 percent of the facade area and the lettering shall not exceed 1 foot in height.
- No illuminated plastic panel signs are allowed except business logos within the non-residential areas of the property.

UTILITIES
The overall aesthetic quality of the property and the natural environment, the visual impact of utilities and equipment should be minimized by the following:

- All electric distribution lines shall be placed underground.
- Transformers, utility pads, and telephone boxes shall be appropriately screened with walls and/or vegetation when viewed from the public right-of-way.

- When an above-ground backflow prevention device is required by the City of Albuquerque, the heated enclosure shall be constructed of materials compatible with the architectural materials used as the main elements of the building. If pre-fabricated fiberglass enclosures are used, they shall be appropriately screened from view by walls and/or landscaping.

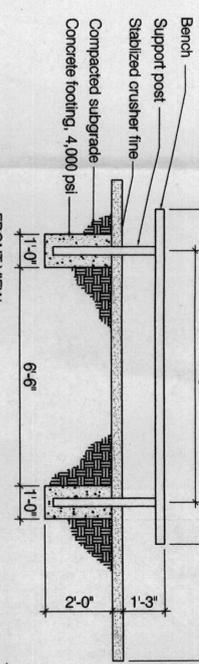
Site Plan for Subdivision

LANDS OF RAY A. GRAHAM III

Prepared For:
Ray A. Graham III
1 Wind Road NW
Albuquerque, NM 87120

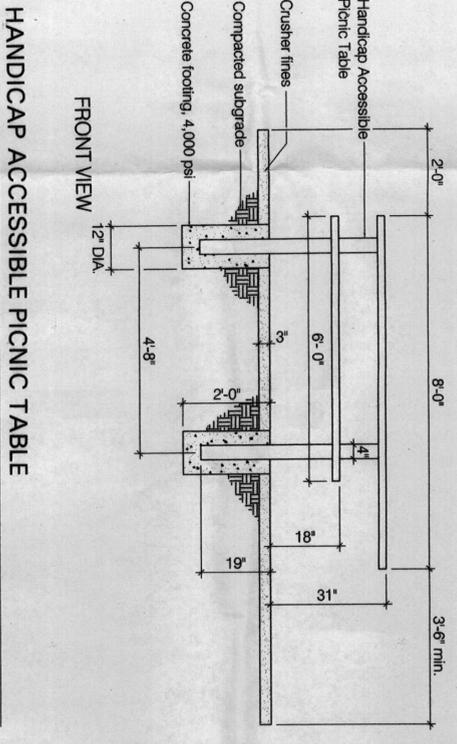
Prepared By:

Consensus Planning, Inc.
924 Park Avenue SW
Albuquerque, NM 87102
January 29, 2002



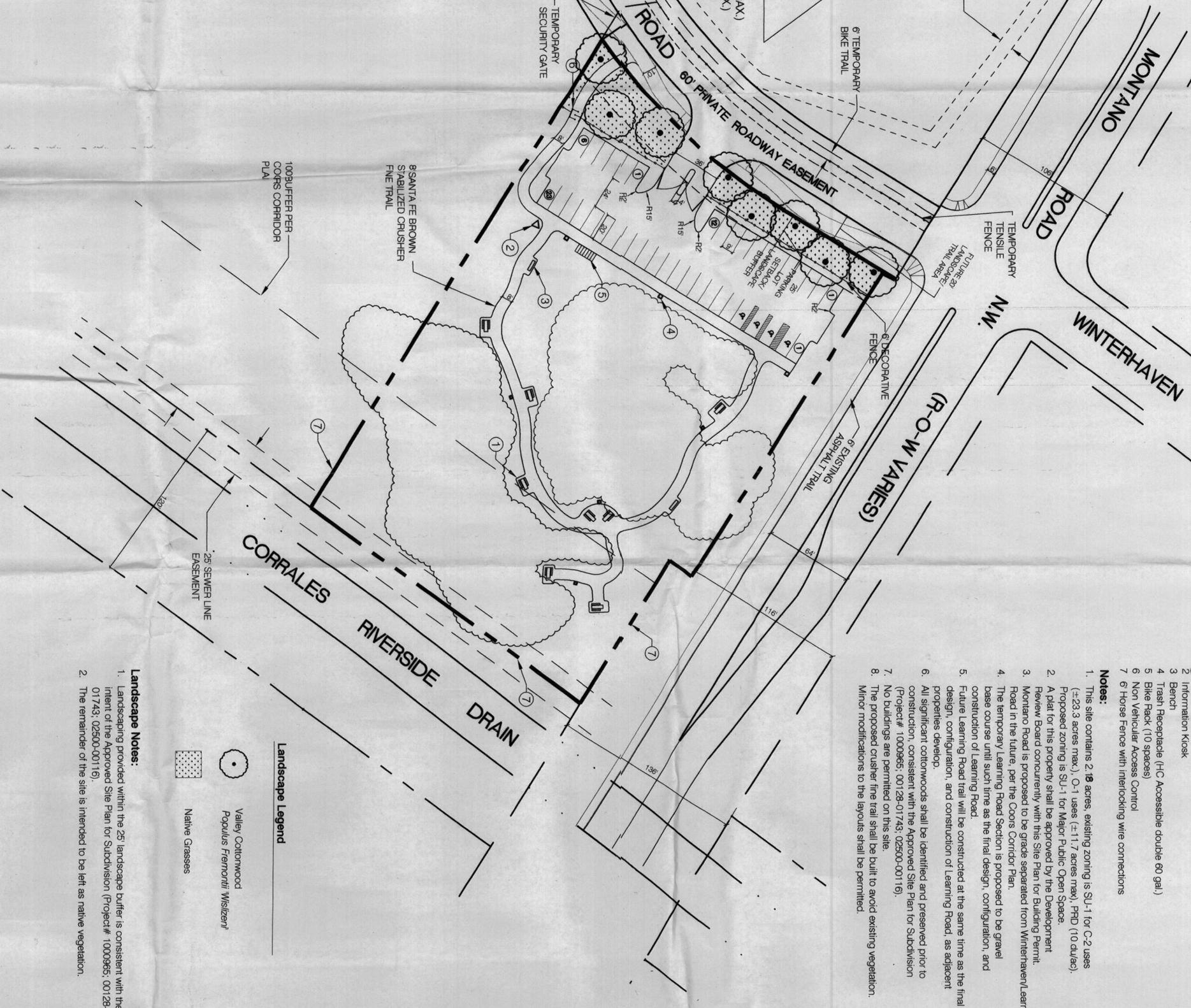
HANDICAP ACCESSIBLE BENCH

N.T.S.



HANDICAP ACCESSIBLE PICNIC TABLE

N.T.S.

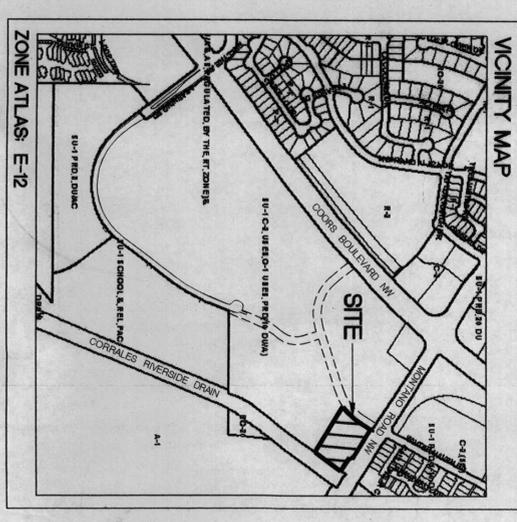
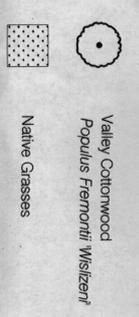


- 2 Information Kiosk
- 3 Bench
- 4 Trash Receptacle (HC Accessible double 60 gal.)
- 5 Bike Rack (10 spaces)
- 6 Non Vehicular Access Control
- 7 Horse Fence with interlocking wire connections

- Notes:**
1. This site contains 2.18 acres, existing zoning is SU-1 for C-2 uses (±23.3 acres max.), O-1 uses (±11.7 acres max.), PRD (10 du/ac). Proposed zoning is SU-1 for Major Public Open Space.
 2. A plat for this property shall be approved by the Development Review Board concurrently with this Site Plan for Building Permit.
 3. Montana Road is proposed to be grade separated from Winterhaven/Learning Road in the future, per the Coors Corridor Plan.
 4. The temporary Learning Road Section is proposed to be gravel base course until such time as the final design, configuration, and construction of Learning Road.
 5. Future Learning Road trail will be constructed at the same time as the final design, configuration, and construction of Learning Road, as adjacent properties develop.
 6. All significant cottonwoods shall be identified and preserved prior to construction, consistent with the Approved Site Plan for Subdivision (Project # 1000965; 00128-01743; 02500-00116).
 7. No buildings are permitted on this site.
 8. The proposed crusher fine trail shall be built to avoid existing vegetation. Minor modifications to the layouts shall be permitted.

Landscape Notes:

1. Landscaping provided within the 25' landscape buffer is consistent with the intent of the Approved Site Plan for Subdivision (Project # 1000965; 00128-01743; 02500-00116).
2. The remainder of the site is intended to be left as native vegetation.



VICINITY MAP

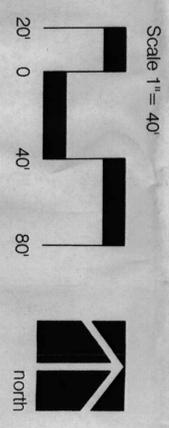
APPROVALS
 PROJECT # 1000965/02EPC-01769/01770/01771
 DRB#03DRB-00432

Sharon M. Nelson 3/26/03
 Planning Director Date
Bruce D. Bigham 12-20-03
 Transportation Development Date
David A. Johnson 3/26/03
 City Engineer/AMAF-CA Date
Christine Sankard 3/26/03
 Utility Development Date
 Parks and Recreation Department

Site Plan for Building Permit
COORS/MONTAÑO TRAILHEAD

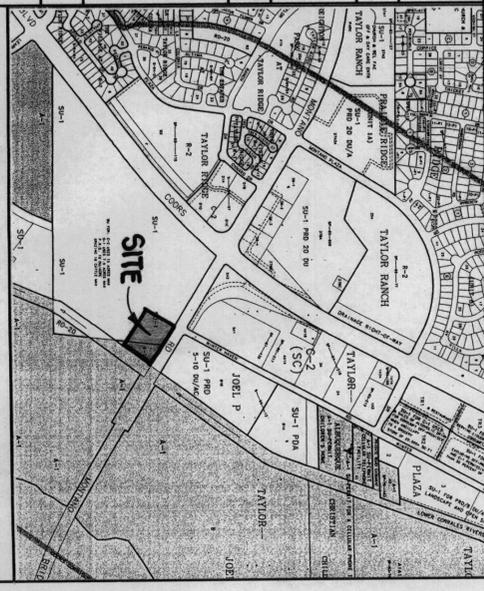
Prepared For:
 City of Albuquerque
 Open Space Division
 P.O. Box 1293
 Albuquerque, New Mexico 87103

Prepared By:
 Consensus Planning, Inc.
 924 Park Avenue SW
 Albuquerque, New Mexico 87102
 Bordenave Designs
 P.O. Box 91194
 Albuquerque, New Mexico 87199

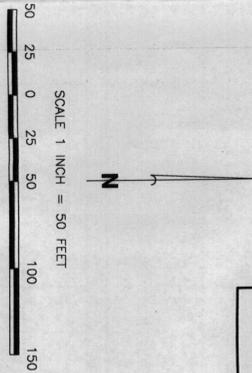
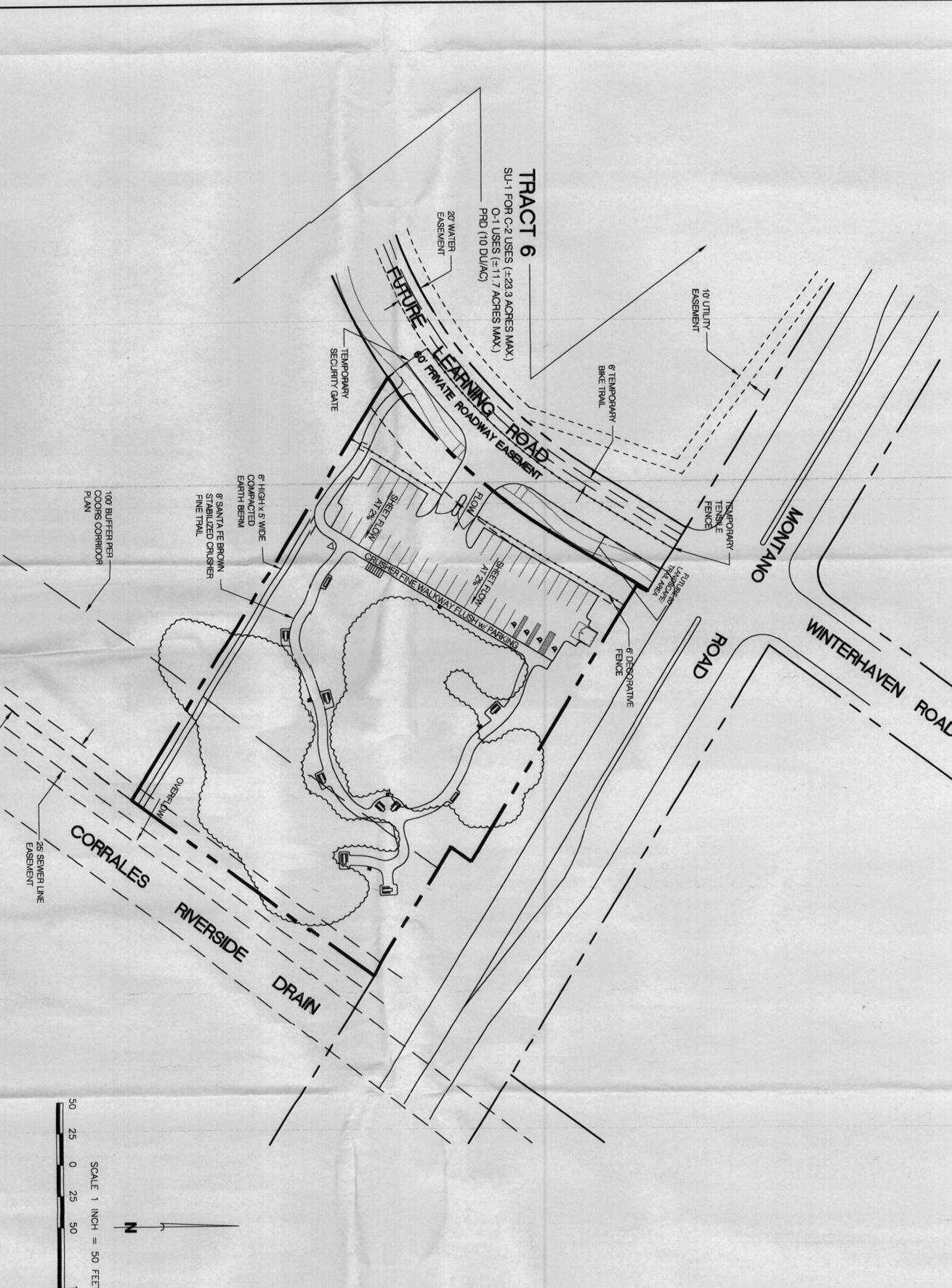


March 14, 2003

VICINITY MAP NO. E-12



LEGAL DESCRIPTION	PORTION OF TRACT 6, LANDS OF RAY A. GRAHAM, OWENSTOWN CORP. AND CITY OF ALBUQUERQUE
PERMANENT BENCHMARK	ACS NM448-N6A ELEVATION 5056.889
GRADING CERTIFICATION	I, Jean J. Bordenave, New Mexico Professional Engineer and Land Surveyor No. 5110, hereby certify that I have personally inspected the property shown hereon and that it appears that no grading, filling or excavation has occurred thereon since the contour map shown hereon was prepared. <i>Jean J. Bordenave</i> Jean J. Bordenave, NM PE & PS No. 5110
GENERAL NOTES	1. THE SITE IS TO BE USED FOR PARKING AND PICNICING FOR THE GENERAL PUBLIC AND WILL BE A CITY OF ALBUQUERQUE FACILITY. THE INTENT IS TO DISTURB THE AREA AS LITTLE AS POSSIBLE BY INCORPORATING FACILITIES IN THE NATURAL ENVIRONMENT. TRAILS WILL BE CROSSER FINES AND THE PARKING AREA WILL BE GRAVEL. NO BUILDINGS ARE PROPOSED FOR THE SITE. 2. THE AREA IS A PART OF THE MASTER GRADING AND DRAINAGE PLAN PREPARED FOR ALL THE AREA BOUNDED BY THE SAN ANTONIO ARROYO, COORS BLDG., MONTANO RD AND THE CORRALES RIVERSIDE DRAIN. THE PLAN WAS APPROVED FEBRUARY 6, 2002. THE PROPOSED PROJECT IS SHOWN TO UTILIZE WATER HARVESTING TO THE MAXIMUM EXTENT POSSIBLE WITH EXCESS STORMWATER RUNOFF TO EXIT THE SITE ALONG THE EASTERN BOUNDARY TO THE LOWER CORRALES RIVERSIDE DRAIN. IT IS NOT ANTICIPATED THAT OVERFLOWS WILL OCCUR IN THE 100 YEAR STORM. 3. THERE ARE NO OFFSITE FLOWS TO THE PROPOSED FACILITY. FLOWS FROM THE WEST ARE DIVERTED SOUTH OF THE PROPOSED FACILITY BY THE ENTRANCE ROAD FROM MONTANO ROAD. THE ENTRANCE ROAD IS APPROXIMATELY TEN FEET ABOVE THE SITE AT MONTANO ROAD AND MEETS THE EXISTING GROUND NEAR THE SOUTH BOUNDARY OF THE PROPOSED FACILITY. A COMPACTED EARTH BERM IS PROPOSED ALONG THE SOUTH BOUNDARY OF THE SITE TO MAINTAIN STORMWATER ONSITE. 4. THE SITE IS SHOWN TO BE IN ZONE X PER FEMA FIRM PANEL NO. 118, DATED SEPTEMBER 20, 1996. 5. PROPOSED FENCES FOR THE PROPERTY WILL BE OF AN OPEN TYPE AND WILL NOT IMPEDE STORMWATER RUNOFF.



DRAINAGE DATA

CONDITION	RETURN PERIOD	TREATMENT TYPE	AREA	EXCESS PRECIPITATION		PEAK RUNOFF		RUNOFF VOLUME	RUNOFF RATE
				in.	depth	cfs	ft/s		
EXISTING	10	A	95000	0.08	0.24	633	0.02	0	0.00
		B	0	0.22	1.48	0	0.00	0	0.00
		C	0	1.48	2.88	0	0.00	0	0.00
		D	0	1.24	2.88	633	0.02	0	0.00
TOTAL	TOTAL	TOTAL	95000	0.44	1.28	3465	0.02	0	0.00
PROPOSED	10	A	95000	0.08	0.24	444	0.00	0	0.00
		B	0	0.22	0.76	0	0.00	0	0.00
		C	0	0.44	1.48	1038	0.07	0	0.00
		D	0	1.24	2.88	1484	0.00	0	0.00
TOTAL	TOTAL	TOTAL	95000	0.22	0.76	3465	0.00	0	0.00
100	100	A	95000	0.44	1.28	2444	1.97	0	0.00
		B	0	0.67	2.03	2338	1.87	0	0.00
		C	0	0.89	2.87	4782	3.04	0	0.00
		D	0	1.97	4.37	4782	3.04	0	0.00
TOTAL	TOTAL	TOTAL	95000	0.44	1.28	94	0.00	0	0.00
TOTAL	TOTAL	TOTAL	11880	0.44	1.28	94	0.00	0	0.00
TOTAL	TOTAL	TOTAL	11880	0.44	1.28	94	0.00	0	0.00

- ASSUMPTIONS**
- PARKING AREA AND ACCESS ROAD ROAD DRAIN VIA SHEET FLOW TO EAST AND WATER PONDS ON EXISTING NATURAL AREA.
 - ONLY AREA EAST OF PROPOSED PARKING LOT AVAILABLE FOR PONDING.
- PONDING**
- 100 YEAR, 6 HOUR VOLUME FROM TABLE = 4782 C.F.
- PONDING AREA = 56400 S.F.
- AVERAGE DEPTH OF WATER = 0.08 FT

LEGEND

TM	TEMPORARY BENCHMARK
FF	FINISH FLOOR
FG	FINISH GRADE
FL	FLOWLINE
TA	TOP OF ASPHALT
TOP	TOP OF CONCRETE
TC	TOP OF CURB
TP	TOP OF EARTH PAD
TS	TOP OF SIDEWALK
TW	TOP OF WALL
FH	FIRE HYDRANT
WM	WATER METER
WV	WATER VALVE
MH	MANHOLE
CB	CATCH BASIN GRATE
GM	GAS MASTER
GV	GAS VALVE
LP	LIGHT POLE
PP	POWER POLE
GW	GUY WIRE
PED	ELEC. OR TEL. PEDISTAL
RD	ROOF DRAINAGE POINT

GRADING NOTES

- TWO (2) WORKING DAYS PRIOR TO ANY EXCAVATION, THE CONTRACTOR MUST CONTACT THE NEW MEXICO ONE CALL SYSTEM AT 280-1990 FOR LOCATION OF EXISTING UTILITIES.
- PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL EXCAVATE AND VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF ALL OBSTRUCTIONS. SHOULD A CONFLICT EXIST, THE CONTRACTOR SHALL NOTIFY THE ENGINEER SO THAT THE CONFLICT CAN BE RESOLVED WITH A MINIMUM AMOUNT OF DELAY.
- ALL WORK ON THIS PROJECT SHALL BE PERFORMED IN ACCORDANCE WITH ALL APPLICABLE FEDERAL, STATE AND LOCAL LAWS, RULES AND REGULATIONS CONCERNING CONSTRUCTION SAFETY AND HEALTH.
- ALL CONSTRUCTION WITHIN THE PUBLIC RIGHT-OF-WAY SHALL BE PERFORMED IN ACCORDANCE WITH APPLICABLE CITY OF ALBUQUERQUE STANDARDS AND PROCEDURES.

EROSION CONTROL NOTES

- THE CONTRACTOR SHALL ENSURE THAT NO SOIL ERODES FROM THE SITE ONTO PUBLIC RIGHT-OF-WAY OR PRIVATE PROPERTY. THIS CAN BE ACHIEVED BY THE CONSTRUCTION OF TEMPORARY SOIL BERMS OR SILT FENCES AT PROPERTY LINES AND BY SOIL TO PREVENT IT FROM BLOWING.
- THE CONTRACTOR SHALL PROMPTLY CLEAN UP ANY MATERIAL EXCAVATED WITHIN THE PUBLIC RIGHT-OF-WAY SO THAT THE EXCAVATED MATERIAL IS NOT SUSCEPTIBLE TO BEING WASHED DOWN THE STREET.
- THE CONTRACTOR SHALL SECURE A TOPSOIL DISTURBANCE PERMIT PRIOR TO BEGINNING CONSTRUCTION.

no.	date	remarks	by

project title
CITY TRAIL HEAD PARKING LEARNING RD AT MONTANO RD ALBUQUERQUE, NM

sheet title
GRADING & DRAINAGE PLAN

sheet date
11/20/02

design by
JJB

project no.
0214

sheet
of

BORDENAVE DESIGNS
P.O. BOX 91194, ALBUQUERQUE, NM 87199
(505)823-1344 FAX (505)821-9105



OFFSITE DRAINAGE MAP SCALE 1" = 200'